



REGULAR CITY COUNCIL MEETING AGENDA

City Hall - Council Chambers
4381 Broadway St., Suite 201, American Canyon
September 5, 2023
6:30 PM

Mayor: Leon Garcia
Vice Mayor: Pierre Washington
Councilmembers: Mariam Aboudamous, Mark Joseph, David Oro

AMENDED AGENDA

This agenda was amended to add Item 10. Support for ACA 13 (Ward).

City Council and other public meetings will be conducted in person at City Hall, 4381 Broadway, Suite 201, American Canyon, CA 94503. This meeting is also available via Zoom Teleconferencing as a convenience for public participation. This meeting will be broadcast live to residents on Napa Valley TV, on our website [here](#) and on YouTube [here](#). Should technical issues with Zoom occur, please select another viewing option.

PUBLIC PARTICIPATION

Oral comments, during the meeting: Oral comments can be made in person during Open and Closed Session. A Zoom Webinar has been established for public comments made via zoom, during Open Session only. To give your public comment via zoom, connect via the below Zoom link and use the “raise your hand” tool, or call into the zoom meeting at 408-638-0968 and press *9 to “raise your hand” when the item is called. To avoid confusion, all hands raised outside of Public Comment periods will be lowered.

Written comments, via eComments: Please submit written comments through the eComments link, located on the Meetings & Agendas page of our website [here](#). Comments will be available to council members in real time. To allow for review of comments, eComments will close at 3:00 pm on the day of the meeting. All comments received will be posted online and become part of the meeting record.

Zoom Meeting Link: [Click here.](#)

Webinar ID: 883 0140 0554 **Passcode:** 123456

The above-identified measures exceed all legal requirements for participation in public comment, including those imposed by the Ralph M. Brown Act. For more information, please call the Office of the City Clerk at (707) 647-4369 or email cityclerk@cityofamericancanyon.org.

AGENDA MATERIALS: City Council agenda materials are published 72 hours prior to the meeting and are available to the public via the City’s website at www.cityofamericancanyon.org.

AMERICANS WITH DISABILITIES ACT: The City Council will provide materials in appropriate alternative formats to comply with the Americans with Disabilities Act. Please send a written request to City Clerk at 4381 Broadway, Suite 201, American Canyon, CA 94503 or by email to cityclerk@cityofamericancanyon.org. Include your name, address, phone number and brief description of the requested materials, as well as your preferred alternative format or auxiliary aid, at least three calendar days before the meeting.

4:30 P.M. – CLOSED SESSION

The Mayor will call the meeting to order and conduct role call. Council will immediately convene into Closed Session after hearing any public comment on Closed Session items. At 6:30 p.m. the Council will reconvene into Open Session and then resume Closed Session at the end of the meeting to address outstanding items, if necessary.

CALL TO ORDER - CLOSED SESSION

ROLL CALL - CLOSED SESSION

PUBLIC COMMENTS - CLOSED SESSION ITEMS

This time is reserved for members of the public to address the City Council on Closed Session Items only. Comments must be made in person and are limited to 3 minutes. Comments for items on the Open Session agenda will be taken when the item is called in Open Session. Comments for Items not on the Closed Session or Open Session agenda will be heard during the Open Session Public Comment period.

MEETING RECESS - COUNCIL TO CONVENE IN CLOSED SESSION

4:30 P.M. CLOSED SESSION ITEMS

1. **Conference with Legal Counsel - Existing Litigation. Authorized pursuant to Government Code Section 54956.9(d)(1):**
 - a. *American Canyon I, LLC vs. Napa Unified School District*(Napa Superior Court Case No. 22CV001145).
 - b. *City of American Canyon v. City of Vallejo, et al.* (Sacramento Superior Court Case No. 34-2022-00327471).
 - c. *City of American Canyon v. Leon Dale Schmidt* (Napa County Superior Court Cases No. 22CV001041 and 23PR00161).
 - d. *Center for Biological Diversity v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000511).
 - e. *Golden State Environmental Justice Alliance v. City of American Canyon et al.* (Napa Superior Court Case No. 23CV000510).
 - f. *City of Vallejo v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000517).

2. **Conference with Legal Counsel – Anticipated Litigation. Authorized pursuant to Government Code Section 54956.9 (d)(2).
One Matter.**

3. **Conference with Real Property Negotiators - Authorized Pursuant to Government Code Section 54956.8
Property:**

APN	ADDRESS
a. 059-351-008	
b. 058-040-018	205 Wetlands Edge Road

Agency negotiator(s): City Manager Jason B. Holley & City Attorney William D. Ross
Under negotiation: Terms of property acquisition.

6:30 P.M. OPEN SESSION - REGULAR MEETING

CALL TO ORDER - COUNCIL TO RECONVENE IN OPEN SESSION

PLEDGE OF ALLEGIANCE

ROLL CALL - OPEN SESSION

REPORT ON CLOSED SESSION/CONFIRMATION OF REPORTABLE ACTION

PROCLAMATIONS AND PRESENTATIONS

4. [Proclamation - Hispanic Heritage Month](#)

PUBLIC COMMENTS - ITEMS NOT ON CLOSED SESSION OR OPEN SESSION AGENDA

This time is reserved for members of the public to address the City Council on items that are not on the Closed Session or Open Session agenda and are within the subject matter jurisdiction of the City Council. Comments are limited to 3 minutes. Comments for items on the Open Session agenda will be taken when the item is called in Open Session. The City Council is prohibited by law from taking any action on matters discussed that are not on the agenda, and no adverse conclusions should be drawn if the City Council does not respond to public comment at this time.

AGENDA CHANGES

The Mayor and Council may change the order of the Agenda or request discussion of a Consent Item. A member of the Public may request discussion of a Consent Item by making that request during Public Comment.

CONSENT CALENDAR

5. [Minutes of the Regular City Council meeting of August 15, 2023](#)

Recommendation: Approve the minutes of the Regular City Council meeting of August 15, 2023.

6. [Report Upon Return from Closed Session for the Regular City Council Meeting of August 15, 2023](#)

Recommendation: Approve the Report Upon Return from Closed Session for the Regular City Council Meeting of August 15, 2023.

7. [Watson Ranch Specific Plan Technical Amendment](#)

Recommendation: Waive final reading, read by title only, and adopt an Ordinance by the City Council of the City of American Canyon amending the Watson Ranch Specific Plan.

PUBLIC HEARINGS

8. [Public Hearing Notification Ordinance Amendment and Discretionary Project Site Posting Policy](#)

Recommendation: Take the following actions related to Discretionary Project Site Posting Policy:

1. Waive first reading, read by title only, and adopt an Ordinance by the City Council of the City of American Canyon amending American Canyon Municipal Code (ACMC) Chapter 19.40.050 to increase hardcopy property owner and tenant notification from 300 feet to 500 feet of a proposed project location.
2. Adopt a Resolution of the City Council of the City of American Canyon approving a Discretionary Project Site Posting Policy.

BUSINESS

9. **Vehicle Miles Traveled (VMT) policy to implement Senate Bill (SB) 743**

Recommendation: Adopt a Resolution of the City Council of the City of American Canyon, California approving a Citywide policy establishing Vehicle Miles traveled (VMT) as the standard of measurement for potential vehicle traffic impacts consistent with the California Environmental Quality Act (CEQA).

10. **Support for ACA 13 (Ward)**

Recommendation: Adopt a Minute Order authoring the Mayor to sign a letter of support for ACA 13 (Ward)

MANAGEMENT AND STAFF ORAL REPORTS

11. **Public Works Department Quarterly Update**

Recommendation: Information Only

12. **Building & Safety Division Update**

Recommendation: Information Only

MAYOR/COUNCIL COMMENTS, COMMITTEE REPORTS, AND FUTURE AGENDA ITEMS

The Mayor and Council may comment on matters of public concern and announce matters of public interest; no collective council action will be taken.

13. **City Council Committee Report - Councilmember Mark Joseph**

14. **Future Agenda Items of Note:**

September 19, 2023, Regular City Council Meeting. 6:30 p.m.

National Community Planning Month

Chamber of Commerce Presentation

Purchasing Policy

General Plan Update

Water Presentation

October 3, 2023, Regular City Council Meeting. 6:30 p.m.

National Code Enforcement Appreciation Month

Fire Prevention Month

Filipino Heritage Month

Paoli Watson Annexation EIR/General Plan Amendment/Rezoning

**City Council Compensation
Parks & Recreation Presentation**

October 17, 2023, Regular City Council Meeting. 6:30 p.m.
Domestic Violence Awareness Month
Environmental Compliance Presentation

ADJOURNMENT

CERTIFICATION

I, Taresa Geilfuss, CMC, City Clerk for the City of American Canyon, do hereby declare that the foregoing agenda of the City Council was posted in compliance with the Brown Act prior to the meeting date.

Taresa Geilfuss, CMC, City Clerk

CITY OF AMERICAN CANYON PROCLAMATION



Hispanic Heritage Month

WHEREAS, National Hispanic Heritage Month, known as “Mes de Herencia Hispana” is celebrated nationwide from September 15th through October 15th of each year. The observation started in 1968 as Hispanic Week and was expanded by President Ronald Reagan in 1988, to cover the month at which it is celebrated today. Understanding that September 15 is significant because it is the anniversary of the independence of Latin American countries Costa Rica, El Salvador, Guatemala, Honduras, and Nicaragua. In addition, Mexico and Chile celebrate their independence days on September 16th and 18th, respectively; and

WHEREAS, the City of American Canyon looks forward to celebrating “Mes de Herencia Hispana”, highlighting the rich heritage, arts, histories, food, music, and traditions of all 26+ Latin-American nations. While recognizing that these nations are not monogamous and this month celebrates, honors, and pays tribute to all for their contributions to our city and our community; and

WHEREAS, the City of American Canyon has a long history of welcoming immigrants and is home to a growing multiethnic and multicultural Latino population, all of whom are welcome and very much appreciated; and

WHEREAS, the diverse Latino population of American Canyon makes a significant economic contribution and has other profound positive influences on our community through their strong commitment to family, faith, education, hard work, culture, and service; and

WHEREAS, this community thrives on the diversity and ingenuity of all people and depends on the continued support and success of our diverse Latino population, and we will continue to be enriched by the transcultural contributions of our Latino friends and neighbors for many decades to come.

NOW, THEREFORE, BE IT RESOLVED that I, Mayor Leon Garcia, on behalf of the entire City Council, do hereby proclaim September 15th through October 15th, 2023, as Hispanic Heritage Month - “Mes de Herencia Hispana” in the City of American Canyon.

Dated: September 5, 2023

Leon Garcia, Mayor

**CITY OF AMERICAN CANYON
REGULAR CITY COUNCIL MEETING**

ACTION MINUTES
August 15, 2023

5:30 P.M. – CLOSED SESSION

CALL TO ORDER - CLOSED SESSION

The Closed Session was called to order at 5:30 p.m.

ROLL CALL - CLOSED SESSION

Present: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Absent: None

Excused: Councilmember David Oro

PUBLIC COMMENTS - CLOSED SESSION ITEMS

Mayor Garcia called for public comments. Written comments: none. Oral comments: none. The public comments period was closed.

MEETING RECESS - COUNCIL TO CONVENE IN CLOSED SESSION

5:30 P.M. CLOSED SESSION ITEMS

1. Conference with Legal Counsel - Existing Litigation. Authorized pursuant to Government Code Section 54956.9(d)(1):

American Canyon I, LLC vs. Napa Unified School District (Napa Superior Court Case No. 22CV001145).

City of American Canyon v. City of Vallejo, et al. (Sacramento Superior Court Case No. 34-2022-00327471).

City of American Canyon v. Leon Dale Schmidt (Napa County Superior Court Cases No. 22CV001041 and 23PR00161).

Center for Biological Diversity v. City of American Canyon et al. (Napa County Superior Court Case No. 23CV000511).

Golden State Environmental Justice Alliance v. City of American Canyon et al. (Napa Superior Court Case No. 23CV000510).

City of Vallejo v. City of American Canyon et al. (Napa County Superior Court Case No. 23CV000517).

2. Conference with Legal Counsel – Anticipated Litigation. Authorized pursuant to Government Code Section 54956.9 (d)(2). One Matter.

6:30 P.M. OPEN SESSION - REGULAR MEETING

CALL TO ORDER - COUNCIL TO RECONVENE IN OPEN SESSION

The meeting was called to order at 6:35 p.m.

PLEDGE OF ALLEGIANCE

The Pledge of Allegiance was recited.

ROLL CALL - OPEN SESSION

Present: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Excused: Councilmember David Oro

REPORT ON CLOSED SESSION/CONFIRMATION OF REPORTABLE ACTION

A report on Closed Session and confirmation of reportable action was given by City Attorney William Ross.

PROCLAMATIONS AND PRESENTATIONS

There were no proclamations.

PUBLIC COMMENTS - ITEMS NOT ON CLOSED SESSION OR OPEN SESSION AGENDA

Mayor Garcia called for public comments. Joan Bennett was called to speak; Justin Hamilton Hole was called to speak; Karen Fritz was called to speak. The public comments period was closed.

AGENDA CHANGES

There were no changes to the agenda.

CONSENT CALENDAR

Action: Motion to adopt CONSENT CALENDAR made by Councilmember Mark Joseph, seconded by Vice Mayor Pierre Washington, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

3. Minutes of the Regular City Council meeting of August 1, 2023

Action: Approved the minutes of the Regular City Council meeting of August 1, 2023.

4. Award Contract for the Irrigation Controller Upgrade Project (PR23-0150)

Action: Adopted Resolution 2023-63 awarding a construction contract (Agreement 2023-A129) to SiteOne Green Tech in the amount of \$394,862 and authorizing the Public Works Director to approve and execute Contract Change Orders in an aggregate amount not to exceed the Project Budget in conjunction with the Irrigation Controller Upgrade Project (PR23-0150).

5. Uniform Services Agreement with the Cintas Corporation

Action: Adopted Resolution 2023-64 authorizing the City Manager to enter into a five-year Agreement (Agreement 2023-A130) with Cintas Corporation No. 2 for a cumulative amount not to exceed \$205,000.

6. City Travel Policy

Action: Adopted Resolution 2023-65 approving the Revised City Travel Policy.

7. City Investment Policy

Action: Adopted Resolution 2023-66 amending the City's Investment Policy.

8. Acceptance of Completion of Deferred Improvement Agreement/Release of Lien: 1111 Green Island Road

Action: Adopted Resolution 2023-67 accepting the satisfaction of the Deferred Improvement Agreement (Agreement 2006-143) and release of the lien thereto for the Deferred Improvement Agreement associated with 1111 Green Island Road.

PUBLIC HEARINGS

9. Watson Ranch Specific Plan and General Plan Technical Amendment

Council received a staff report from Community Development Director Brent Cooper. Mayor Garcia opened the public hearing and called for public comments. Jeffery Gale was called to speak; Terrence McGrath was called to speak; Carlotta Sainato was called to speak; Hugh Marquez was called to speak. Public comments and the public hearing were closed.

Action: Motion to adopt Resolution 2023-68 of the City Council of the City of American Canyon amending the General Plan to ensure consistency with and between the General Plan and the Watson Ranch Specific Plan made by Vice Mayor Pierre Washington, seconded by Councilmember Mariam Aboudamous, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

Action: Motion Waive first reading, read by title only, and adopt an Ordinance by the City Council of the City of American Canyon amending the Watson Ranch Specific Plan made by Vice Mayor Pierre Washington, seconded by Councilmember Mariam Aboudamous, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

10. Napa Valley Ruins & Gardens Hotel and Condominiums Vesting Tentative Subdivision Map

Council received a staff report from Senior Planner William He. Mayor Garcia opened the public hearing and called for public comments. Written comments: none. Oral comments: none. Public comments and the public hearing were closed.

Action: Motion to adopt Resolution 2023-69 of the City Council of the City of American Canyon approving a Vesting Tentative Subdivision Map for the Napa Valley Ruins and Gardens Hotel and Condominiums to subdivide a 17.98-acre site into fourteen lots for condominium purposes in the Watson Ranch Specific Plan Mixed-Use Zoning District, APN 059-430-020 and 059-430-021 (File No. PL23-0007) made by Councilmember Mariam Aboudamous, seconded by Councilmember Mark Joseph, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

11. Promontory Subdivision Vesting Tentative Subdivision Map

Council received a staff report from Senior Planner William He. Mayor Garcia opened the public hearing and called for public comments. Written comments: none. Oral comments: Karen Fritz was called to speak; Hugh Marquez was called to speak; Reed Robinson was called to speak; Sarah Brunt was called to speak. Public comments and the public hearing were closed.

Action: Motion to adopt Resolution 2023-70 of the City Council of the City of American Canyon, California approving a Vesting Tentative Subdivision Map for development of 216 single-family residential lots and 54 junior accessory dwelling units (JADUS) on a 56.94-acre site in the Watson Ranch Specific Plan area, APN 059-030-007 (File No. PL22-0034) made by Councilmember Mark Joseph, seconded by Councilmember Mariam Aboudamous, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

BUSINESS

12. Voting Delegate and Alternate(s) for the League of California Cities 2023 Annual Business Meeting

Council received a staff report from City Clerk Taresa Geilfuss. Garcia/Aboudamous

Action: Motion to adopt Minute Order 2023-14 designating Mayor Leon Garcia as the Voting Delegate and Councilmember Mariam Aboudamous as the alternate for the League of California Cities 2023 Annual Business Meeting made by Councilmember Mark Joseph, seconded by Councilmember Mariam Aboudamous, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

MANAGEMENT AND STAFF ORAL REPORTS

13. Wastewater Operations Quarterly Update

Council received oral updates from Wastewater Operations Manager Jay Atkinson.

MAYOR/COUNCIL COMMENTS, COMMITTEE REPORTS, AND FUTURE AGENDA ITEMS

Councilmembers gave oral updates on their committees.

15. City Council Committee Report - Councilmember Mark Joseph

16. Future Agenda Items of Note:

September 5, 2023, Regular City Council Meeting. 6:30 p.m.
Latino Heritage Month

National CERT Week
Domestic Violence Awareness Month
Development Project Notice Requirements
Vehicle Miles Traveled Policy Resolution
Building and Safety Update
Public Works Quarterly Update

September 19, 2023, Special City Council Meeting. 6:30 p.m.
National Community Planning Month
Chamber of Commerce Presentation
Paoli/Watson Annexation EIR, General Plan Amendment and Pre-zoning
General Plan Update
Purchasing Policy - CUPCCAA
Water Presentation - MUD

October 3, 2023, Regular City Council Meeting. 6:30 p.m.
National Code Enforcement Appreciation Month
Fire Prevention Month
Filipino Heritage Month
Youth Commissioner Appointments

Action: Motion to add a future agenda item: 4th of July feedback and planning for 2024 made by Councilmember Mariam Aboudamous, seconded by Councilmember Mark Joseph, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

Action: Motion to add a future agenda item: Identify City owned properties suitable for low-income condos made by Councilmember Mark Joseph, seconded by Vice Mayor Pierre Washington, and CARRIED by roll call vote.

Ayes: Councilmember Mark Joseph, Councilmember Mariam Aboudamous, Vice Mayor Pierre Washington, Mayor Leon Garcia

Nays: None

Abstain: None

Excused: Councilmember David Oro

ADJOURNMENT

The meeting was adjourned at 9:25 p.m.

CERTIFICATION

Respectfully Submitted,

Taresa Geilfuss, CMC, City Clerk

William D. Ross
David Schwarz
Kypros G. Hostetter
Christina Bellardo

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File No: 199/6

August 16, 2023

VIA E-MAIL

The Honorable Leon Garcia, Mayor
and Members of the City Council
City of American Canyon
4381 Broadway, Suite 201
American Canyon, CA 94503

Re: Report Upon Return from Closed Session; Virtual Regular City Council Closed
Session Meeting of the American Canyon City Council of August 15, 2023

Dear Mayor Garcia and Members of the City Council:

This communication sets forth reportable action, if any, of the City Council (“Council”) of the City of American Canyon (“City”), consistent with provisions of the Ralph M. Brown Opening Meeting Act (Government Code Section 54950, *et seq.*) resulting from the Closed Session of the Virtual Regular City Council Closed Session Meeting of August 15, 2023, consistent with Government Code Section 54957.1.

Mayor Leon Garcia called the meeting to order in Open Session at 5:30 p.m. with all Council Members present and after determining that there were no public comments on Closed Session matters, the Council then adjourned to Closed Session at 5:30 p.m.

There were two matters agendized for City Closed Session consideration.

1. Conference with Legal Counsel – Existing Litigation pursuant to Government Code Section 54956.9(d)(1):
 - a. *American Canyon I, LLC v. Napa Valley Unified School District* (Napa County Superior Court Case No. 22CV001145).
 - b. *City of American Canyon v. City of Vallejo, et al.* (Sacramento Superior Court Case No. 34-2022-00327471).
 - c. *City of American Canyon v. Leon Dale Schmidt*, (Napa County Superior Court Case No. 22CV001041).

- d. *Center for Biological Diversity v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000511)
 - e. *Golden State Environmental Justice Alliance v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000510)
 - f. *City of Vallejo v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000517)
2. Conference with Legal Counsel – Anticipated Litigation
Authorized Pursuant to Government Code Section 54956.9(d)(2)
One Matter

With respect to Closed Session Agenda Item No. 1.a. (*American Canyon I, LLC v. Napa Valley Unified School District* (Napa County Superior Court Case No. 22CV001145)), although direction was given to the City Attorney, there was no reportable action under the common law attorney-client privilege and that provided by Government Code Section 54956.9(d)(1).

With respect to Closed Session Agenda Item No. 1.b. (*City of American Canyon v. City of Vallejo, et al.* (Sacramento Superior Court Case No. 34-2022-00327471)), there was no reportable action under the common law attorney-client privilege and that provided by Government Code Section 54956.9(d)(1).

With respect to Closed Session Agenda Item No. 1.c. (*City of American Canyon v. Leon Dale Schmidt*, (Napa County Superior Court Case No. 22CV001041)), there was no reportable action under the common law attorney-client privilege and that provided by Government Code Section 54956(d)(1).

With respect to Closed Session Agenda Item No. 1.d. (*Center for Biological Diversity v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000511)), 1.e. (*Golden State Environmental Justice Alliance v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000510)) and 1.f. (*City of Vallejo v. City of American Canyon et al.* (Napa County Superior Court Case No. 23CV000517)), collectively referred to as the *Golden State* matter, there was no reportable action under the common law attorney-client privilege or that provided by Government Code Section 54956.9(d)(1). All parties have received notice from the Sacramento Superior Court that the Court has received the transferred files.

With respect to Closed Session Agenda Item No. 2., there was no other reportable action under the common law attorney-client privilege and that provided by Government Code Section 54956.9(d)(2).

The Honorable Leon Garcia, Mayor
and Members of the City Council
August 16, 2023
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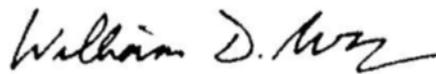
Your Council concluded the Closed Session at 6:24 p.m.

In Open Session, it was indicated that a written report upon return from Closed Session consistent with Government Code Section 54957.1, would be prepared concerning the matters agendized for Closed Session.

This communication should be reviewed under the Consent portion of the Agenda of your next Regular or Special City Council Meeting.

Should you have questions concerning this Report, it may be taken off the Consent calendar when agendized in the future, or our office may be contacted in the interim.

Very truly yours,



William D. Ross
City Attorney

WDR:jf

cc: Jason B. Holley, City Manager
Maria Ojeda, Assistant City Manager
Taresa Geilfuss, City Clerk
Cherri Walton, Deputy City Clerk



TITLE

Watson Ranch Specific Plan Technical Amendment

RECOMMENDATION

Waive final reading, read by title only, and adopt an Ordinance by the City Council of the City of American Canyon amending the Watson Ranch Specific Plan.

CONTACT

Brent Cooper, AICP, Community Development Director

BACKGROUND & ANALYSIS

The Watson Ranch Specific Plan (WRSP) is a comprehensive planning document that encompasses 309-acres and will contribute significantly to the identity of American Canyon with its unique draw of new residential neighborhoods and the Napa Valley Ruins & Gardens (NVRG). The WRSP area is located north of Vintage Ranch; east of the Union Pacific Rail Line; west of the City limits, and generally south of Watson Lane.

On August 15, 2023, the City Council approved the Watson Ranch Specific Plan and amended the General Plan to maintain consistency with the WRSP. A copy of the approved WRSP Ordinance is included as Attachment 1.

COUNCIL PRIORITY PROGRAMS AND PROJECTS

Community and Sense of Place: "Build on the strength of our local community to develop a clear 'sense of place' and establish our unique identity."

FISCAL IMPACT

A Fiscal Impact Analysis (FIA) was prepared for the WRSP at the time of the original approval. This FIA concluded that the proposed development will create a positive overall fiscal impact on the City's General Fund.

ENVIRONMENTAL REVIEW

The proposed Watson Ranch Specific Plan changes are consistent with the Project FEIR (SCH# 2015022030) and the Watson Ranch Specific Plan Development Agreement (Ordinance 2019-06,

Agreement 2019-A107). In accordance with CEQA, the Watson Ranch Specific Plan Project FEIR is a program EIR and is generally assumed to be used at a "development level." See, e.g., CEQA Guidelines § 15168. There is no substantial evidence of changed circumstances under CEQA Guidelines section 15162 requiring further CEQA review as the whole of the approvals sought are contemplated under the Project FEIR analysis.

ATTACHMENTS:

[1. CC WRSP Ordinance](#)

[1.1. Exhibit A WRSP Amendment - Final Draft May2023](#)

ORDINANCE NO. 2023-XX

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON AMENDING THE WATSON RANCH SPECIFIC PLAN (PL22-0023)

WHEREAS, pursuant to Section 65300 of the State Planning and Zoning Law (Gov. Code § 65000 *et seq.*), the City of American Canyon has adopted a General Plan to provide comprehensive long-range planning and a blueprint of the city's future form, including land use and circulation maps that specify the roadway network and the distribution of types and intensities of land; and

WHEREAS, Section 65358 of the State Planning and Zoning Law provides that the City Council may amend all or part of the General Plan if it deems the amendment to be in the public interest; and

WHEREAS, the Watson Ranch Specific Plan (WRSP) is a comprehensive planning document that encompasses 309-acres and will contribute significantly to the identity of American Canyon with its unique draw of new residential neighborhoods and the Napa Valley Ruins & Gardens (NVRG); and

WHEREAS, the WRSP area is located within north of Vintage Ranch; east of the Union Pacific Rail Line; west of the City limits, and generally south of Watson Lane; and

WHEREAS, on October 16, 2018, the City Council of the City of American Canyon amended the General Plan to recognize and ensure consistency with and between the City of American Canyon General Plan and the Watson Ranch Specific Plan; and

WHEREAS, on October 16, 2018, the City Council certified the Watson Ranch Specific Plan Draft and Final Environmental Impact Report (collectively, the "WRSP Final EIR") pursuant to the California Environmental Quality Act (Pub. Res. Code § 21000 *et seq.*, "CEQA"); and

WHEREAS, on November 6, 2018, the City Council of the City of American Canyon adopted the Watson Ranch Specific Plan by Ordinance pursuant to Municipal Code Chapter 19.12.020, establishing it as the regulating document for the development of the Watson Ranch Specific Plan site; and

WHEREAS, the Watson Ranch Specific Plan (WRSP) is a detailed planning document which provides a land use and policy framework for future development of approximately 309 acres within the City of American Canyon; and

WHEREAS, as a policy framework for development of 309 acres, the WRSP by necessity will require amendment from time-to-time to provide appropriate modifications to address current needs and improved ideas; and

WHEREAS, on June 4, 2019, the City Council of the City of American Canyon amended the General Plan to accommodate technical revisions to the Watson Ranch Specific Plan (Resolution 2019-40); and

WHEREAS, on June 18, 2019, the City Council of the City of American Canyon approved the first amendment to the WRSP (Ordinance 2019-05); and

WHEREAS, subsequent to June 18, 2019, the City of American Canyon has approved numerous implementing discretionary applications that have resulted in a Development Agreement; Large Lot Vesting Tentative Map; Street and Park Dedications; Lemos Pointe Apartments, Artisan and Harvest single-family homes; Tentative and Final Maps for single family subdivisions; the NVRG Hotel and Condominiums; two NVRG Amphitheaters; and approval of ministerial construction permits; and

WHEREAS, without changing the approved residential and commercial development intensity, the applicant, AC1-LLC, proposed a General Plan Amendment and Watson Ranch Specific Plan Amendment to implement improved ideas for a more efficient roadway circulation, park locations, public school needs, and clarification of narrative language and policy descriptions throughout the Specific Plan document (PL22-0023); and

WHEREAS, on May 25, 2023, the City of American Canyon Planning Commission conducted a public hearing and unanimously recommended approval of the Watson Ranch General Plan Amendment and Watson Ranch Specific Plan Amendment; and

WHEREAS, on August 15, 2023, the City of American Canyon City Council conducted a public hearing and approved the Watson Ranch Specific Plan General Plan Amendment; and

WHEREAS, on August 15, 2023, the City of American Canyon City Council conducted a public hearing, at which time all those in attendance were given the opportunity to speak on this Watson Ranch Specific Plan Amendment.

NOW, THEREFORE THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON DOES HEREBY ORDAIN AS FOLLOWS:

SECTION 1. Findings to Approve the Specific Plan Amendment in accordance with American Canyon Municipal Code Section 19.17.080

- A. The distribution, location and extent of land uses, including open space, as depicted in the specific plan is consistent with the general plan.

The Watson Ranch Specific Plan sets forth the land uses, residential densities, non-residential intensities, permitted and conditional uses, development standards, and design guidelines for the WRSP Area and further implements the Policies set forth for the Town Center land use designation. The Watson Ranch Specific Plan is consistent with the General Plan. This Amendment has a corresponding amendment to the General Plan and is therefore consistent.

- B. The specific plan provides for public infrastructure and services needed to support the land uses described in the plan, including adequate distribution, location, extent and intensity of transportation, sewage, water, drainage, solid waste disposal, energy, parks, community facilities and other essential facilities.

The Specific Plan does set forth the design for roadways, water, wastewater, recycled water, storm drainage systems, parks and other community facilities necessary to support the land

uses described in the plan. Specifically, Chapters 4, 5, 7 and 8 of the WRSP address these facilities. This Amendment is consistent with the provision of these facilities.

- C. The standards and development criteria, including requirements for resource utilization, will ensure that development proceeds in an orderly fashion and maintains a high level of quality.

Chapter 6 of the WRSP sets forth development standards which are supplemented by Design Guidelines in Appendix A. The development standards and design guidelines together promote efficient resource utilization, best practices, and sustainability to ensure a high level of quality. Further Chapter 9, Implementation, sets forth the phasing program to ensure the development proceeds in an orderly fashion. This Amendment does not alter the standards and development criteria set forth in the Specific Plan and is therefore consistent.

- D. The specific plan contains implementation measures, including financing programs, to ensure that development is supported by adequate infrastructure as it occurs.

WRSP Chapter 9, Implementation, sets forth the financing principles and policies, identifies different financing mechanisms, and sets forth phasing of supporting infrastructure. This Amendment supports the provisions of the Development Agreement which, consistent with the Specific plan, provides further clarity on financing and phasing of infrastructure. Therefore, this Amendment does not materially alter the implementation plan set forth in the Specific Plan and is therefore consistent.

- E. The site is suitable for the type and intensity of development proposed.

As determined through the initial existing conditions analysis and the preparation of the WRSP Final EIR, the WRSP site is suitable for the type and intensity of the proposed development associated with this Amendment. The Amendment does not alter the type and intensity of development proposal and is therefore consistent.

- F. The flexibility in development standards afforded by the specific plan process has resulted in a project providing more superior design and amenities than would occur under more traditional zoning practices, and the project provides clear and substantial benefit to the city.

The WRSP Project implements the General Plan for the Town Center land use designation, consistent with Objective 1.19 of the General Plan: "Provide for the development of a Town Center that physically and functionally serves as the symbolic and identifiable focus of community activities and events for the City of American Canyon and which is a regional destination within Napa Valley." General Plan Policy 1.19.7 designated that the land uses in this area be developed pursuant to one or more specific plans. The WRSP includes Development Standards and Design Guidelines that promote diverse neighborhood design and integrated housing types. It further provides for the creation of a "Town Center" through the renovation of the cement factory ruins while preserving the unique character of this site feature. This Amendment does not alter the flexibility in development standards, the superior design or amenities of the Specific Plan and is therefore consistent.

SECTION 2: Based on the findings described above, the City Council of the City of American Canyon hereby amends the Watson Ranch Specific Plan as depicted in Exhibit A to this Ordinance.

SECTION 3: CEQA FINDINGS: Regarding Environmental Review pursuant to the California Environmental Quality Act (CEQA)

1. This Specific Plan Amendment does not change the total number of dwelling units and commercial square footage established by the WRSP. Therefore, the environmental impacts of the WRSP Project which included the Specific Plan were adequately considered in the certified Final EIR for the Watson Ranch Specific Plan Project (SCN. 2015022030) which includes findings, a Statement of Overriding Considerations for the Project, and a Mitigation Monitoring and Reporting Program.
2. Approval of the Specific Plan Amendment based on the Final EIR, the Statement of Overriding Considerations for the Project, and the Mitigation Monitoring and Reporting Program complies with CEQA.

SECTION 4. EFFECTIVE DATE. This ordinance shall become effective effect 30 days after its final passage pursuant to Government Code section 36937.

SECTION 5. SEVERABILITY. If any section, sentence, clause or phrase of this Ordinance is for any reason held to be invalid or unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have passed this Ordinance and adopted this Ordinance and each section, sentence, clause or phrase thereof, irrespective of the fact that one or more sections, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

SECTION 6. CUSTODIAN OF RECORDS. The documents and materials that constitute the record of proceedings on which this Ordinance is based are located at the City Clerk's office located at 4831 Broadway, Suite 201, American Canyon, CA 94503. The custodian of these records is the City Clerk.

The foregoing Ordinance was introduced at a regular meeting of the City Council of the City of American Canyon, State of California, held on the 15TH day of August, 2023 by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

The foregoing Ordinance was adopted at a regular meeting of the City Council of the City of American Canyon, State of California, held on the 5TH day of September, 2023, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

ATTEST:

Taresa Geilfuss, CMC, City Clerk

Leon Garcia, Mayor
APPROVED AS TO FORM:

William D. Ross, City Attorney

Exhibits

A. Revised Watson Ranch Specific Plan

EXHIBIT A

WATSON RANCH SPECIFIC PLAN

**SPECIFIC PLAN AMENDMENT
APRIL 2023**



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ACKNOWLEDGEMENTS

City of American Canyon

City Council

Leon Garcia, Mayor

Pierre Washington, Council Member

Mariam Aboudamous, Council Member

Mark Joseph, Council Member

David Oro, Council Member

Kenneth Leary, Council Member*

Planning Commission

Crystal Dispo Mallare, Chair

Eric Altman, Commissioner

Brando Cruz, Commissioner

Andrew Goff, Commissioner

David Mohammed, Commissioner

Tyrone Navarro, Commissioner*

Tammy Wong, Commissioner*

Bernie Zipay, Commissioner*

Parks and Community Services Commission

Clarence Mamaril, Chair

Cathy Margolati, Commissioner

David Garcia, Commissioner

Charlie Plummer, Commissioner

Gina Griggs, Commissioner

Selah Hmun, Honorary Commissioner

Kristin Einberger, Commissioner*

Karina Servente, Commissioner*

Janelle Sellick, Commissioner*

Open Space, Active Transportation, and Sustainability (OSATS) Commission (Formerly Open Space Advisory Committee)

Nance Matson, Chair

Barry Christian, Commissioner

Tara Clinton-Horner, Commissioner

Scott Artis, Commissioner

Kim Hester Williams, Committee Member*

Todd Novak, Committee Member*

**Active during original adoption (2018)*

Steering Committee

Leon Garcia, Mayor
Mark Joseph, Councilmember
Barry Christian, Open Space Advisory Committee Representative
Don Evans, NVUSD Representative
Vanessa Kearns, Vintage Ranch Resident
Sanjay Mishra, Vintage Ranch Resident
Elizabeth Putnam, Planning Commission Representative
Janelle Sellick, Parks and Community Services Commission Representative

Staff

Jason Holley, City Manager
William Ross, City Attorney
John Wilbanks, Contract Planner
Brent Cooper, Community Development Director
Erica Ahmann Smithies, Public Works Director
Alexandra Ikeda, Parks and Recreation Director
Steve Hartwig, Public Works Director*
Creighton Wright, Parks and Community Services Director*

**Active during original adoption (2018)*

Applicants

American Canyon I, LLC
Newell Family

Adopted by Ordinance (Ordinance 2018-08)

October 16, 2018
Second Reading November 6, 2018

Amended by Ordinance (Ordinance 2019-06)

June 4, 2019
Second Reading June 18, 2019

Amended by Ordinance (Ordinance 2023-_____)

_____, 2023
Second Reading _____

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EXECUTIVE SUMMARY

The Watson Ranch Specific Plan (WRSP) Area is located within the city limits of American Canyon, in Napa County, California. The WRSP is a comprehensive planning document that will guide development of the approximately 309-acre WRSP Area. The WRSP Area is an important part of the City's General Plan and is intended to contribute significantly to the identity of American Canyon with its unique draw of new residential neighborhoods and the Napa Valley Ruins & Gardens (NVRG). The adaptive re-use of the NVRG will support community and private gatherings in a setting that is unlike anything else in the region. The WRSP completes the fabric of American Canyon by linking the community through roadways and regional trails and by creating a "Town Center" with community gathering places. New residential neighborhoods will provide a wide range of housing options. The WRSP development program includes the following elements:

amphitheater(s)

1. A mix of commercial, recreational, and community uses in and around the NVRG that will consist of:
 - a. An adaptive reuse of the NVRG area to include a variety of commercial uses, including a combination of indoor and outdoor spaces for an amphitheater, restaurants, pubs, distillery, wine tasting, breweries, food trucks, farmers' market, weddings, and corporate/public/private events. Uses not listed in the NVRG may be allowed through the City development review process (e.g., Conditional Use Permit).
 - b. A boutique hotel with approximately 200 rooms and 30 private residences as part of the hotel.
 - c. The Quarry Lake, with park, trail and open space uses.
 - d. Mixed-use and/or live-work housing of up to 10% of total units (included in residential project total).
2. Residential development consisting of varying housing types and densities to accommodate the current and future demographic trends. The summary of residential uses proposed in the plan are:
 - a. Approximately 102 acres (623 units) of medium density residential (MDR-12).
 - b. Approximately 65 acres (630 units) of medium density residential (MDR-16).
 - c. Approximately 6 acres (186 units) of high density residential (HDR) affordable housing.
3. Community amenities consisting of:
 - a. Approximately 63 acres of parks and open space.
 - b. A community trail system that includes connecting portions of the Napa Valley Vine Trail and the River to Ridge Trail.
 - c. A community plaza and site for an approximately 20,000 square foot community center that will serve as a gathering place for American Canyon visitors and residents.
4. Infrastructure improvements that include:
 - a. The extension of Newell Drive along the WRSP Area's eastern boundary.
 - b. The extension of Rio Del Mar from SR 29 to the Newell Drive Extension, to be named "Rio Del Mar East."
 - c. A new rail crossing at the Union Pacific rail line at Rio Del Mar East or South Napa Junction Road.
 - d. Internal circulation for the WRSP Area.
 - e. The water distribution system, sanitary sewer system, storm water drainage system and dry utilities to service the WRSP Area.

Finally, the WRSP sets forth a conceptual phasing plan and a financing program to address the timing of build out and the funding of the infrastructure associated with the WRSP.

General Plan Amendment

The General Plan Land Use and Circulation Elements were amended in October 2018 as specified in the WRSP and concurrent with adoption of the WRSP. A General Plan Amendment will be required as specified in this amended WRSP, as described below and in subsequent chapters.

PURPOSE

The purpose of the General Plan Amendment is to recognize, and ensure consistency with and between, the City of American Canyon General Plan and the Watson Ranch Specific Plan, as required by controlling law, by amending the Land Use Element and by amending the Circulation Element to provide for the optimally feasible access and circulation connection between SR 29 and Newell Drive through the WRSP Area.

In concert with the General Plan Amendment, which requires the adoption of this amended WRSP, the WRSP will be the document regulating development of the WRSP Area. Specifically, this General Plan Amendment revises the General Plan Land Use Element to reflect the WRSP and the Land Uses set forth therein, and revises the General Plan Circulation Element to reflect the roadways set forth therein and summarized below.

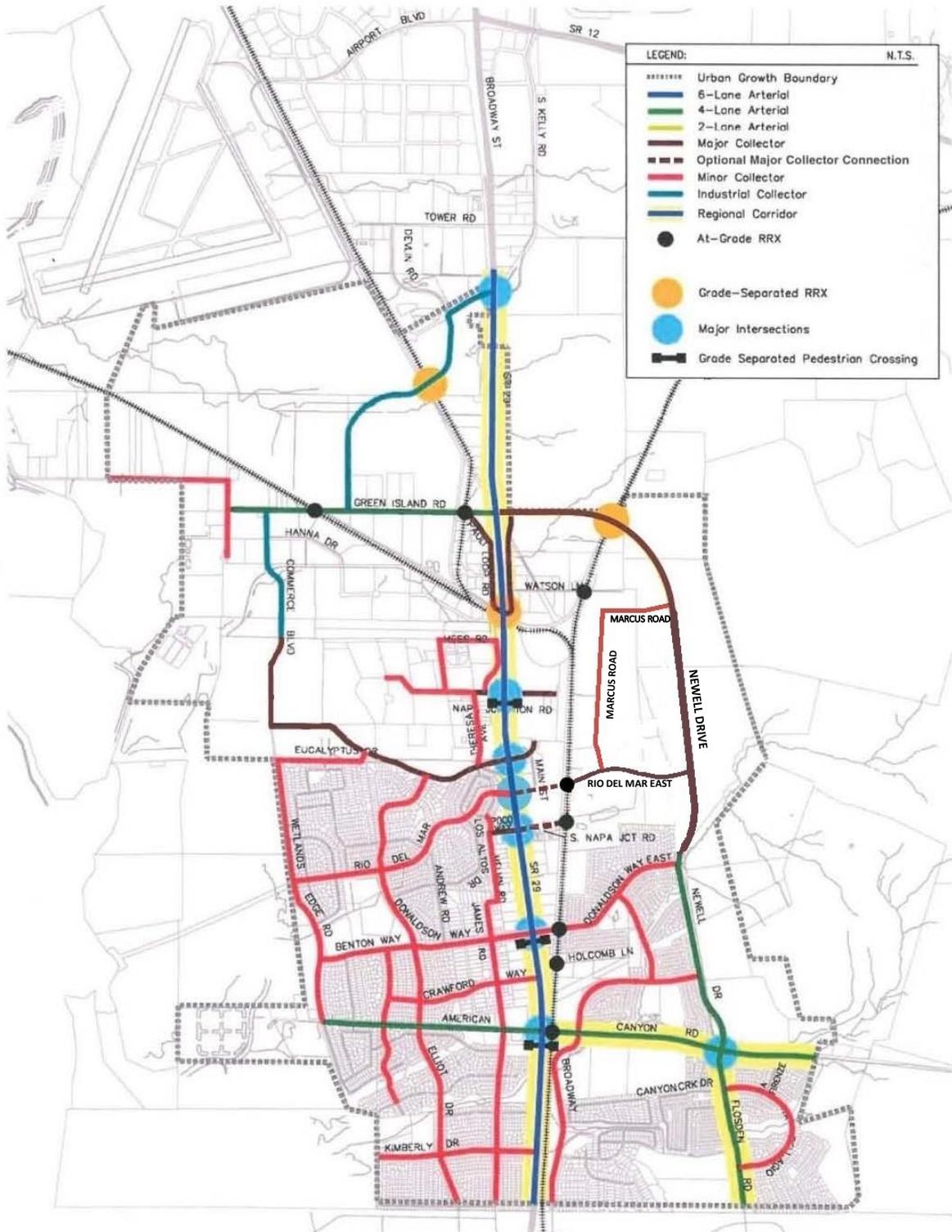
PROPOSED AMENDMENT

The General Plan amendment will be consistent with this amended WRSP, and will require the following changes:

5. Land Use Element
 - a. Amend the General Plan Map (Land Use Map) Figure 1-1 to incorporate the revised Land Use Plan set forth in the amended WRSP (Figure 4.1).
 - b. Amend the Land Use Element Policy 1.19.5 to read as follows: Require that the Town Center Residential Neighborhoods provide sites for public parks, with the size of facilities corresponding to the future population of the Town Center and which may allow community facilities that support residences.
6. Circulation Element
 - a. Amend Figure 3 (Major Circulation Improvements) of the Circulation Element to incorporate the revised Circulation Plan set forth in this amended WRSP (Figure 7.1). Amendments consist of:
 - Adding Marcus Road (formerly Loop Road) between Rio Del Mar East and Newell Drive as a “Minor Collector” roadway classification.
 - Changing the roadway classification of Newell Drive between Rio Del Mar East and the south project boundary to “Major Collector.”
 - Realigning Rio Del Mar East between the Union Pacific rail line and Newell Drive.

The above amendments to the Land Use Element and Circulation Element are consistent with the Goals, Objectives and Policies of the General Plan, which remain otherwise unchanged.

FIGURE 3:
CIRCULATION MAP



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CHAPTER 1 - INTRODUCTION

1.1 Purpose

The Watson Ranch Specific Plan (WRSP) is a detailed planning document which provides the land use and policy framework for the future development of approximately 309 acres within the City of American Canyon. The WRSP Area is located toward the northeastern side of American Canyon and to the east of Lincoln Highway, which is also designated as California State Highway 29. It is generally bound by open space to the north and the Vintage Ranch residential neighborhood to the south. It is further framed by Union Pacific railroad tracks and existing development to the west, and the City limit line, grassland, and open space to the east. Within the City of American Canyon General Plan, the WRSP Area is designated as Town Center (TC). The City's objective for the WRSP Area as stated by its General Plan is to:

“Provide for the development of a Town Center that physically and functionally serves as the symbolic and identifiable focus of community activities and events for the City of American Canyon, and which is a regional destination within the Napa Valley (GP Objective 1.19)”

An extensive public outreach process, demographic and market research, and the physical characteristics of the land have molded the WRSP to foster a premier mixed-use community with a strong sense of place. The development in accordance with the WRSP will provide an array of new residential homes and services for both residents and visitors. The WRSP is consistent with the community vision of American Canyon, as a community with a small-town character and feel, socially and ethnically diverse, and ideally located in Napa County in close proximity to the Bay Area. The proposed mix of housing, retail, commercial, entertainment, and parks will further strengthen the existing community ties while also providing a community gathering place. The purpose of the WRSP is to provide a comprehensive set of goals, objectives, policies, development standards, and design guidelines for the development of the WRSP Area. The implemented WRSP will result in a unique and appealing new community which provides a variety of new amenities and infrastructure improvements necessary to implement the development. The organization of this document reflects the progression of the planning process that helped shape the WRSP, moving from an explanation of the over-arching goals to specific chapters that deal with certain topics such as land use, and circulation which have policies that implement those goals.

The WRSP details the following elements:

- A range of new housing types and densities including High Density Residential (HDR) and Medium Density Residential (MDR-12 and MDR-16) neighborhoods.
- The Napa Valley Ruins & Gardens (NVRG) area consisting of civic, retail, entertainment, commercial, visitor serving, and residential uses focused in and around NVRG.
- Community amenities, including parks, trails and open space areas.
- Necessary public and private infrastructure improvements, including streets, pedestrian circulation, water distribution system, sanitary sewer system, storm water drainage system, and dry utilities for the WRSP Area.



1.2 Specific Plan Authority

Pursuant to the California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457), the WRSP is both a regulatory and policy document that will encourage and guide development within the WRSP Area over the coming years.

The WRSP provides a strong vision, a mix of land uses, development regulations, design guidelines, infrastructure improvements and an implementation plan for the creation of a cohesive community.

California Government Code (Section 65450) states that planning agencies may prepare specific plans for the systematic implementation of the general plan for all or part of the area covered by the general plan. A specific plan shall include text and diagrams which specify all of the following in detail:

- The distribution, location, and extent of the uses of the land within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, domestic water, recycled water, storm drainage, solid waste disposal, dry utilities, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
- Standards and criteria by which development will proceed, including development standards, design guidelines and a phasing program.
- A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out the plan.
- A statement of the relationship of the specific plan to the General Plan.

The WRSP has been prepared in accordance with the requirements of the California Government Code, Sections 65450-65457. These sections establish the WRSP as a legal mechanism by which future development within the WRSP Area is regulated.

1.3 Relevant Documents and Policies

This section examines several existing documents, policies and programs that have bearing on or otherwise provide direction for the WRSP. These relevant documents and policies are summarized below:

1.3.1 AMERICAN CANYON GENERAL PLAN

The WRSP is consistent with the American Canyon General Plan, which directs that a specific plan be prepared for the area identified as Town Center. The General Plan directs this area to be a mixed-use development with a mix of commercial, residential, parks and open space. State law states that specific plans can only be adopted or amended if they are consistent with the General Plan. The WRSP implements this policy direction and provides more specific detail and implementation policies. Appendix B contains a discussion of WRSP compliance with the City's General Plan.

The City of American Canyon Land Use Element, updated in 2010 and amended through October 2018, is an integrative element containing policy to guide development consistent with all elements of the general plan, addressing the retention of existing uses and open spaces, re-use of obsolete uses, and development of vacant lands. It also outlines opportunities and constraints, most notably the City's lack of a cohesive identity and structure within neighborhood districts and the need for a "symbolic and functional "downtown" for American Canyon. Additionally, the NVRG presented a uniquely identifiable asset to create a regionally significant project. The Land Use Element was amended in October 2018, as specified by the 2018 WRSP, to incorporate an amended General Plan Map (Land Use Map) and amended Policy 1.19.6.

The City of American Canyon Circulation Element, updated in 2013 and amended through October 2018, identifies a core value "to provide safe and easy travel within and through the City for pedestrians, bicyclists, and motor vehicles correlated with the Land Use Element". The City has adopted a "Complete Streets" policy which further reinforces the City's intention to ensure safe, comfortable, and attractive access and promoting connectivity between uses and areas of the City. The WRSP incorporates the City's core value for circulation and the complete streets policy.

The WRSP provides the community vision, land use plan, circulation plan, development regulations, design guidelines, and implementation measures to ensure development in a manner that is consistent with the goals, objectives, principles, and policies of the City of American Canyon General Plan.

1.3.2 AMERICAN CANYON MUNICIPAL CODE

The City of American Canyon Municipal Code, Title 19 (Zoning Ordinance) provides standards for site-specific development and land use regulations that govern the size, shape, and type of use for development in the City of American Canyon. In any instance where the WRSP regulations and development standards may vary from the Zoning Ordinance, the WRSP will take precedence. Where the WRSP is silent on a topic, the City of American Canyon Zoning Ordinance requirements will remain in force.

The WRSP will be adopted by ordinance and become the controlling zoning for the WRSP property pursuant to City of American Canyon Municipal Code Title 19, Chapter 19.12. Town Center Zoning District.

1.3.3 NAPA COUNTY AIRPORT LAND USE COMPATIBILITY PLAN

The Napa County Airport Land Use Commission (ALUC) has the responsibility of preparing an Airport Land Use Compatibility Plan ensuring that land uses in the area surrounding the airport are compatible with airport operations. A very small portion of the WRSP Area is partially subject to airport restrictions for Airport Zone D in the northwestern corner of the WRSP Area. Under the Napa Airport Land Use Compatibility Plan, this zone restricts residential development. The proposed plan is compatible with this requirement. A majority of the WRSP Area is within Zone E where there are no land use restrictions.

1.3.4 RELATIONSHIP TO OTHER PLANS

The WRSP Area is located adjacent to the SR 29 Priority Development Area (PDA) designated by Association of Bay Area Governments and Metropolitan Transportation Commission. The goal of the PDA is to relieve congestion and adopt a complete streets concept for the section of Highway 29 through American Canyon, while promoting a healthy mix of uses surrounding the highway that support the various modes of transportation.

The Broadway District Specific Plan (BDSP) was adopted by City Council in 2019 and updated September 1, 2020. The WRSP and the BDSP represent parallel planning efforts that work together to meet regional housing requirements and provide the opportunity for improved transit and walkability

consistent with the City's General Plan. The Broadway District is a Priority Development Area (PDA) as designated by the Association of Bay Area Governments (ABAG) and Bay Area communities. PDAs are areas specifically designated for new housing and job growth with some incentives to encourage investment. PDAs are considered foundational for sustainable regional growth and Plan Bay Area. PDAs emphasize housing at higher densities near transit or transit corridors.

Both the WRSP and the BDSP are coordinated with one another, especially on the Rio Del Mar extension which connects Highway 29 to Newell Drive. The overall goal of both projects is to create a complete street concept with mixed use walkable neighborhoods that is consistent with the vision of the American Canyon General Plan within both project boundaries. The WRSP has planned connections to and segments of the Napa Valley Vine and River to Ridge Trails. The WRSP Area is a key part of these future trail extensions, and will provide an important part of the overall regional trail system. The Napa Valley Vine Trail is expected to link 47 miles of dedicated multi-use trails from Calistoga south to Vallejo Ferry Terminal crossing through the WRSP Area from the north and intersecting the River to Ridge Trail (running east/west) as it makes its way south. The Circulation Element was amended in October 2018, as specified by the 2018 WRSP, to incorporate an amended Circulation Map and amended Table 3 (Major Circulation Improvements).

1.4 Public Outreach

1.4.1 STEERING COMMITTEE

For development of the 2018 WRSP, the City appointed an eight person Steering Committee made up of two Council members, one Planning Commissioner, one member each from the Parks and Community Services Commission (PCSC) and the Open Space Advisory Committee (OSAC), a representative of the Napa Valley Unified School District and two residents at large. City Department Heads also attended the Steering Committee meetings. Over several meetings early in the WRSP preparation process, the Steering Committee identified the Planning Principles which defined the vision for the WRSP Area. These Principles have been restated in Section 1.5 below,

1.4.2 COMMUNITY INPUT

The City values public involvement in the planning process. So, in addition to the involvement of the Steering Committee throughout the WRSP process, an extensive Community Outreach Program was implemented including community workshops and opportunities to participate and comment online.

1.5 Planning Principles

The WRSP Steering Committee (a group of citizens, City staff members, and elected officials) identified key issues and desired outcomes for the WRSP. After a review of General Plan goals, policies, and a visual character exercise, the Steering Committee identified a series of Watson Ranch Planning Principles that frame the vision for the WRSP Area. The following Principles have been restated and are intended to guide the WRSP Area towards a residential, commercial, and recreational destination that will serve as a centerpiece for the City and home to future residents. Planning principles are defined as vision-oriented statements developed by the Steering Committee during the community outreach process.

1.5.1 WATSON RANCH PLANNING PRINCIPLES

Principle 1 - Create an active, vibrant town center.

Principle 2 - Deliver an access and circulation plan that sufficiently addresses traffic.

Principle 3 - Advance an economically viable project that is compatible with existing American Canyon businesses.

Principle 4 - Provide a mix of retail, commercial, hospitality, and residential land uses.

Principle 5 - Protect and integrate the NVRG as a central feature/place.

Principle 6 - Provide a variety of public spaces and amenities.

Principle 7 - Reinforce the distinctive character of American Canyon and reflect the comfortable, and nostalgic small-town charm.

Principle 8 - Provide a blend of visitor and local serving uses.

Principle 9 - Provide gathering places for locals.

Principle 10 - Prioritize the restoration of the NVRG into the Town Center.

Principle 11 - Ensure a sustainable project – economic, social and environmental.

Principle 12 - Connect and integrate neighborhoods with vehicular, bicycle, and pedestrian connections throughout the WRSP Area.

Principle 13 - Ensure a range of housing opportunities to meet physical, economic and social needs of the population.

Principle 14 - Create traditional neighborhoods with strong architectural character and tree-lined streets.

Principle 15 - Reflect a historic small-town character in the residential neighborhoods adjacent to the NVRG

Principle 16 - Create a unique place that is the “gem” of American Canyon.



CHAPTER 2 - EXISTING CONDITIONS

2.1 Project Location

The City of American Canyon is situated along the southern tip of Napa County. Located on Highway 29, American Canyon is approximately eight miles south of the City of Napa and thirty-five miles northeast of San Francisco. The Watson Ranch Specific Plan (WRSP) Area occupies a

portion of the eastern edge of American Canyon within the City's urban limit line.

Figures 2.1 and 2.2 show the location of the WRSP Area in context with the surroundings, both regional and citywide.

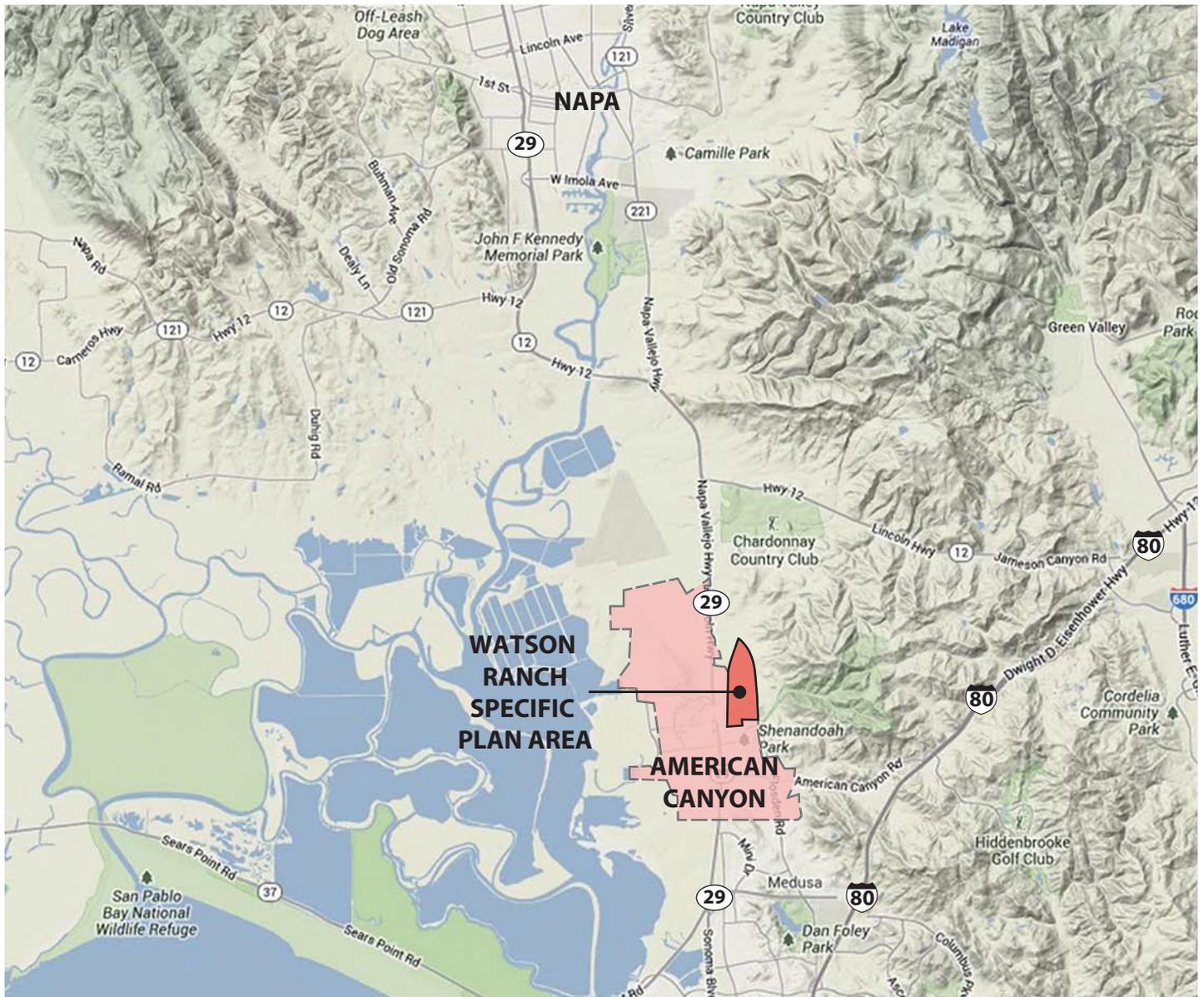


Figure 2.1: Regional Site Context

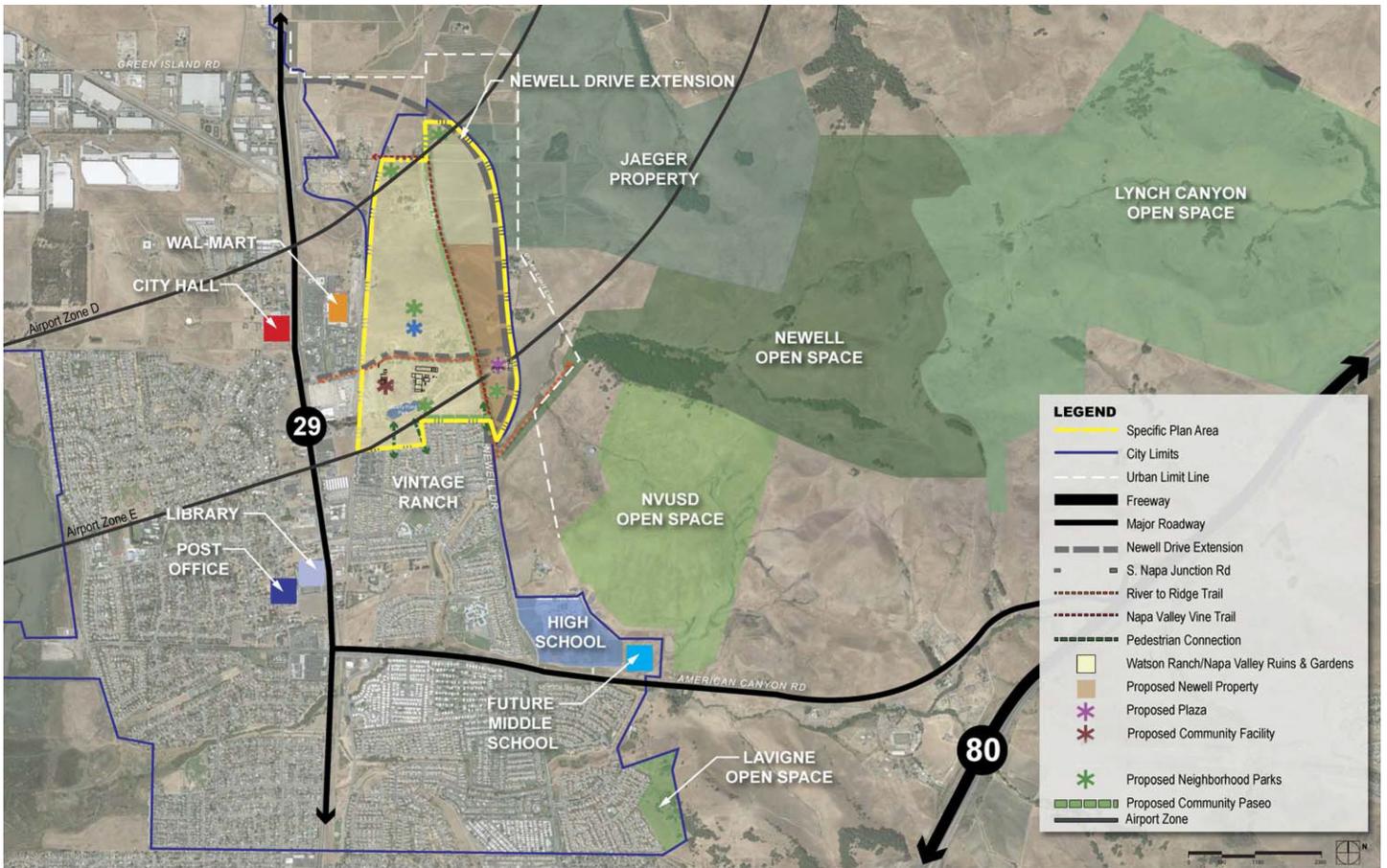


Figure 2.2: Local Context

2.2 Ownership

The WRSP Area is approximately 309 acres of which approximately 252 acres is owned by American Canyon 1, LLC (and its successor interests), while the remaining 57 acres is owned by the Newell Family interests. Figure 2.3

indicates the location of each individual ownership within the WRSP Area. American Canyon 1, LLC, and the Newell Family interests are co-applicants for the WRSP.

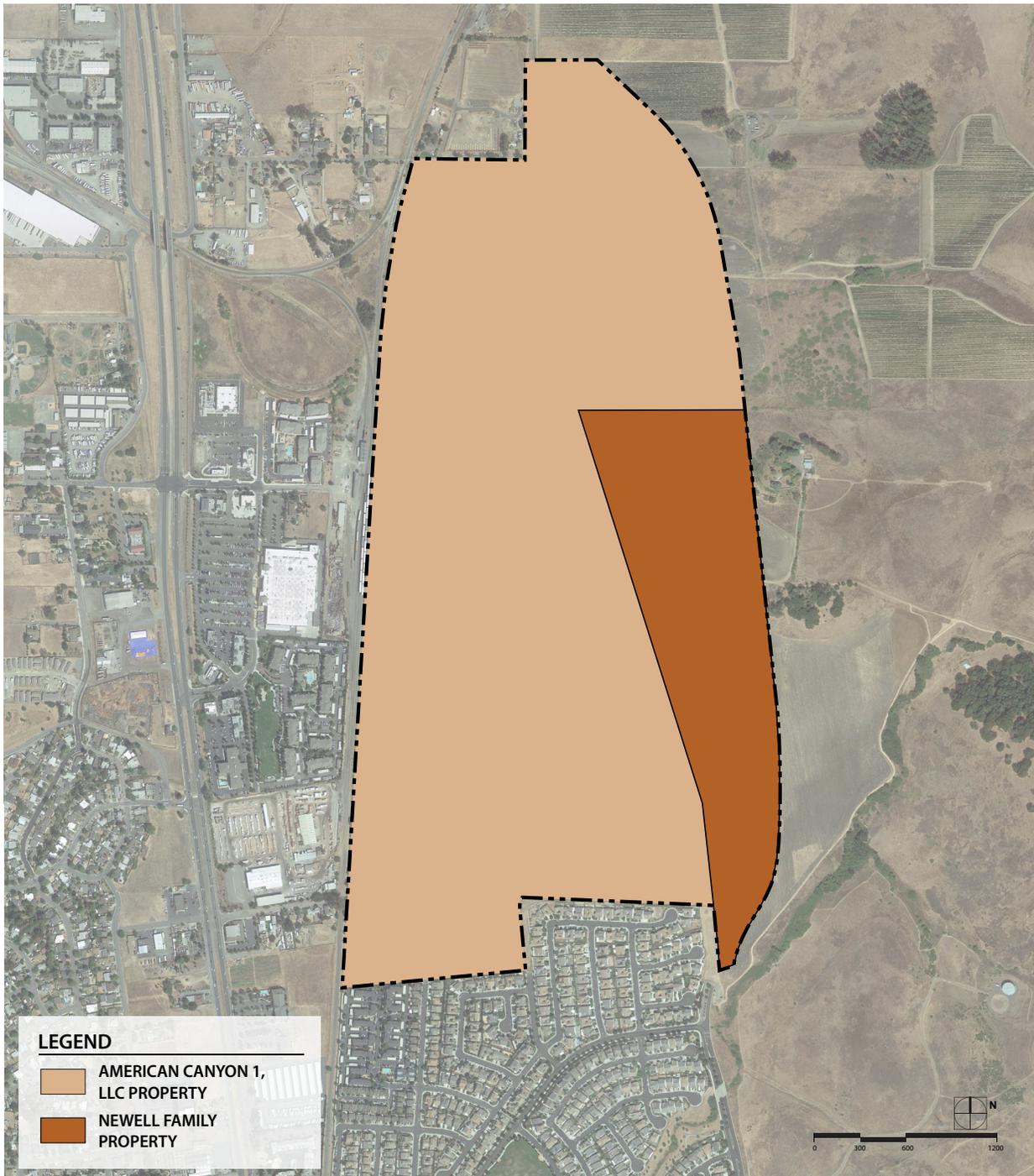


Figure 2.3: Property Ownership

2.3 Project Setting

The WRSP Area is bound on the west by a Union Pacific Railroad line, which is currently leased to California Northern Railroad (CFNR). CFNR has a switching yard adjacent to the western edge of the WRSP Area at Napa Junction Road. There is a permitted and unimproved private at-grade crossing at South Napa Junction Road. The rail line while active, is not heavily used. To the immediate south of the WRSP Area is the existing Vintage Ranch neighborhood

development. The eastern edge of the WRSP Area is the City limit line and will be defined by the future extension of Newell Drive. Beyond Newell Drive to the east is open space which includes the Newell Open Space Preserve. A small rural residential enclave is located outside the City limits off Watson Lane at the northwest end of the WRSP Area. (See Figure 2.4)

The existing terrain within the WRSP Area varies from relatively flat in the west, to gently rolling hills to the north

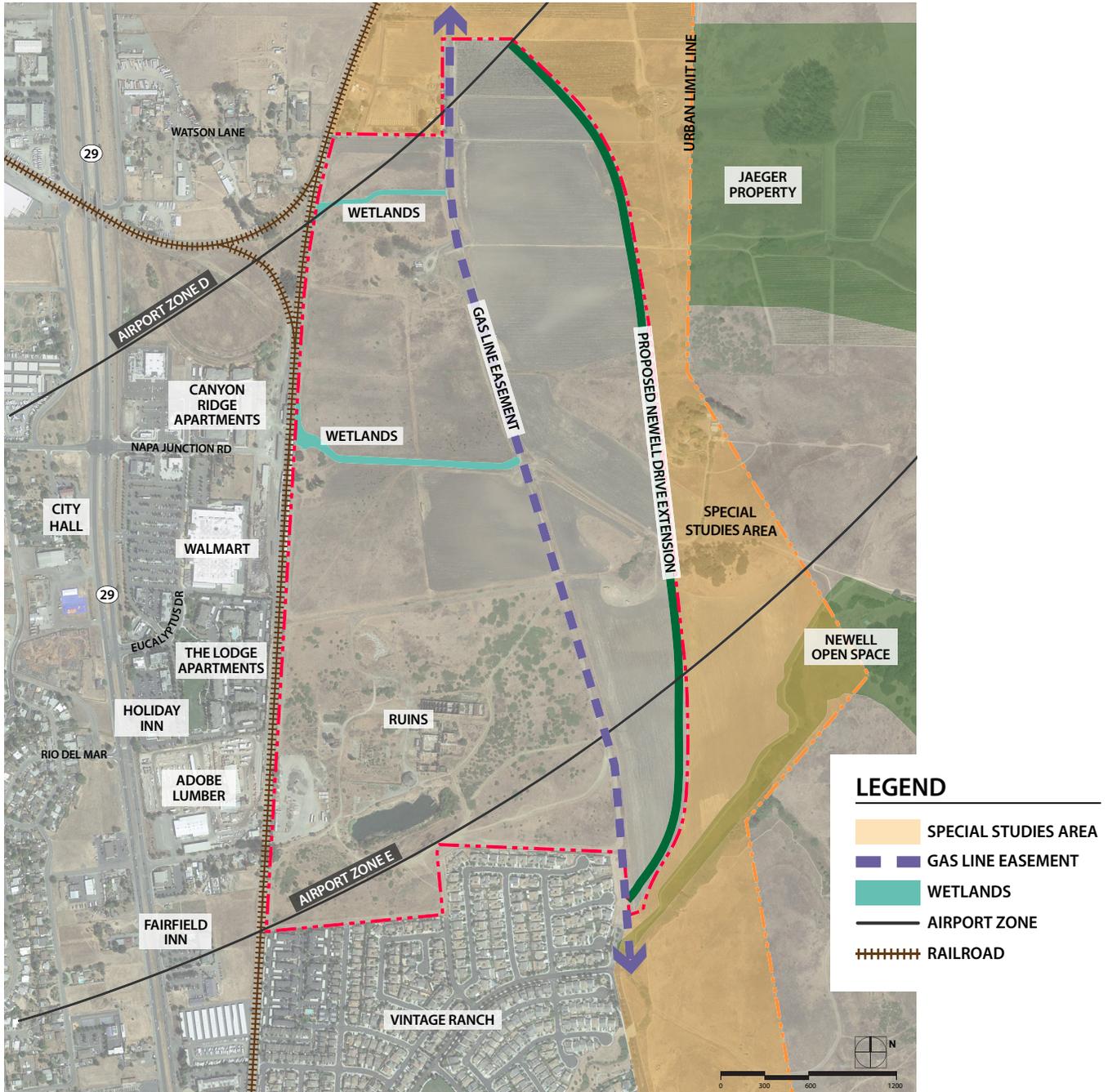


Figure 2.4: Existing Conditions Map

and east. In general, elevations drop from the east to the west. The vegetation on-site is predominantly short seasonal grasses with some limited hay production. Trees on site are limited with a cluster of eucalyptus trees on the north end of the WRSP Area.

Located in proximity to the Napa Valley Ruins & Gardens (NVRG) is an approximately two (2) acre lake referred to as the “quarry lake” or “pond”. There are two minor wetlands that generally run east to west across the site between the PG&E Gas Line and the Union Pacific Rail line. Numerous field studies were conducted for the WRSP Area. Biological features including wetlands and special status species were evaluated and preliminary results are reflected on Figure 2.4. Additional analysis and information may be found in the WRSP Environmental Impact Report (EIR).

Existing structures on site include the substantial concrete structures in the south end known as the NVRG. These are the remnants of a Portland cement plant, a turn of the century industrial use which manufactured cement. The Napa County Airport is located approximately two (2) miles northwest of the WRSP Area. As a result, the WRSP Area is subject to Napa County Airport Land Use Plan Compatibility Zones. These zones are designed to address airport noise and safety concerns by restricting certain land uses that are incompatible with airport operations. A small portion of the northwestern corner of the WRSP Area is affected by Zone D, which substantially restricts residential development. No residential uses are proposed for this area of the WRSP. A large portion of the WRSP Area is within Zone E (see Figure 2.4). Zone E has no land use restrictions but requires review by the Airport Land Use Commission for any General Plan Amendment or Specific Plan Amendment or approval. Therefore, the WRSP was referred to the Airport Land Use Commission (ALUC) in 2016, and the ALUC made a determination of consistency for the WRSP with the Airport Land Use Plan. The amended WRSP was referred to the ALUC in 2022, and the ALUC made a determination of consistency for the amended WRSP with the Airport Land Use Plan.

2.4 Site History

The Standard Portland Cement Company opened in 1898 to manufacture cement by excavating clay & limestone, which created seven limestone quarries in the southern portion of the site within the WRSP Area. This company, with up to 400 employees working in 12-hour shifts, produced 2,000 barrels of cement a day. This cement was vital in helping rebuild San Francisco after the 1906 earthquake and fire.

In the 1930s the cement company closed. In the 1950s, the Basalt Rock Company took over and produced lightweight aggregate used in concrete for high-rise buildings. This continued until 1978 when the Basalt Rock Company closed.

The 1898-1930 industrial uses created most of the structures known as the “ruins” that remain on the site today within the NVRG. Because of this long time frame, the walls and facilities have an amazing texture and context from weathering leaving the aggregate rough and exposed, giving it the feel of something from ancient Rome or Greece.

In 1985, Jaeger Vineyards purchased the property. After failed attempts at grape cultivation, it was determined that the soils on the site will not support vineyards and the property should be put to other uses.

American Canyon incorporated as a city in 1992. Thirty acres of the subject site, in the vicinity of the ruins, were included within the newly formed City limits. The remaining acreage stayed in the unincorporated County of Napa. Interest in the property began to grow as the future site of a new Town Center and, in 1994, the City’s General Plan designated the property as the “Town Center”. In 1999, as a result of ongoing public support for the project, American Canyon voters approved “Measure C” with more than 83% of the vote. This citizen initiative pre-zoned 70 acres of the property to “Town Center”. That same year, Napa County LAFCO added the 70-acre portion to the City’s Sphere of Influence, making it eligible for annexation.

In 2008, an Urban Limit Line (“ULL”) was approved by the City of American Canyon. The ULL governs growth boundaries for the City through the year 2030. In 2010, LAFCO approved the annexation of the WRSP Area. A lot line adjustment establishing the approved City limits was recorded in 2011.

In 2012, a concept for adaptive reuse of the ruins within the NVRG as a local and regional destination was prepared. The City of American Canyon reviewed the concept as well as the feasibility study prepared by the consulting firm Economic and Planning Systems, Inc. (EPS). In 2013 the City of American Canyon updated its Circulation Element, which assumed the land uses set forth in the WRSP. In 2018 the Circulation Element was amended to incorporate amendments set forth in the 2018 WRSP. The Circulation Element (along with the Land Use Element) will again be amended to reflect land use and circulation changes set forth therein as described in subsequent chapters. Both the Circulation Plan in Chapter 7 and the Land Use Concept in Chapter 4 respond to the policies and standards included in the Circulation Element.

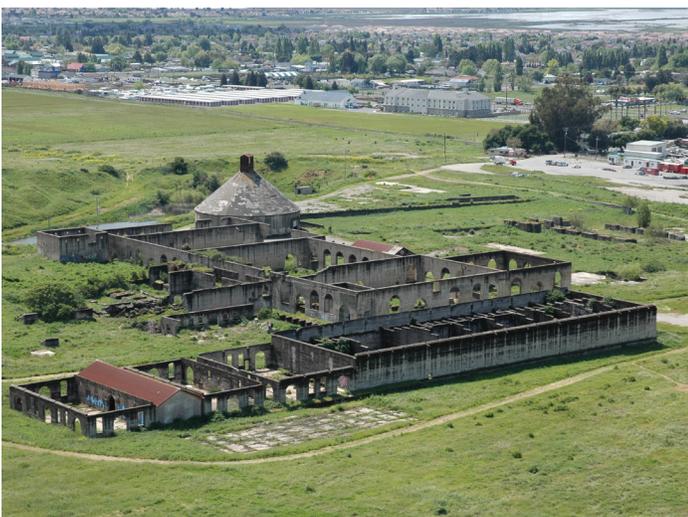
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CHAPTER 3 - VISION AND DESIGN STRATEGY

3.1 Vision

The Watson Ranch Specific Plan (WRSP) promotes the development of a vibrant, progressive, and fully integrated community; one that will be recognized as a memorable place to live, work, and play for present and future generations. The overall plan will include a series of neighborhoods that integrate housing, recreation, retail, and commercial opportunities. The WRSP Area will be a vibrant, memorable place where civic, destination shopping, dining, hospitality, employment, residential, and recreational uses converge. This opportunity provides a community gathering space within the City of American Canyon serving visitors and residents while preserving American Canyon's small town feel and character. The execution of this character is governed by the Development Regulations in Chapter 6 and influenced by the Design Guidelines in Appendix A of the WRSP.

Key components to the success of this urban fabric include community threads such as pedestrian and bicycle friendly streets, open space connections, and vistas to important community monuments, such as the Napa Valley Ruins & Gardens (NVRG). These urban design features link neighborhoods into a cohesive community yet allow them to have individual character. These linkages and their focal points establish the framework and character of the new community. In addition to this community structure, strong and simple distinctive architecture and landscape themes will help to establish a unique identity.



Standard Portland Cement Company ruins, present day



Community event opportunities

3.2 Community Framework

Primary access points into the WRSP Area will be from Newell Drive, Rolling Hills Drive, Summerwood Drive, and Rio Del Mar East, the latter of which will provide a direct link from Highway 29 to the Newell Drive extension. Rio Del Mar East extends into the WRSP Area in an east-west alignment along the north side of the NVRG and ultimately connects to Newell Drive on the east. It provides access to various activities and establishments within the NVRG as well as residential neighborhoods to the north.

Connectivity is a key component to ensuring the WRSP Area is an integrated, walkable community that accommodates a variety of uses. Pedestrian-oriented streets form a modified grid system where neighborhoods flow together and are



Activated street scene as could occur at the NVRG



Organized community events



Casual community gathering spaces



Homes activate the streetscape



Event at the ruins

not developed in tracts separated by walls. The modified grid also allows the neighborhood pattern to work with the existing topography.

The majority of the residential neighborhoods lie to the north of Rio Del Mar East and are accessed from Rio Del Mar East and Newell Drive via a minor collector road (Marcus Road). This road includes on-street striped bike lanes and six-foot wide sidewalks that encourage walking and cycling within the community to trails, parks, and the NVRG.

To further strengthen community connectivity, two regional trails, the River to Ridge Trail and the Napa Valley Vine Trail are linked to a series of internal trails that loop through the WRSP Area. Parks are strategically located along the primary trail linkages with one major park creating a larger combined central open space feature. A detailed description of the parks and trails is provided in Chapter 5.

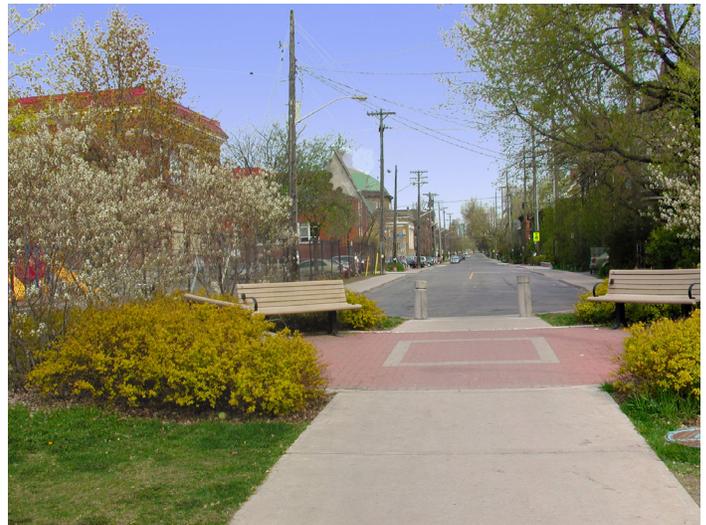
The Napa Valley Vine Trail is a major north/south pedestrian and bicycle amenity forming the backbone of the community. In order to facilitate an optimum trail experience, road crossings are minimized to only two road crossings. Pedestrian and bicycle connectivity to the adjacent neighborhoods is seamlessly integrated, while east/west vehicular connectivity is minimized. To promote pedestrian and vehicular access to the Napa Valley Vine Trail, a variety of design techniques are used, such as single loaded streets, integration of the park and trail systems, and an extensive network of secondary trails and sidewalks that connect the neighborhoods with the regional trail. Section A.3.1 in Appendix A addresses how roads and homes interact with this trail corridor.

A palette of coordinated streetscape furnishings will enhance the WRSP's urban areas. Furnishings such as bike racks, drinking fountains, newspaper racks, trash receptacles, and benches are envisioned to be provided at appropriate locations.

The NVRG will function as a mixed-use Town Center and will be the focal point of the WRSP community where uses such as festivals, wine tasting, brew pubs, distillery, dining opportunities, hospitality, and retail all converge within the central organizing element of the NVRG structures. It is anticipated that this area will develop organically over time so the standards and guidelines in this document, while being directive, are intended to allow flexibility to encourage creative uses and design solutions in the adaptive reuse of the existing NVRG structures.



Coordinated streetscape furnishings



Appropriately located street furnishings; trail connections

3.3 Napa Valley Ruins & Gardens Vision

A unique feature of this site is the physical remnants of the property’s former industrial history. The surviving concrete walls and partial structures from the late 1800s operations are commonly referred to as “the ruins.” These ruins form an important and highly visible feature within the WRSP Area. These building remnants, which have a unique mass and scale, will play an important role in establishing the WRSP Area as a memorable place and important community feature of the City of American Canyon. See Figure 3.1 for the location of the existing structures.

The vision for the NVRG is to ensure the return of the site to a productive use for the enjoyment of future generations as a public and private multi-use area while maintaining the significance of the historic resource and integrating the historic essence throughout the WRSP Area by reusing



Lights add ambiance to gathering spaces



Historic photo of operational industrial use



Existing structures today



Existing structures today



Figure 3.1: Existing Structures

some building elements such as concrete footing piers creatively on site as markers, signs or landscape elements.

With the uses intended for the NVRG, it is important to understand how the existing structures will be repurposed. This overview outlines that intent. NVRG

As structures with local historical interest, the ruins of the Standard Portland Cement Company plant, which were built between 1898 and the 1950s (including the round building & silos), are considered important to the City of American Canyon. The site includes two buildings which still have partial roofs, numerous concrete monuments, and a quarry. The site has an informal rustic and industrial yet rural character. The existing structures occupy a knoll on the east side of the City of American Canyon. The site has views of open space oak savannah and hills to the east; the City of American Canyon to the south; the City of American Canyon, Napa River, wetlands, Marin County, and Mt. Tamalpais to the west; and American Canyon, vineyards, and greenbelt to the north.

Where commercially feasible, many of the existing structures on the site will be retained for reuse. Given the property's advanced state of deterioration, a mix of preservation approaches will be used, including rehabilitation, alteration, addition, selective demolition, stabilization, and new alternative uses.

The site includes several buildings with partial roofs, numerous concrete monuments, and a quarry. The warehouses, rotary kilns, manufacturing buildings, machine shop, laboratory, and cooperage are envisioned to be stabilized and repurposed preserving the character defining arches. The silos may or may not be demolished depending on economic and market factors.

3.3.1 ENVISIONED FEATURES AND USES OF THE NAPA VALLEY RUINS & GARDENS

Uses within the NVRG are envisioned to be integrated among the ruins with a blend of new structures, semi-improved ruins with and without roofs, and dedicated outdoor spaces. The envisioned uses include small-scale convenience or grocery, retail shops, micro-brew pub, distillery, wine tasting and picnic venues, amphitheater(s), restaurants and mobile food venues, hospitality, wedding event center, corporate and private event center, an area for community event activities such as farmers markets, art shows, and/or seasonal festivals, as well as other consistent uses. In addition to traditional café, restaurant, and take-out uses, a gourmet food truck parking court is proposed. Other uses such as a public market that focuses on local food and products and community gardens are intended to celebrate the agricultural heritage of the Napa region and bring food production directly into the site. Live/work and/or mixed-use residential units are also proposed to complement the surrounding commercial and hospitality uses.

The layout of the NVRG takes its cues from the existing main ruins structures. These structures create a series of "rooms" or cloisters that are distinct yet connected. While the intent is described here, the WRSP provides for flexibility of the commercial uses and locations that is needed in order to respond to market opportunities. The NVRG is intended to be a private commercial and retail facility that has the feel of public open space and serves as a central community facility offering access for both public and private events. The NVRG will be privately maintained and operated with agreement for public use defined through separate agreement with the City (see Implementation Chapter 9).

The following are some examples of potential types of uses and amenities envisioned for the NVRG.

- **Chapel / Wedding Area.** Amenities such as a small structure and gardens may be used for weddings and other similar private gatherings.
- **Main Ruins Structure.** The main structure is envisioned to contain spaces, gardens and plazas for various events such as receptions, business venues and other private gatherings. While it is not envisioned that the main ruins structures would have roofs added to them, they may incorporate permanent or temporary tensile structures or other free-standing means to provide shelter from the sun or rain.

- **Restaurants.** The NVRG is an ideal location for one or more restaurants, providing a synergy with adjacent/ nearby social uses.
- **Café.** The NVRG is well suited for a café or similar use that encourages people to spend time socializing and enjoying the NVRG.



Ruins structures today



Potential use for weddings and events



Ruins walls form enclosed spaces or “cloisters” in the main ruins area

- **Winery / Wine Tasting Area.** The NVRG is envisioned to house a use such as a winery and/or wine tasting venue. This is an important use that could serve somewhat as an anchor and will help establish the overall atmosphere and vibrancy of the NVRG.
- **Farmer’s Market.** A farmer’s market, craft festival, or other similar event that involves a village of individually tented vendors or exhibitors would be a popular attraction to the NVRG.
- **Picnic Area.** A suitable open area can provide passive uses such as picnicking, enjoying a glass of wine and food, and people watching. This area may include elements such as permanent chess/checker tables, benches and seat walls, bocce courts, or a fire pit. This area can also be utilized for additional space for the various festivals or events the NVRG will host.
- **Sculpture Garden.** Artisan beverages, food, and gathering venues are fundamental to the creation of a sense of place at the NVRG. A strong tie to the arts is a natural fit with this character. Space could be available to host exhibits from local artists, regional museums, private collections from wineries in Napa Valley and other sources. This will be a unique draw for American Canyon as nothing similar exists in the region.



The Round Building at NVRG



Activities at NVRG



Activities at NVRG



Sculpture Garden

- Mobile Food Truck Venue.** Mobile food trucks provide high quality, freshly prepared food and beverages and are untethered to a static location. In most cases they simply arrive at a destination such as a festival or park wherever they can because they are self-contained and are open for business. In response to this dining industry movement, an area for food trucks could be provided. This area may be supplied with power for the trucks, eliminating their need to use generators. While the location and amenity may end up being fixed to a particular area, the individual trucks will vary, thus allowing for variety and change in the offering and a dynamic food experience. Additionally, this area may serve as overflow or flex space when trucks are not present.
- Children’s Play Area.** Play structures will contain play experiences that encourage discovery of the history of American Canyon, the WRSP Area and the Basalt Plant/Portland Cement Factory ruins.
- Parking.** The manner in which parking is provided for the various uses in the NVRG is critical to creating a project that is both economically successful and a truly pedestrian oriented place. Parking will be provided on Rio Del Mar East as parallel or angled parking and in parking lots that support the various uses. Parking areas will be distributed throughout the NVRG to allow for proximity and ease of access and to reduce their scale. Parking for larger events may be provided in vineyard or orchard lots that when not in use, simply support the landscape character of the area.
- Hospitality / Hotel / Hotel Parking.** This use is proposed to be located on the north facing slope of the hill in the eastern side of the NVRG. This location provides commanding views and allows for the opportunity to provide adequate parking, and a bold landscape statement. The site terrain will be sensitively graded to provide a road to the hilltop site. The Hotel will be a “boutique hotel” of approximately 200 rooms, additional casitas, and supporting amenities. There will also be private residences located near the hotel on the south side. The use is intended to be complementary to and work in conjunction with the wedding and event center use in the NVRG.
- Overflow/Event Parking.** Large community events and private events will take place at the NVRG. As these will be scheduled events and not daily activities, the parking needed to accommodate the influx of people can be provided in the orchard. When planted, the orchard could be laid out to allow appropriate spacing and surfacing (crushed rock or other appropriate multiuse surface). This would create an area that is functionally



Potential area for outdoor performances



Mobile culinary trucks



Example gathering space

suitable for parking, yet when not being utilized, is simply an element of a strong landscape statement. Parking needs for very large events above and beyond what the on-street and on-site parking can accommodate could be provided through special arrangement at American Canyon High School and accessed via shuttle.

- **Additional Parking.** To minimize the visual impacts of parking lots, parking is distributed throughout the NVRG to allow for proximity and ease of access, including parking near the hotel.
- **Brewery / Brew Pub / Distillery.** Another use that is important to support the vibrancy of the NVRG is the inclusion of a brewery/ brew pub, and/or craft distillery with associated tasting rooms.
- **Retail/Commercial.** Areas in the NVRG could be envisioned as a retail, commercial, or similar space.
- **Outdoor Performance Venue / Amphitheater.** Adjacent to the east and south sides of the main ruins structures are two areas that due to its topography could be a natural place for an outdoor stage and performance venue such as an amphitheater. This performance venues would be conveniently located next to parking, as well as the overflow orchard parking.
- **Community Gardens.** With the importance of agriculture regionally and food being an important element of the NVRG uses, food production is also an important component. Community gardens (“urban agriculture”) can play an important role in the education and participation in the production of healthy food. Gardens that serve individual restaurants may also be included.



Hotel Example

Amphitheater(s)



Brewery Example

amphitheater(s)



Example children's play area



Example community gardens

- **Charcuterie.** At the southwest corner of the group of ruins structures is a small concrete structure. This structure and location may be appropriate for artisan food uses such as a charcuterie.
- **Festival Space.** The space between the Quarry Lake Park and Community Gardens could provide a gathering space, where local festivals and community events could occur.
- **Quarry Lake Park.** The former quarry lake and its surroundings can provide a unique park and recreational experience. This park facility is proposed to be privately maintained and operated, yet accessible as a public amenity. Access to the water for recreational purposes will be restricted. On the west, south, and east sides of the lake will be a trail and viewing places. Picnic opportunities will be provided, predominantly at the east end where a natural bowl occurs. Specific programming of recreational facilities in public parks will be coordinated with the City Parks and Recreation Department. The trail will connect to the NVRG and the residential neighborhoods to the south and north.
- **Artisan Building.** The “Maker” movement is gaining momentum across the Bay Area and the country. Appreciation for hand crafted items of all types is on the rise. Providing a place for artisans (“makers”) to work their craft or art is consistent with the concept of the adaptive reuse of the ruins. It may also house a market similar to the Oxbow Market in Napa that establishes a place for local crafts people and artists to sell their wares, or to enjoy local food from those who produce it.

- **Live/Work Building.** There could be an opportunity for a future live/work building located in the NVRG.
- **Mixed Use Building.** There could be an opportunity for a future mixed-use building to house residential above restaurants, retail, or a variety of other uses that complement and add vibrancy to the NVRG.
- **Potential Bus Stop / Shelter.** Vine Transit has two bus routes through the City of American Canyon. For more on transit and public services refer to Chapters 7 and 8 of the WRSP. Should a bus route be established along Rio Del Mar East, a stop at the NVRG should be considered along the Rio Del Mar East frontage. Specific siting of a stop and shelter will be coordinated with the transportation agency at tentative map stage (see Implementation Chapter 9).



Existing graffiti art at NVRG



Existing Quarry Lake



Existing graffiti art at NVRG



Artistic, unique signage



Artistic, unique signage

- **River to Ridge Trail Connection.** The River to Ridge Trail is proposed to be incorporated as a multi modal trail along the NVRG frontage at Rio Del Mar East, connecting across the WRSP Area to the Newell Open Space on the east.
- **Vineyards.** Around the edges of NVRG adjacent to Rio Del Mar East and along the southern edge along the Quarry Lake Park vineyards may be planted to act as a buffer from the road and will also provide a visual amenity that is coherent with the character of the area.

3.3.2 WALKABILITY

The various spaces and uses described above will be interconnected by a network of wide walks and plazas. The entire NVRG is envisioned to have a European feel, where a devotion to quality of life is evident and the rich history of the site is celebrated. It is a place where safety and service vehicles have access, but the area is a pedestrian dominated realm. The widest walks can function as promenades where



Existing community art



Landscape furnishings as community art

removable bollards allow vehicular access to different areas as needed. For example, vehicular access for drop-off could be provided at the wedding chapel. When not being used for vehicular access, the space simply feels like part of the gardens, not an asphalt street or parking area.

3.3.3 COMMUNITY ART & SIGNAGE

Given the unique identity established by the NVRG, community art can have a complementary role in energizing and defining the plaza and surrounding spaces. Community art may take the form of tile mosaics on the buildings, interactive fountains, sculpture, custom street furnishings such as benches, trash receptacles, and manhole covers, or seasonal items such as a playful and unique banner program. It may also include preservation of particularly creative graffiti art on the ruins themselves, or wall space dedicated to shows of street art.

Signage guidelines included in Appendix A, Design Guidelines, Section A.5.5 of the WRSP are intended to guide merchants towards unique and individual signage as this is critical in supporting this unique character.

3.3.4 LANDSCAPE CHARACTER

Supporting the unique architectural environment of the NVRG will be a strong landscape design concept. Plantings will be composed of drought-tolerant species. Landscaping of this area will draw heavily from classic images and include lavender, roses, grasses and flowering vines. Trees such as olives, in groves or strong linear patterns may be used to further reinforce the wine country character. Rows of vineyards create a strong geometric statement and support the wine country aesthetic. Consistent with the direction of the City's Steering Committee, the landscape and public spaces should celebrate the history of the place through thoughtful design and interpretive signage.



Vertical landscaping

The planting palette and design will be robust to match the strength of the existing ruins structures. Blending structure and gardens, the massive walls of the ruins could support vertical gardens that create soft yet geometric planes of plantings. The opportunity for vertical landscaping (or “green walls”) builds on the unique identity of the place. See Appendix A, Design Guidelines, Section A.5.3 for specific guidelines.

3.4 Neighborhood Design Concept

Residential neighborhoods within the WRSP Area will provide variety in terms of individual character. NVRG. Moving north away from the NVRG, lots may increase in size resulting in a transition to lower density moving away from the NVRG.

Neighborhoods are not walled off from each other and pedestrian connectivity between neighborhoods, amenities, and regional trails is provided. A hierarchy of streets is defined in Chapter 7 (Circulation), which shows where they occur in the community and if they provide through connections or simply serve a neighborhood of homes. The street system is based on the concept of “complete streets”. Streets are strategically located to provide the greatest access to parks and regional trails. Open-ended cul-de-sacs will be utilized extensively to minimize the conflict for vehicles and pedestrians along major trail routes and to provide both neighborhood traffic calming and visible pedestrian access to amenities.

3.5 Residential Design Vision

The WRSP provides for a range of residential densities with the intent of creating a dynamically diverse and integrated community of residential neighborhoods. Residential densities will generally be more intense in and around the NVRG and transition to lower densities moving northward from there. This concept is in keeping with the City General Plan Policy for the Town Center designation. Concentrating density directly adjacent to the NVRG improves compatibility of adjacent uses and supports activity within this mixed-use area. Transitioning to lower densities in neighborhoods farther away from the NVRG responds to the increasingly rolling terrain to the north and east.

Drawing on classic neighborhoods of the early twentieth century, a wide range of housing types is envisioned in the interconnected neighborhoods. The range of housing types provided in the WRSP includes town homes, live/work

units, apartments, row town homes, duet units, cottages, paseo oriented homes, small and standard lot homes. This diversity of housing types will both respond to changing lifestyles and preferences in housing as well as offer a wide variety of price levels.

By including options for homes, home buyers will have the ability to choose a home that is affordable by design. Empty nesters may choose the maintenance-free living offered by a townhome. Small business owners may choose the dynamic lifestyle of live-work home in the NVRG, and families may choose a traditional home with great pedestrian and bicycle connections to trails and parks.

While diverse in product type and architectural character, the different neighborhoods will be woven together by the consistent design of the public realm (i.e., streets, trails and parks), carefully avoiding the segmenting of separate neighborhood enclaves. Architectural styles are envisioned to provide a wine country flavor to the neighborhoods, without creating a community that is too heavily themed. The architectural palette is a contemporary interpretation



Agrarian inspired style



Rustic, wine country architectural style



Craftsman architectural style



Simple forms inspired by the agrarian structures

of Craftsman and American styles. These styles will be reinforced with a carefully selected landscape palette that creates a memorable community with a strong sense of place compatible with the small-town character of the City of American Canyon. Refer to Appendix A, Design Guidelines.

3.6 Parks, Recreation, Open Space, and Trails Vision

The parks and open spaces for the WRSP Area form an integrated system with a wide variety of options for people to enjoy. In an urban neighborhood, this system is intended to provide more variety than a typical suburban park that is typically dominated by organized play fields. Park components within the WRSP Area include an active park in the central WRSP area abutting the Napa Valley Vine Trail, small private pocket/mini parks, and a park on the Newell Property – called Newell Park – bordered by Newell Drive to the east and Rio Del Mar East to the north. In the southern portion of the WRSP Area is a park surrounding Quarry Lake with passive recreational opportunities. Additionally, there are natural drainage areas and basins that are part of the overall WRSP Area drainage system that will include trails that are integrated into the trail network.

An important objective of the WRSP, consistent with the General Plan goal of creating a Town Center, is to provide a place that is available for events sponsored or scheduled by the City. The location for this community gathering space is to the east of the NVRG ruins at the intersection of Rio Del Mar East and Newell Drive. A future community center building will look out onto a community plaza. The

community center and plaza are integrated with Newell Park (see Chapter 5 for further discussion).

Many of these recreational amenities can be linked together by a series of looping pedestrian trails and bikeways, providing access to the immediate neighborhoods as well as connections to the regional amenities of the Napa Valley Vine Trail and the River to Ridge Trail.

The vision for recreational amenities with the WRSP Area also includes the development of small, privately owned and maintained pocket or mini parks within the neighborhoods. These parks may be as small as a single lot and be passive in nature but provide important places of urban relief within their context.

Community gardens in the NVRG will build and support a sense of community around local food sources and stimulate events in support of health and sustainability. The community gardens may also be available for partnership with food and



Community gathering area



Large active play areas



Pedestrian and bike paths

farm-based education programs for community members and for interactive educational opportunities for youth.

The community gardens can be located near multiuse paths to allow the pedestrian and bicyclist to view the “farm” from the path, making the gardens part of the daily pedestrian/ bicyclist’s experience and providing ways for residents to access them without using a motor vehicle.

The WRSP Area provides critical links in two regional trail systems. First, the River to Ridge Trail connects from Highway 29 on the west, along Rio Del Mar East through the Plan area, to the extension of Newell Drive and the Newell Open Space beyond, providing a public connection to this open space on the east side of town. Second, the Napa Valley Vine Trail will run from north to south the length of the WRSP Area. The alignment of the Napa Valley Vine Trail correlates to an existing PG&E gas line easement that runs through the WRSP Area. These two regional trails converge at Rio Del Mar East adjacent to the NVRG, providing a great example of regional pedestrian connectivity.

Additional detail on the vision for open space, parks, and trails can be found in Chapter 5 (Parks and Open Space).



Play area



Informal gathering areas



Dog park



Community gardens

CHAPTER 4 - LAND USE

4.1 Introduction

The proposed Land Use Plan for the Watson Ranch Specific Plan (WRSP) implements the City of American Canyon's General Plan land use designation of "Town Center" (see Figure 4.1), which calls for a variety of residential densities and mix of land uses. The organization of land uses responds both to the physical features of the site and the policy framework of the General Plan.

The formation of the Land Use Plan (Figure 4.2) was a response to four key factors: (1) the policies set forth in the American Canyon General Plan, (2) the incorporation of the existing Napa Valley Ruins & Gardens (NVRG) structures and quarry lake, (3) the provision of higher residential densities around the NVRG, transitioning to lower densities, and (4) the site constraints such as the PG&E gas easement and wetlands.

Any lawfully existing land use occurring at the adoption date of the WRSP may be continued, notwithstanding any omission of a particular use in the Permitted Uses Section.

4.2 Land Use Goals and Objectives

The following goals, objectives, and policies have been prepared to establish the implementation framework for land use oriented Planning Principles found in Chapter 1.

Goal 4A – Provide an integrated and diverse mix of land uses.

Objective 4.1 – Provide integrated neighborhoods that contain both single-family and multifamily residential.

Policy 4.1.1 – Residential neighborhoods shall provide a broad range of housing types, including detached single family, attached single family, townhouses, condominiums, and apartments.

Policy 4.1.2 – The highest residential density shall be located around the NVRG with a transition to lower density in neighborhoods further away.

Policy 4.1.3 - Within the MDR-16 and/or the HDR residential categories, a diversity in the mix of residential housing types shall be achieved by including both townhome and condominium housing unit types to be constructed as determined by the market. Evaluation of the product mix will occur on a phase by phase basis to ensure adequately integrated neighborhoods with a variety of housing types.

Objective 4.2 – Create a pattern of land use development that respects the environmental setting of the WRSP Area.

Policy 4.2.1 – The environmental resources of the City shall be protected including significant wildlife habitats and vegetation, hillsides and canyons, creeks, rivers, and wetlands.

Policy 4.2.2 – For the portion of the WRSP located within Airport Overflight Zone E, aviation easements shall be required. Prior to development or with the recording of Final Maps in any given phase, whichever occurs first, aviation easements shall be recorded on all existing and proposed parcels located within the given phase.

Objective 4.3 – Provide for recreational, institutional, commercial and service uses that support resident needs within or in close proximity to residential neighborhoods.

Policy 4.3.1– Neighborhood serving retail and commercial use shall be located in the NVRG.

Policy 4.3.2 – Parks, trails and open space amenities shall be interconnected as an open space system and integrated into the fabric of the residential neighborhoods.

Goal 4B – Establish the WRSP Area as a traditional neighborhood development.

Objective 4.4 – Provide diverse, high-quality residential neighborhoods with a range of densities, creating neighborhood character that appears to have developed over time in conjunction with the NVRG.

Policy 4.4.1 – Neighborhoods shall include a variety of housing types.

Objective 4.5 – Provide an open network of streets with pedestrian and bicycle connections.

Policy 4.5.1 – Unique streetscape design standards, including elements such as landscape, street furniture, signage, way-finding elements, street trees and lighting shall be set forth in the development standards and design guidelines herein.

Goal 4C – Establish a “Town Center” for the City of American Canyon.

Objective 4.6 – Provide a diversity of retail, entertainment and service commercial uses that are oriented to the needs of both residents and visitors and positively contributes to the economic vitality of American Canyon.

Policy 4.6.1 – A range of permitted uses shall be established for the NVRG (Mixed Use Land Use Category) such as an events center, wine and wine industry-related businesses (e.g., brewery, distillery), restaurants, outdoor dining, retail, commercial, hotel and visitor serving uses, etc.

Objective 4.7 – Provide public gathering places within the NVRG.

Policy 4.7.1 – The NVRG shall include opportunities for public and quasi-public community gathering spaces such as a community plaza, event lawns, and promenades.

Objective 4.8 – Establish the NVRG as an important destination within the City of American Canyon.

Policy 4.8.1 – The ruins of the Standard Portland Cement Company (NVRG) site shall be rehabilitated as a functioning public and private multiuse complex with new businesses, gardens, performance spaces, and emerging artist spaces, while retaining the open rural setting.

Policy 4.8.2 – The historic character and significance of the NVRG ruins shall be retained as part of the adaptive reuse of the site.

Objective 4.9 – Ensure that commercial development within the NVRG (Mixed Use Category) be designed to exhibit a high quality of architectural character that reflects its historical and cultural context.

Policy 4.9.1 – Specific design standards and guidelines for architecture in the NVRG (Mixed Use Category) shall be provided in the WRSP.

4.3 General Plan Land Use Designation

The City of American Canyon General Plan Land Use Element was amended in June 2010 to assign a single Town Center (TC) Land Use Designation for the entire subject property. In coordination with adoption of the WRSP, the Land Use Element was amended in October 2018 to incorporate the WRSP Land Use Map in further defining specific land uses designated by the WRSP (Figure 4.1). A further General Plan Amendment is required in concurrence with the adoption of this amended WRSP, as summarized in the Executive Summary in this document.

4.3.1 GENERAL PLAN LAND USE DESIGNATION DEFINED

The WRSP Area, as defined by the City of American Canyon’s General Plan Section 1.19.2, is designated Town Center (TC):

“Town Center (TC):

A) A rich diversity of land uses which may include government and community services, retail commercial, professional offices, entertainment, restaurants, cultural facilities (museums, libraries, etc.), visitor-serving facilities (hotels, information centers), event center/conference center, wineries, transit, parking, variety of housing types including single family attached and detached, townhouses, condominiums, mixed-use and apartments, and public park and other amenities.

B) A community plaza or “town square” to facilitate community gatherings and events.”

4.3.2 GENERAL PLAN LAND USE MAP AMENDMENT

The Town Center designation requires the preparation and adoption of a specific plan(s) to implement the General Plan land use. As such, the WRSP includes a General Plan Amendment (see Executive Summary) that, among other things, amends the General Plan land use map to reflect the revised specific land uses set forth in the WRSP Land Use Map (Figure 4.2). This supersedes an amendment to the General Plan made in 2018. For purposes of the General Plan, these land uses are designated Town Center with the prefix TC-1 (see Figure 4.1), reflecting the first specific plan to be adopted under the Town Center Designation.

4.4 Specific Plan Land Use Summary

Table 4.1a and Table 4.1b summarize the various land uses proposed within the WRSP Area. This summary allows for a comparative analysis of the development and corresponds to the Land Use Plan (Figure 4.2). The amount of development reflected in the “Overall Development Program” and by individual ownership, represents the maximum allowable development intensity permitted under the WRSP.

Overall Development Program		
Watson Ranch Specific Plan		
Land Use	Area (+/- Acres)	Maximum Development Intensity
Medium Density Residential (MDR-12)	102.5	623 units
Medium Density Residential (MDR-16)	65.4	630 units
High Density Residential	6.0	186 units (Affordable)
Mixed Use	38.6	200 room hotel 176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial
Parks and Open Space	63.6	
Roads	32.6	
Total	308.7	1,439 units 200 room hotel 176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial

Table 4.1a: Overall Land Use Summary Table

American Canyon I, LLC Property		
Land Use	Area (+/- Acres)	Anticipate Development Intensity
Medium Density Residential (MDR-12)	86.5	503 units
Medium Density Residential (MDR-16)	53.3	507 units
High Density Residential	6.0	186 units (Affordable)
Mixed Use	38.6	200 room hotel 176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial
Parks	18.0	
Open Space and Trails	14.9	
Detention	9.4	
Wetlands	3.1	
Roads	22.0	
Total	251.8	1,196 units 200 room hotel 176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial
Newell Family Property		
Land Use	Area (+/- Acres)	Anticipate Development Intensity
Medium Density Residential (MDR-12)	16.0	120 units
Medium Density Residential (MDR-16)	12.1	123 units
Parks	7.4	(Includes 2.0 acre Community Center)
Open Space and Trails	7.5	
Detention	3.3	
Roads	10.6	
Total	56.9	243 units

Table 4.1b: Land Use Summary Table by Ownership

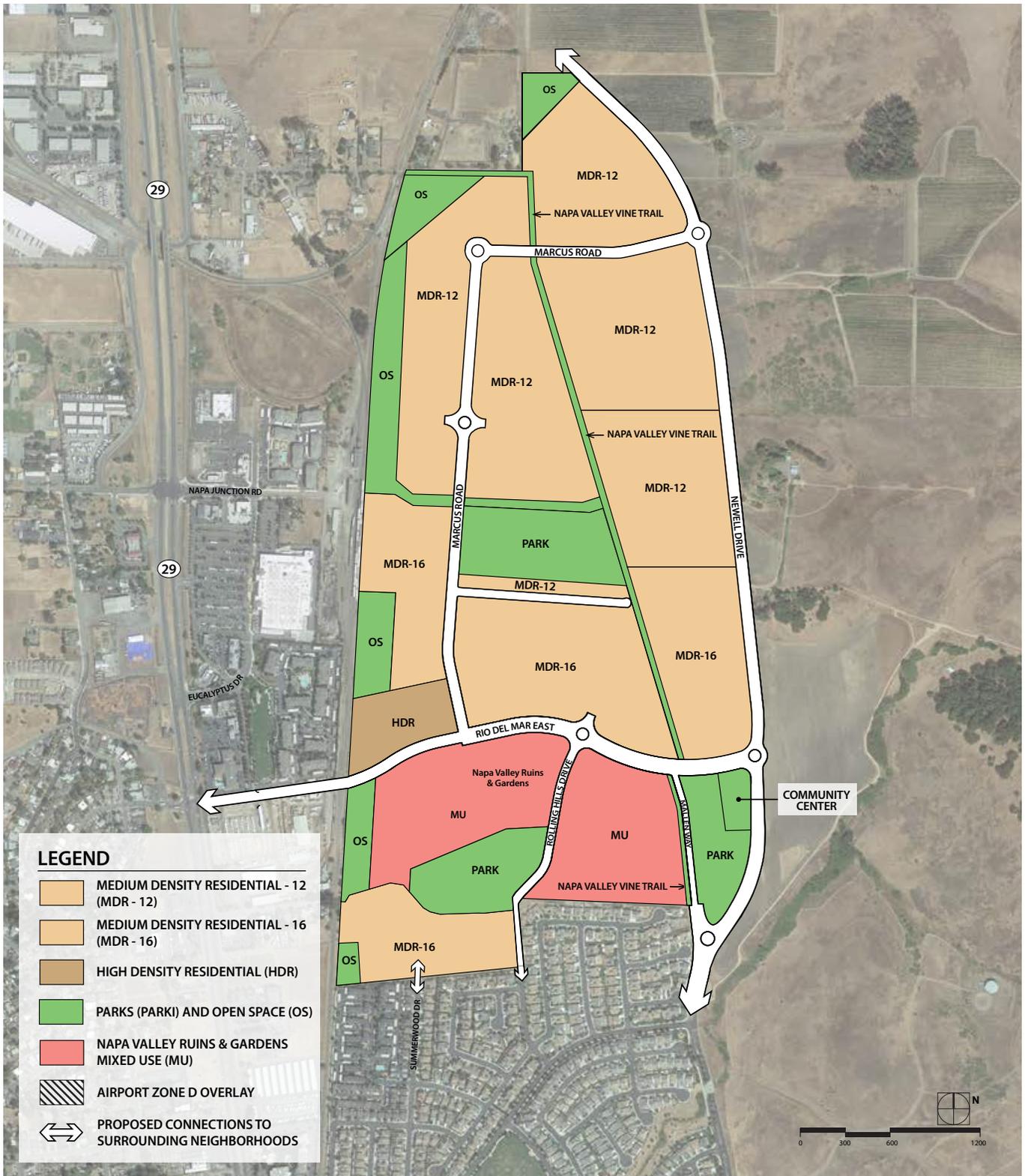


Figure 4.2: Specific Plan Land Use Map

4.4.1 SPECIFIC PLAN LAND USE DESIGNATIONS

The WRSP establishes a land use and regulatory framework within the parameters of the General Plan that allows a maximum of 1,253 residential units (plus additional units allowed by density bonus and related laws), 200,000 square feet of commercial/retail uses, and a 200-room hotel in the WRSP Area. Figure 4.2 identifies the location of the land uses within the WRSP Area.

Below, each Land Use Category and the intended uses are described in more detail. These land use designations are consistent with the overall General Plan Land Use designation of “Town Center” and serve to refine the land use configuration for the WRSP Area. As described in Section 4.3.2 above, a General Plan Amendment to further define the specific land uses set forth by this specific plan under the Town Center Land Use Designation and supporting policies, amends the General Plan Land Use Map to incorporate the land uses with the prefix TC-1 thereby denoting the WRSP land uses (see Figure 4.1). This amendment recognizes and ensures the consistency between the City of American Canyon’s General Plan and the WRSP, as required by controlling law.

These land use designations also serve as the “zoning” for the WRSP in reference to the allowable uses described further in Section 4.6, the Development Regulations set forth in Chapter 6, and the Design Guidelines in Appendix A. Where the WRSP and the City zoning regulations may conflict, the WRSP standards will apply. Where the WRSP is silent, the City Zoning regulations will apply.

RESIDENTIAL LAND USE DESIGNATIONS

The City’s General Plan requires that the Specific Plan(s) for the Town Center land use designation establish the densities and intensities of land uses. For the residential land uses, the WRSP sets forth three residential categories with varying density ranges - Medium Density Residential (MDR-12 and MDR-16) and High Density Residential. The density ranges for the two Medium Density categories overlap slightly with the intention of encouraging a higher diversity of housing product types within single neighborhoods, allowing greater flexibility to respond to market forces and creating an opportunity to achieve what is referred to as the “Missing Middle” of housing types. The “Missing Middle” is a range of multi-unit or clustered housing types compatible in scale with single-family homes that help to achieve workforce housing and promote vibrant walkable residential neighborhoods. At the same time, the densities transition from higher to lower away from the NVRG consistent with the General Plan Policies.

Medium Density Residential (MDR-12): This Medium Density Residential is intended for single-family homes

with a permitted density between 2 to 12 dwelling units per gross developable acre.

Medium Density Residential (MDR-16): This Medium Density Residential is intended for areas of medium density detached and attached housing such as small lot single family homes, duets, duplexes, three-plex, four-plex, townhome and condominium units. The allowable density range is 8 to 16 dwelling units per gross developable acre.

High Density Residential (HDR): High Density Residential is intended for areas of high-density attached housing such as, townhomes, apartments, and condominiums. Per the General Plan, this residential category density shall be provided at a minimum of 20 dwelling units per gross developable acre.

NON-RESIDENTIAL LAND USE DESIGNATIONS

Napa Valley Ruins & Gardens (MU): The MU designation includes the former site of the Standard Portland Cement Factory. It comprises approximately 39 acres. The MU designation is a mixed-use designation made up of several different uses including but not limited to commercial uses (such as retail, restaurants, personal services, dining, wineries, breweries, pubs, distillery, mobile food trucks, and farmers markets), mixed-use residential (including live/work), and office uses. It also includes visitor serving uses such as hotel, events center, entertainment, and recreation. Finally, it permits civic uses such as a community plaza, community center and parks. Development within this area may be comprised of single use buildings or buildings containing multiple uses including mixed use and/or live-work housing of up to 10% of total units (included in the residential project total).

Parks (Park) and Open Space (OS): The Park designation allows for a variety of recreational uses such as the lake, multipurpose trails, tot lots, playground equipment, passive and active recreational amenities such as ball fields, courts, skating ramps, and picnic facilities.

The Open Space (OS) designation includes privately or publicly owned property to be retained for open space purposes such as resource management, storm water management and environmental mitigation. Open space uses may also include passive recreation (such as hiking, walking and biking trails, and nature observation). Passive recreational uses associated with open space areas while encouraged are not eligible for parkland credit.

The Parks and Open Space land uses may also contain civic or community uses such as community gardens, vineyards, orchards, and other edible landscape elements.

Town Center (TC): The Town Center designation allows for community uses such as a community plaza, and community center.

4.5 Adjustment/Transfer/Conversion Regulations

In order to create the most desirable community possible, there is a certain amount of flexibility that needs to be provided for in the WRSP. This flexibility allows the developer to adjust for shifts in market demand. The parameters for this flexibility within the WRSP are detailed in this section. Requests for these adjustments shall be submitted to the City of American Canyon. The Community Development Director or his/her delegate shall make the determination as to their consistency with the intent of the WRSP. If the adjustment meets the definition of minor amendment (see Implementation Chapter), the Community Development

Director may approve the adjustments at the administrative level. Major amendments to the WRSP must be processed pursuant to State Law and will be subject to review and approval by the Planning Commission and City Council.

4.5.1 PLANNING AREA ADJUSTMENTS

While a Land Use Plan and Land Use Summary Table are contained in the WRSP, precise land use boundaries and acreages shall be established by the recording of Final Maps and with final engineering. A Tentative Map submittal may incorporate an adjustment to the boundaries and acreages on file with the City for minor amendments to the WRSP. An amendment will be considered minor if the total gross acreage of any given land use designation does not change by more than 5 acres or a 20% increase or decrease

Use Classification	Specific Plan Land Use Category					
	MU (NVRG)	MDR-12	MDR-16	HDR	PARK & OS	TC
COMMERCIAL						
Retail stores, shops, galleries and offices supplying commodities or performing services such as those provided by department stores, specialty shops, personal, pet and business service establishments, artists' supply stores and similar uses.	P	-	-	-	-	-
Convenience retail commercial such as a corner convenience store, grocery store and other neighborhood serving retail commercial uses.	P	C	-	-	-	-
Restaurants uses including outdoor eating areas and establishments. For the purpose of this use, a restaurant is an eating establishment which serves food to customers for consumption on or off the premises. It includes, but is not limited to, coffee shops, cafes, brew pub, pizza parlors, soda fountains and full-service dining establishments	P	-	-	-	-	-
Restaurants and pubs that may provide alcoholic beverage sales for drinking on premises.	P	-	-	-	-	-
Distilleries, wineries, Brewery establishments that may or may not include food service and may provide alcoholic beverage sales for consumption off	P	-	-	-	-	-
Entertainment such as non-amplified music in conjunction with a restaurant, coffee house or pub establishment.	P	-	-	-	-	-
Outdoor movies.	P	-	-	-	C	-
Winery and wine tasting establishments	P	-	-	-	-	-
Mixed-use buildings that contain restaurant, retail or storefront office uses on the ground floor with residential, office or educational uses on the floors above.	P	-	-	C	-	-
Artist studios and maker space	P	-	-	-	-	-
Business and technical schools, and schools and studios such as, but not limited to photography, art, music and dance.	P	-	-	-	-	-
Hospitality and visitor serving uses such as a wedding chapel, hotel, condo hotel or time-share.	P	-	-	-	-	-
Assembly, meeting or event spaces for corporate or private events either as a standalone facility or as ancillary to another use.	P	-	-	-	-	-
Parking structures.	P	-	-	P	-	-
Home occupation uses.	P	C	C	C	-	-
Retail sales in kiosks, food trucks, passive recreation, live musical or stage performances, periodic outdoor sales such as art shows, or farmer's markets, community and holiday events.	P	-	-	-	C	C
Office, medical, financial, real estate, general business and personal services.	P	-	-	-	-	-
Small-family day care facilities.	P	-	-	-	-	-
Outdoor Entertainment including amplified music or other productions as scheduled events.	C	-	-	-	P	-
Drive-thru facilities	-	-	-	-	-	-

Table 4.2: Permitted and Conditionally Permitted Uses

Use Classification	MU (NVRG)	MDR-12	MDR-16	HDR	PARK & OS	TC
RESIDENTIAL						
Single-family residential with a minimum density in this district is between 2 and a maximum of 12 dwelling units per gross acre and a minimum lot size of 3,200 sf. For other allowable and conditional uses refer to RS-6500 zone district in the City of American Canyon Municipal Code.	-	P	P	P	-	-
Medium Density Residential is intended for areas of medium density detached and attached housing such as small lot single-family homes, duets, duplexes, three-plex, four-plex, townhome and condominium units. The allowable density range is 8 to 16 dwelling units per gross developable acre.	P	-	P	P	-	-
High-density attached housing such as townhomes, apartments and condominiums. The allowable density range is a minimum of 20 dwelling units per gross acre. For other allowable and conditional uses refer to RH zone district in the City of American Canyon Municipal Code.	P	-	-	P	-	-
Mixed Use and Live/work residential units	P	-	C	C	-	-
Secondary living units. Secondary living units do not count as a separate unit from the primary residence.	C	C	C	C	-	-
RECREATIONAL						
Active and passive public recreational uses but not limited to multipurpose trails, tot lots, playground equipment, passive and active recreational amenities including, but not limited to, ball fields, courts, skating ramps, picnic facilities, and recreational use structures. Civic or community uses including but not limited to community gardens, vineyards, orchards and other edible landscape.	P	C	C	C	P	C
Active and passive recreational uses such as play areas, community gardens, swimming pools, etc. that are provided as part of an HOA and are privately maintained.	P	P	P	P	-	-
Environmental mitigation lands, drainage and detention/retention basins, drainage system appurtenances such as culverts, wetlands, and natural drainage ways. Also permitted are pedestrian and bicycle trails, viewing areas, interpretive signage, and site furnishings such as benches, and trash receptacles.	P	P	P	P	P	P
Recreation uses such as community plazas and small pocket parks.	P	P	P	P	P	P
Community gardens.	P	C	C	C	C	C
PUBLIC						
Civic Uses	P	-	-	-	-	C
Educational and institutional uses.	P	-	-	-	-	P

Table 4.2 (continued): Permitted and Conditionally Permitted Uses

of the original gross acreage approved under the WRSP, whichever is less. A revised Land Use Plan as well as a revised Land Use Summary Table must be submitted to the City of American Canyon for each proposed amendment or set of amendments to the land use area boundaries (see Implementation Chapter, Section 9.6.3)

4.5.2 TRANSFER OF DWELLING UNITS

The transfer of dwelling units between land use areas is permitted provided that there is no net increase to the total dwelling units permitted in the WRSP. A revised Land Use Plan as well as a revised Land Use Summary Table must be submitted to City of American Canyon for each proposed transfer of dwelling units, and the phasing plan and schedule adjusted accordingly (See Implementation Chapter, Section 9.8).

4.6 Permitted and Conditionally Permitted Uses

Table 4.2 summarizes permitted and conditionally permitted uses within the WRSP Area. While the descriptions are fairly comprehensive, they are not intended to prohibit an omitted but related use. Any uses not specifically identified here shall be considered through subsequent approval processes as defined in the Implementation Chapter.

4.7 Inclusionary Housing

The City of American Canyon has an Inclusionary Housing Ordinance (Chapter 19.28 of the City's Municipal Code). The intent of the Inclusionary Housing Ordinance is to see the provision of affordable housing occur throughout the residential neighborhoods of the city. Section 19.28.040 of the ordinance requires that residential projects of "for sale" housing containing five or more parcels or units provide at least 10% of the total applicable project units at affordable prices or rents to lower income households. As of the date of this WRSP amendment, the AC-1 portion of the WRSP Area has fully complied with the City of American Canyon's Inclusionary Housing Ordinance. Compliance has been satisfied by construction of the 186-unit Lemos Pointe affordable housing project located at the intersection of Rio Del Mar East and Marcus Road.

CHAPTER 5 - PARKS AND OPEN SPACE

5.1 Introduction

The proposed parks, trails, and open space plan for the Watson Ranch Specific Plan (WRSP) includes a variety of parks, trails, and open spaces for a wide number of active and passive recreational use. The integrated network of parks, trails, and open space, both local and regional, is one of the fundamental design principles for WRSP providing the residents and the larger community recreational throughout the plan area. Approximately 63 acres of recreation facilities, park land, trails, and open space areas are provided within the WRSP Area. The parks, trails, and open space system consists of a series of inter-connected parks and trail types ranging from pocket parks to community parks with linkages to larger regional facilities such as the Napa Valley Vine Trail, River to Ridge Trail, and the Newell Open Space Preserve. Parks are programmed with a variety of uses to support activities throughout all seasons of the year. A range of park and trail typologies are distributed throughout the WRSP Area in order to provide ease of access and close proximity to all residents. The character of each park is based on the use and the role it plays within the overall open space network. They are a combination of the natural and the manicured; the informal and the formal; and the active and the passive, depending upon their location and functions.

The trails system consists of a combination of paved and unpaved trails that link major destinations within the WRSP Area, such as parks and the Napa Valley Ruins & Gardens (NVRG). The internal project trail system includes segments of regional trails (the Napa Valley Vine Trail and the River to Ridge Trail) and connects to the Newell Open Space Preserve. The network forms a series of loops of differing lengths. The loops provide alternative routes and allow choices between short, medium, and long distances for recreation and exercise. The trails run through various parks and open space conditions and their design reflects these differing conditions. The trails provide for a range of user groups including bikers, runners, and pedestrians.

The parks and open space system is designed to provide easy and convenient access to all residents. Most homes within the WRSP Area are no more than ¼-mile walking distance from a park, natural open space area, or trail alignment. This public network serves to encourage walking and to provide opportunities for a convenient and healthy lifestyle choice for all residents.

5.2 Goals, Objectives, and Policies

Goal 5A: Integrate Parks, Trails, and Open Space into a diverse system of active and passive recreation amenities designed to fulfill the needs of residents and support healthful, active lifestyles.

Objective 5.1 – Provide public parks distributed throughout the WRSP Area that include a range of recreational opportunities adequate to support the future population of the WRSP Area.

Policy 5.1.1 – Parks shall meet at minimum the City wide standard of 5 acres per 1,000 residents in the WRSP Area.

Objective 5.2 – Parks, Trails and Open Space will be designed as an integrated system of recreational amenities.

Policy 5.2.1 – Parks shall be programmed and designed through cooperation with the City of American Canyon Parks and Recreation Department and the general public and shall reflect the results of the City's Parks and Recreational Needs Assessment.

Policy 5.2.2 – Parks shall be centrally located in the WRSP Area with access afforded to a majority of the area residents via trail and street access.

Policy 5.2.3 – The Napa Valley Vine Trail and the River to Ridge Trail shall be interconnected and aligned to integrate with the primary parks where feasible in the WRSP Area.

Policy 5.2.4 – Adequate open spaces shall be maintained to protect environmental resources. Where feasible, trails and recreational amenities shall be provided along and through open space as part of the larger integrated system of recreational amenities.

Policy 5.2.5 – All trails in the WRSP shall be constructed pursuant to the City design standards and as set forth in Chapter 7 of the WRSP.

5.3 Parkland Requirements

The City of American Canyon General Plan requires a minimum of 5 acres of credited parkland per 1,000 residents (GP Policy 7.1.1). Based on the maximum of 1,439 WRSP Area dwelling units (including 186 affordable units) at 3.49 persons per unit, a total of 25.1 acres of dedicated parkland is required. The WRSP exceeds this requirement, providing approximately 63 acres of total land designated as Parks and Open Space within the Land Use Plan. Table 5.1 summarizes the total distribution of these parks and open space.

Depending on the ultimate use and recreational value, however, only partial credit may be granted for some parks, open space land containing informal recreational facilities, open space amenities, or natural features. Approximately 33 of the 63 total acres count towards these required parkland credits. Parkland requirements and credits are compared in Table 5.2.

Table 5.3 further details the breakdown of the different parkland uses and credit calculations within the WRSP Area. The credits applied are reflective of each parcel’s recreational value as a park or open space amenity. Park parcels and the Community Plaza are credited at 1:1 ratio, while trails and open space are credited at 0.5:1 ratio. Detention basins and wetlands acreage are not counted to fulfil the park requirement.

Parkland facilities are conceptually illustrated in Figure 5.1. Final sizing and location of parkland facilities will be determined during the tentative and final mapping stages.

Overall Open Space and Park Distribution	
Watson Ranch Specific Plan	
Open Space Types	AC
Parks	23.4
Community Plaza	2.0
Open Space and Trails	22.4
Detention	12.7
Wetlands	3.1
Total	63.6

Table 5.1: Open Space and Park Distribution

American Canyon I, LLC Property			
Units	Units	Population (@ 3.49 persons/unit)	Park Acreage Required @ 5 ac/1,000 people
Residential Units	1,196	4,174	20.87
Newell Family Property			
Units	Units	Population (@ 3.49 persons/unit)	Park Acreage Required @ 5 ac/1,000 people
Residential Units	243	848.1	4.24
Total			25.11

Table 5.2: Parkland Requirement and Credit Comparison

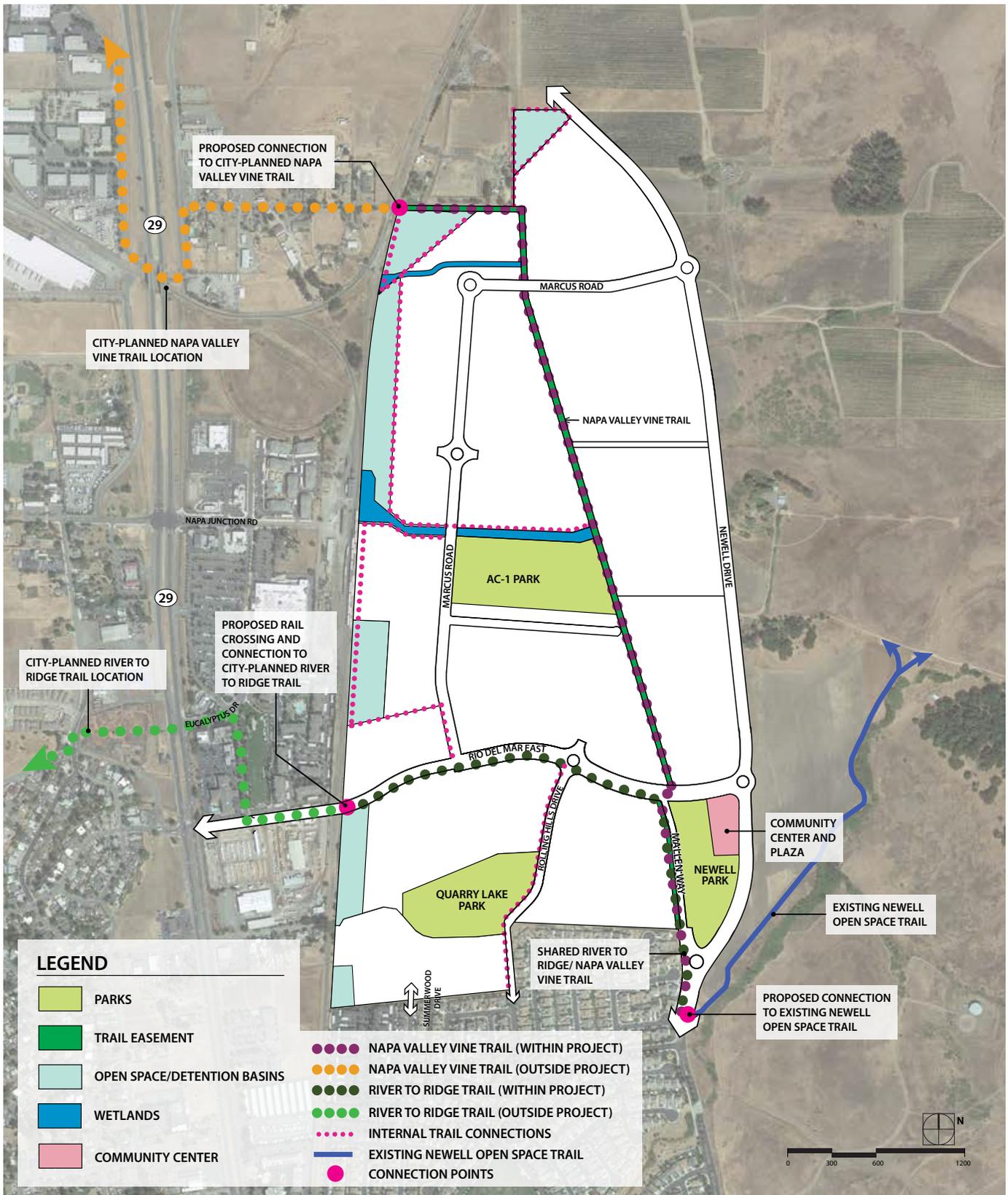


Figure 5.1: Parks Typology Map

Watson Ranch Specific Plan: Open Space/Park Credits

American Canyon I, LLC Property

Open Space Type/Name	Area (acres)	Park Credit	Credited Park Acreage
AC-1 Park	11.34	1	11.34
Quarry Lake Park (net)	6.67	0.5	3.33
Open Space and Trails	14.9	0.5	7.45
Detention	9.4	0.0	--
Wetlands	3.1	0.0	--
	45.41	Subtotal	22.12

Newell Family Property

Open Space Type/Name	Area (acres)	Park Credit	Credited Park Acreage
Newell Park	5.4	1	5.40
Community Plaza	2.0	1	2.00
Open Space/Trails	7.5	0.5	3.75
Detention	3.33	0.0	--
Wetlands	0.0	0.0	--
	18.23	Subtotal	11.15
Total Credited Acreage			33.27

Table 5.3: Detailed Parkland Credit Calculations

5.4 Parks and Open Space Typologies

The parks and open space system is comprised of differing typologies. The sizes, functions, configurations and locations of the parks are dependent upon their surroundings, use, and social/cultural needs. The typologies are distributed across the WRSP Area to serve the various neighborhoods and provide easy and convenient access to all residents. Refer to Figure 5.2 for a map identifying the proximity of parks within the WRSP Area.

5.4.1 COMMUNITY PLAZA

Plazas are formal urban parks that serve as focal elements for a community. The community plaza located at the intersection of Rio Del Mar East and Newell Drive is intended to serve as a centerpiece of activity for both the WRSP community and the City of American Canyon as a whole. It should be a flexible space that is comfortable for intimate conversations or solitary people-watching but can

also accommodate larger community gatherings such as art festivals, concerts, and other celebrations.

The following program includes amenities appropriate for the plaza:

- Public art
- Hardscape surface areas
- Turf seating areas
- Seat walls
- Landscape planters
- Landscaping
- Lighting
- Trash receptacles
- Formal Tree alley
- Accommodating a community building of up to 20,000 sf.

All improvements to the Community Plaza and Community Center site and building shall be at the City's sole cost and expense.

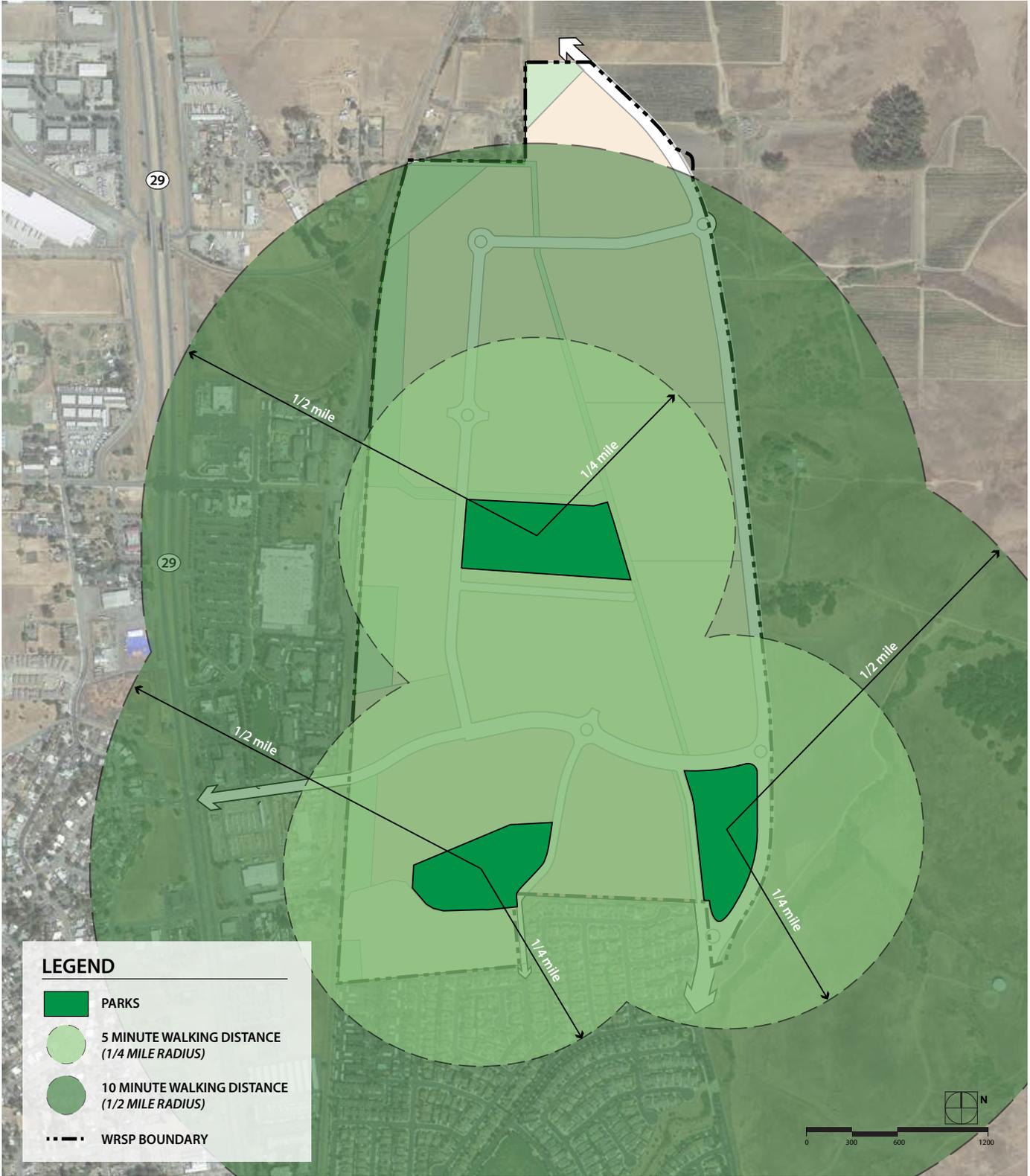


Figure 5.2: Parks Proximity Map

5.4.2 PARKS

Parks within the WRSP Area will provide a wide range of functions offering smaller recreational and social needs of the neighborhood to larger community wide gathering spaces. The sizes, functions, configurations and locations of the parks are dependent upon their surrounding natural features, neighborhood demographics, and social needs. Both public and private parks are included within the WRSP Area. Private parks include Quarry Lake Park and several neighborhood mini parks. There are two large public parks – “AC-1 Park” located at the neighborhood’s center and “Newell Park” located in the southeast corner of the WRSP Area adjacent to the Community Plaza and Community Center site.

QUARRY LAKE PARK

Surrounding and including the Quarry Lake, this approximately 8.4-acre park (Figure 5.3) is privately owned and maintained and serves as an important design and community element adjacent to the NVRG. Net usable

area (not including the lake) is approximately 6.7 acres. Public access will be determined by site access agreement between the property owner and the City. It is envisioned to be used by both visitors and residents and will be designed as a passive park that provides a more “natural” experience.

While offering a water viewing amenity, Quarry Lake may also serve important environmental functions such as water cleaning, biofiltration, stormwater detention, and riparian habitat. Aeration for the lake will be provided as both a visual amenity and for water quality.

The following program elements and landscape features should be considered for Quarry Lake Park:

- Lakeside trail
- Water observation stations
- Resting spaces with seating
- Exercise par course
- Hardscape plazas or promenades
- Connections to commercial uses and community facilities



This illustration represents a preliminary concept for the Quarry Lake area and is subject to modification

Figure 5.3: Conceptual Design - Quarry Lake Park

- Drought tolerant plant species
- Informal tree clusters
- Enhanced riparian habitat plantings around lake edge
- Landscaping that appears natural vs. manicured
- Accent (manicured) planting at entries
- Limited amount of turf grass
- Low maintenance native grasses
- Downward facing pole-mounted lights and bollards for safety
- Accent lighting on special landscape elements and park features – used sparingly
- Trail connections to the neighborhoods to the south

AC-1 PARK

This centrally located park is approximately 11.3 acres. AC-1 Park should be programmed with both passive and active uses. Level turf areas should be provided for active uses such as soccer and ball fields as well as smaller areas for passive recreation. The park should be buffered from the street with shrubs or earth berms. Flowering accent trees can be located to provide landmark areas and focal points, while a long windscreen run of tall narrow trees along the westerly boundary can screen the adjacent uses.

The following program elements and amenities should be considered for AC-1 Park:

- Playground
- Sports fields (soccer, baseball)
- Picnic facilities
- Community gardens
- Downward facing safety lighting
- Pathways
- Benches
- Trash receptacles
- Bike racks
- Drinking fountains
- Enhanced materials at entry nodes and seating areas
- Incorporation of the Napa Valley Vine Trail
- Wayfinding and educational signage
- Public restroom
- Downward facing pole-mounted lights and bollards for safety
- Accent lighting on special landscape elements and park features – used sparingly



Pedestrian path along water feature

PLANTING

The planting palette for this park should include a variety of deciduous and evergreen trees, shrubs, turf and grasses. Deciduous trees should be planted in clusters to separate active uses from passive spaces. Shade will be an important commodity in the summer months to shade users from the hot summer sun. During winter months defoliated trees will allow natural light to filter through, warming the users below. Flowering accent trees can be located at landmark features and focal points.

The following landscape features should be considered this park:

- Turf planting for open active areas
- Evergreen shrubs at key landmark or focal areas
- Boundary understory planting at western edge
- Flowering accent trees for landmark and focal areas
- Large canopy shade trees

NEWELL PARK

Newell Park (+/- 5.4 acres – not including the community center or plaza) is located in a triangular block bounded by Rio Del Mar East to the north, Newell Drive to the east, and a new north-south roadway to the west named “Mallen Way.” The Community Center will be located adjacent to this park at the corner of Rio Del Mar East and Newell Drive.

The following program elements and amenities should be considered for Newell Park:

- Picnic areas
- Playground

- Downward facing safety lighting and bollards
- Pathways
- Benches
- Seat walls
- Trash receptacles
- Bike racks
- Drinking fountains
- Enhanced paving materials at entries and seating areas
- Active turf areas
- Dog Park

PLANTING

Like many parks and public spaces, deciduous trees are used in clusters to provide specific functions such as shade for larger areas. Clustering trees separates passive uses from the open turf areas and active uses. Shade will be an important commodity in the summer months; trees shall be located for the purposes of shading the users from the hot summer sun. During the winter months the defoliated trees allow natural light and the limited sun exposure to filter through, warming the users below. General planting elements for this park may include:

- Turf planting for open active areas.
- Evergreen shrubs at key landmark or focal areas
- Boundary understory planting at western edge.
- Large canopy shade trees
- Flowering accent trees for landmark or focal areas.



Play Fields



Children's Play Area

5.4.3 POCKET PARKS AND MINI PARKS

These small passive spaces provide urban relief within a neighborhood. They will be privately owned and maintained and passive in nature. They may be as small as an individual lot and generally include: landscaping that complements the neighborhood, a play structure, turf, and benches or seat walls. While not required they are encouraged throughout the neighborhoods.

5.4.4 OPEN SPACE AREAS

Unprogrammed open space is located along the periphery of the WRSP Area and in transitional landscape areas. The areas comprised of level-to-moderate slopes can be planted with a simple understory mass of evergreen groundcovers and low to medium grasses: A single species of tree may be planted in rows symbolic of orchards and vineyards throughout northern California.

The Open Space Area landscape program may include, but not be limited to the following:

- Native and non-native non-invasive drought tolerant grasses.
- Native and non-native drought tolerant evergreen groundcover.
- Native and non-native trees planted in rows.
- Low maintenance landscape.

Open space areas may include:

Seasonal Wetlands - The seasonal wetlands and their surrounding buffer areas protect the existing sensitive

areas within the WRSP Area. They take advantage of natural systems to capture and clean storm water for the community while also providing for passive recreation. These areas will be connected to the rest of the open space system with multi-use trails.

Detention Basins - Detention basins are designed to be multiuse. During the dry months, when not being used for the purpose of detaining stormwater flow, they may allow for some recreational activities. The appropriate design, plant material and program elements make these a valuable component of the open space network providing both physical and visual benefits that are associated with open space.



Naturalized Planting



Open Play Area



Children's Play Area

5.5 Community Gardens and Edible Landscape

Supporting a healthy community with local food production supports establishing the WRSP Area as a contributor to all of American Canyon. To this end, the WRSP encourages the development of small and large community gardens throughout the WRSP Area. The community gardens could occur in locations such as:

- Within the mixed-use NVRG as public gardens, a garden of a particular restaurant, or on a larger urban farming scale, such as managing and harvesting groves of olive trees that occur within parking fields.
- Community gardens within the public parks.
- Edible gardens to teach where food comes from and how to grow healthy fruits and vegetables.
- Gardens established and managed by homeowners associations for use of their members.
- Individual or neighborhood yard gardens cared for by the resident or a contracted urban farmer.

In addition to community garden space dedicated to growing food, landscape throughout the community may include food bearing plants. There currently exists one significant fig tree in the NVRG that may be the catalyst for this edible landscape concept. Building on that concept, plantings may combine non-edible and edible species such as combinations of fruit and nut trees, berry bushes, vegetables, herbs, edible flowers, and ornamental plants.



Dog Park



Community Garden

5.6 Pedestrian and Bicycle Circulation

Pedestrian and bicycle circulation is integral to the overall open space and park network. Refer to Chapter 7 Circulation for details.



Park Furniture



Bike Racks

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CHAPTER 6 - DEVELOPMENT REGULATIONS

6.1 General Provisions

6.1.1 PURPOSE AND INTENT

The Development Regulations set forth in the Watson Ranch Specific Plan (WRSP) provide a comprehensive set of regulations governing the use and development of the land. While including references to specific provisions of the American Canyon Zoning Ordinance, these Development Regulations replace the American Canyon Zoning Ordinance within the WRSP Area. Should there be a conflict between the WRSP and the American Canyon Zoning Ordinance, the provisions of the WRSP shall govern. Any issue not directly or specifically covered by the WRSP shall be subject to non-conflicting regulations and procedures of the American Canyon Zoning Ordinance.

6.1.2 SEVERABILITY CLAUSE

If any term or provision of the WRSP, or the application of any provision of the WRSP to a particular situation, shall for any reason be found to be void, invalid, illegal or unenforceable by a court of competent jurisdiction, all other terms and provisions of the WRSP or the application of the WRSP to other situations shall remain in full force or effect.

6.2 Residential Development Regulations

The following development regulations apply to all residential developments within the Medium Density Residential (MDR-12 and MDR-16) and High Density Residential (HDR) designations of the WRSP Area. Nonresidential development such as parks and public facilities within these land use designations shall comply with the City of American Canyon Zoning Ordinance.

The regulations for the individual residential category do not specify building prototypes but rather address the building mass, scale and urban form through setback, height, and massing regulations. This will allow for flexibility and accommodation of different product types while still meeting the overall vision of the plan.

Residential plans are shown for graphic illustrative purposes to demonstrate the application of the development regulations. They do not constitute an actual design submittal. Lot sizes shown graphically are not necessarily minimums. Setbacks are illustrated as minimums.

6.2.1 ARCHITECTURAL VARIETY (MDR-12 AND MDR-16)

The WRSP vision is to craft a community that has variety in the residential neighborhoods, and to prevent homogenous or mono-culture subdivisions. The following standards are intended to support the vision.

6.2.2 MEDIUM DENSITY RESIDENTIAL (MDR-12) AND MEDIUM DENSITY RESIDENTIAL (MDR-16) DETACHED DEVELOPMENT

In order to avoid monotony of streets lined with single family detached homes of similar width, height, and setback, the following criteria shall be applied:

- Corner lots side yard setback shall be at least 5 feet wider than interior lots.
- Each block shall include at least three different and distinct elevations. The elevations shall vary in massing, roof form, and appearance from streets or paseos. The blocks are assumed to be a maximum of 600 feet and where block sizes exceed that distance, the excess length shall be considered a new block.
- Refer to Table 6.2.1 and Table 6.2.2 for development regulations.

6.2.3 MEDIUM DENSITY RESIDENTIAL (MDR-12) AND MEDIUM DENSITY RESIDENTIAL (MDR-16) ATTACHED DEVELOPMENT

- When buildings are located adjacent to detached units in the same block, the closest attached building will have the same front yard setback as the detached unit.
- To avoid overly long buildings and create the appropriate scale and building wall face, buildings longer than 180 feet or 8 units, whichever is less, are prohibited.
- Refer to Table 6.2.3 for development regulations.

6.2.4 HIGH DENSITY RESIDENTIAL (HDR) DEVELOPMENT

- Building facades facing public rights-of-way or public open spaces exceeding a length of fifty (50) feet shall provide modulation in the building form. Modulation could occur on the horizontal and/or vertical plane and may include, but not be limited to, a change in the building plane, fenestration pattern, material and/or color.
- Blank walls at the ground floor are strongly discouraged. Additional architectural features and/or landscaping shall be provided as an alternative.
- Entries should be a prominent feature of the building and be visually distinctive from the rest of the facade.
- Refer to Table 6.2.4 for development regulations.

6.2.5 SETBACKS AND IRREGULAR LOTS

Setbacks are measured from the appropriate front, side, or rear lot line, or ultimate street right of way line, whichever is most restrictive. Irregular lots are defined as lots that are non-rectangular, lots with three sides, or lots with more than four sides that require specific definition of lot lines in order to achieve the purpose of the specific setbacks. Refer to Figure 6.1 for measuring setbacks on irregular lots.

Front Lot Line: On an interior lot, the line separating the lot from the street. On a corner lot, the shorter lot line abutting a street. On a through lot, the lot line abutting the street or lane providing the primary access to the lot. On a flag or panhandle lot, the interior lot line most parallel to and nearest the street or lane from which access is obtained.

The width of the “flag” must be a minimum of 20’ and can be no longer than 2 times the width of the lot it serves. One “flag” driveway may access a maximum of 2 lots.

Rear Lot Line: The lot line that is opposite and most distant from the front lot line. In the case of an irregularly shaped lot, a minimum ten-foot-long line which is within the lot and parallel to and most distant from the front lot line shall be considered the rear lot line for purposes of determining setback.

Side Lot Line: All lot lines, which are not front or rear lot lines, shall be considered side lot lines for setback purposes.

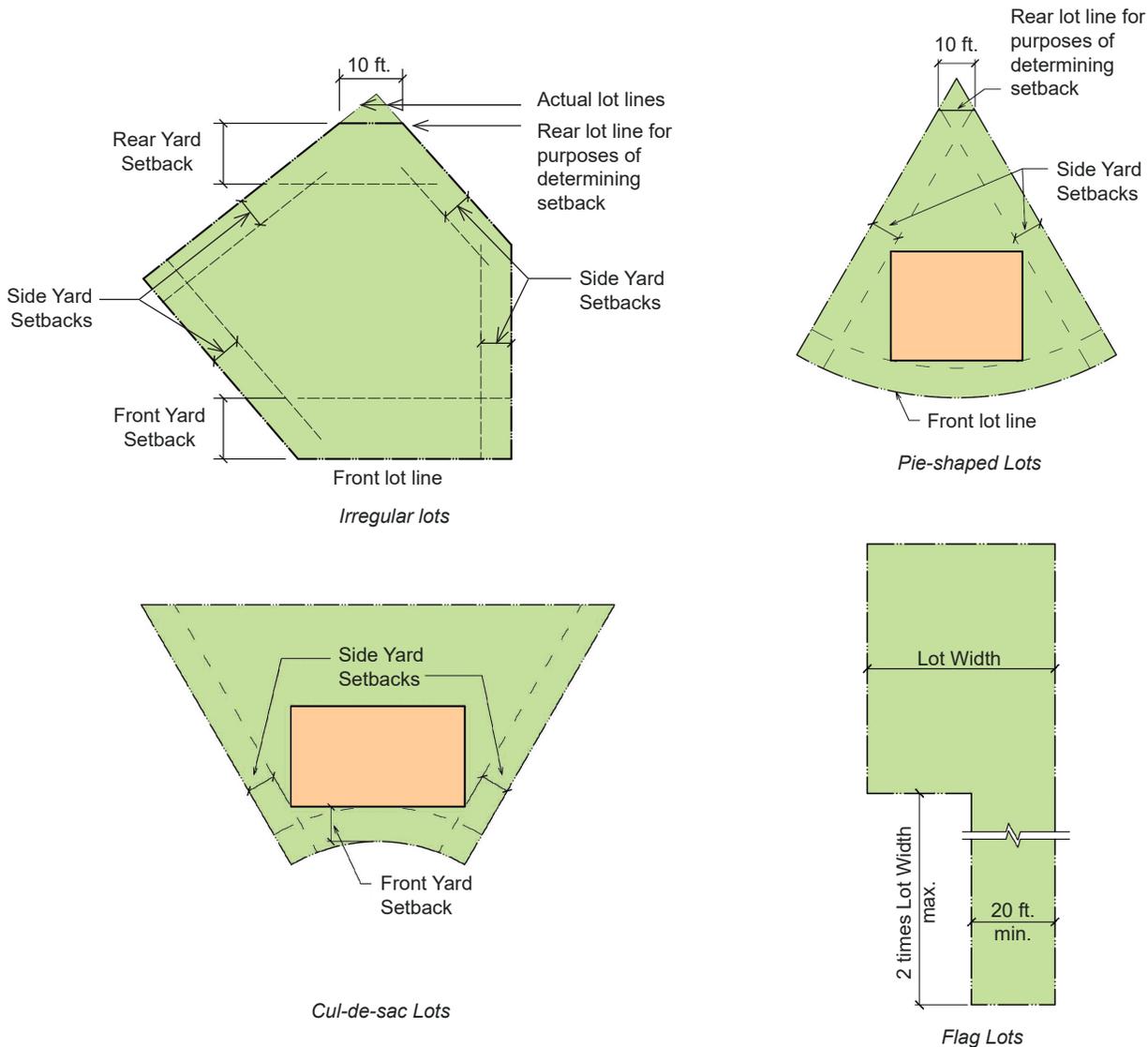


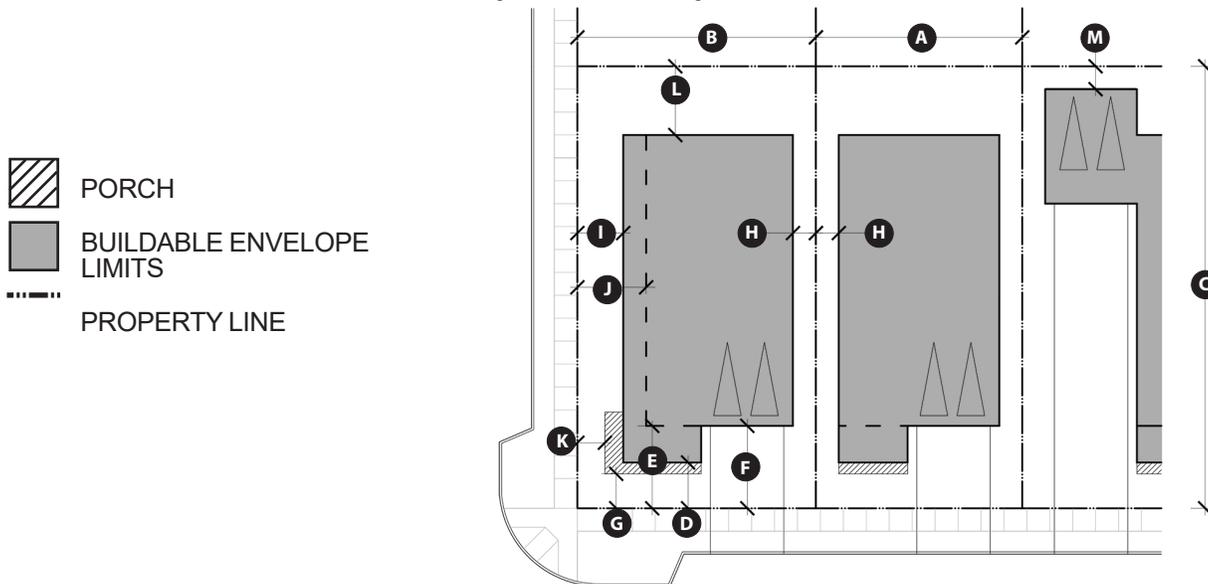
Figure 6.1: Irregular Lots: Measuring Setbacks

Table 6.2.1: Medium Density Residential (MDR-12) Development Regulations

Table 6.2.1: Medium Density Residential (MDR-12) Development Regulations		
Minimum Lot Size	3,200 sf	
A Minimum width, interior lot	40 ft.	
B Minimum width, corner lot	45 ft.	
C Minimum lot depth	55 ft.	Lot lines may extend to the center line of the alley if alleys are designed as private streets
Setbacks ^{1,2}		Chimneys, fireplaces, accent walls or pilasters, bay windows, eaves or similar architectural projections may encroach as per City of American Canyon zoning ordinance.
D Front, 1 st & 2 nd stories	15 ft.	At least 30% of homes on the same block face shall provide a minimum 18 ft. front setback
E Front, 3 rd story	20 ft.	
F Front, front-loaded garage door	20 ft.	
G Front, covered porch	10 ft.	
H Side, internal lot	5 ft.	
I Side, corner lot 1 st & 2 nd stories	10 ft.	
J Side, corner lot 3 rd story	15 ft.	
K Side, corner lot covered porch	6 ft.	
L Rear	15 ft.	Rear setbacks may be reduced to 10' for wide shallow lots if the usable yard space is provided on the side yards.
M Rear, garage	5 ft.	
N Rear, alley	4 ft.	
Maximum Height	35 ft.	
Maximum FAR	.7	FAR does not include covered garages
Maximum Lot Coverage	55%	Walkways, driveways, porches (covered or uncovered) are excluded from lot coverage calculations

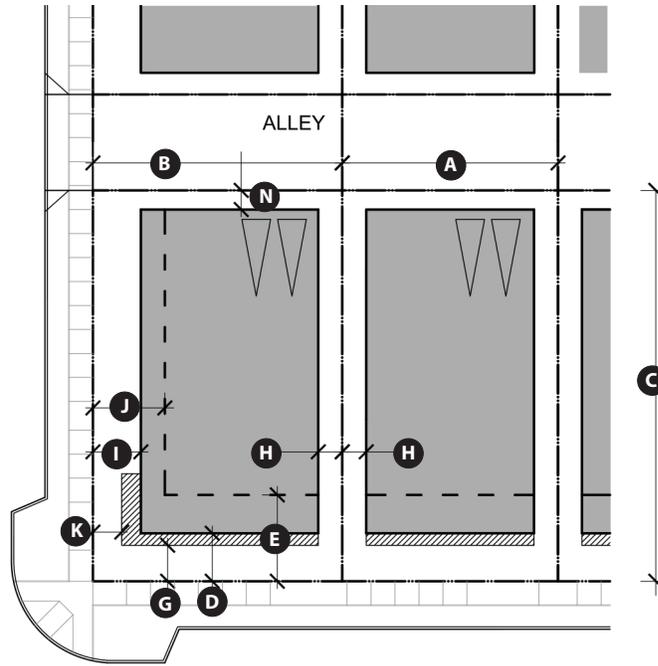
1For cluster type residential products, lot sizes required setbacks shall be determined through the review process.

2Minimum front and rear setbacks shall be 25 ft. along arterials, 15 ft. along minor collectors.

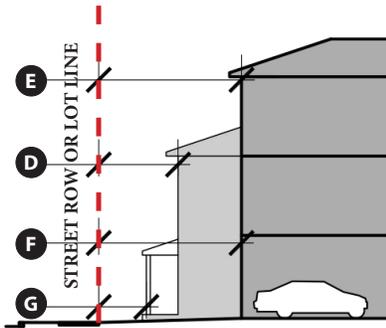


Medium Density Residential (MDR-12)

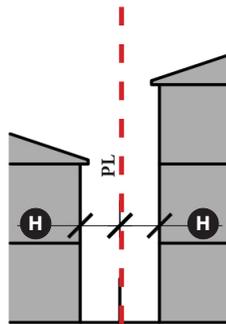
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-  BUILDABLE ENVELOPE LIMITS
-  PROPERTY LINE



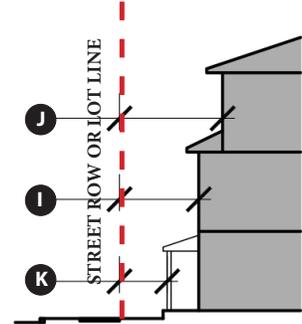
Medium Density Residential (MDR-12) - Alley Loaded



Front setbacks



Side setbacks at internal lots



Side setbacks at corner

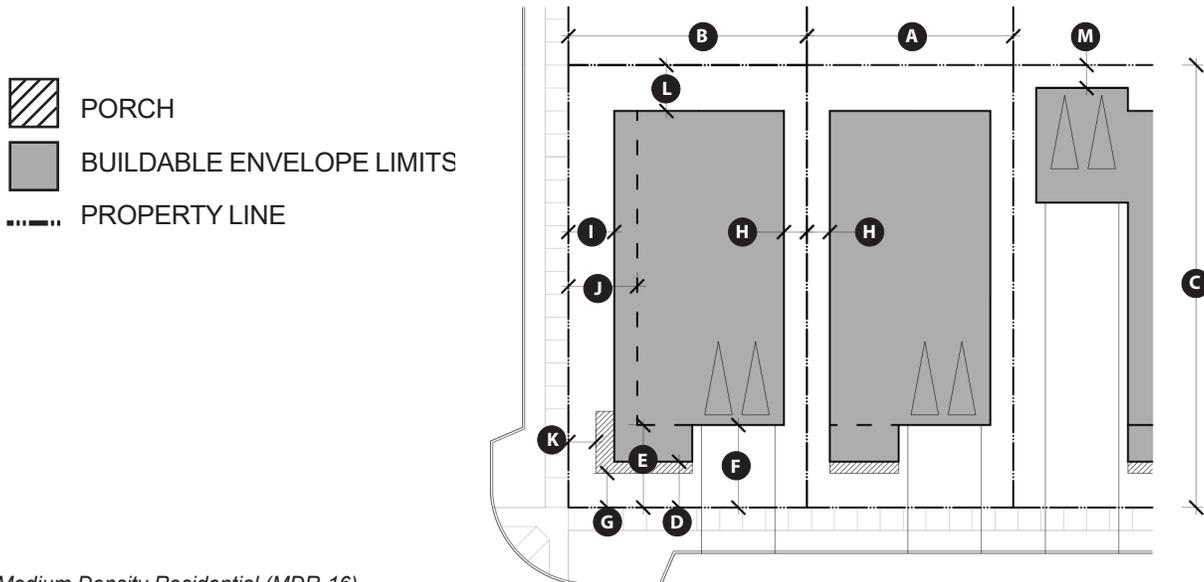


Table 6.2.2: Medium Density Residential (MDR-16) - Development Regulations, Detached Dwellings

Table 6.2.2: Medium Density Residential (MDR-16) - Development Regulations, Detached Dwellings		
Minimum Lot Size	1,500 sf	
A Minimum width, interior lot	30 ft.	
B Minimum width, corner lot	35 ft.	
C Minimum lot depth	50 ft.	Lot lines may extend to the center line of the alley if alleys are designed as private streets
Setbacks^{1,2}		Chimneys, fireplaces, accent walls or pilasters, bay windows, eaves or similar architectural projections may encroach as per City of American Canyon zoning ordinance.
D Front, 1 st & 2 nd stories	10 ft.	At least 30% of homes on the same block face shall provide a minimum 20 ft. front setback
E Front, 3 rd story	15 ft.	
F Front, front-loaded garage door	20 ft.	
G Front, covered porch	10 ft.	
H Side, internal lot	3 ft.	
I Side, corner lot 1 st & 2 nd stories	8 ft.	
J Side, corner lot 3 rd story	10 ft.	
K Side, corner lot covered porch	6 ft.	
L Rear	10 ft.	
M Rear, garage	5 ft.	
N Rear, alley	4 ft.	
Maximum Height	35 ft.	
Maximum FAR	.7	FAR does not include covered garages
Maximum Lot Coverage	55%	Walkways, driveways, porches (covered or uncovered) are excluded from lot coverage calculations

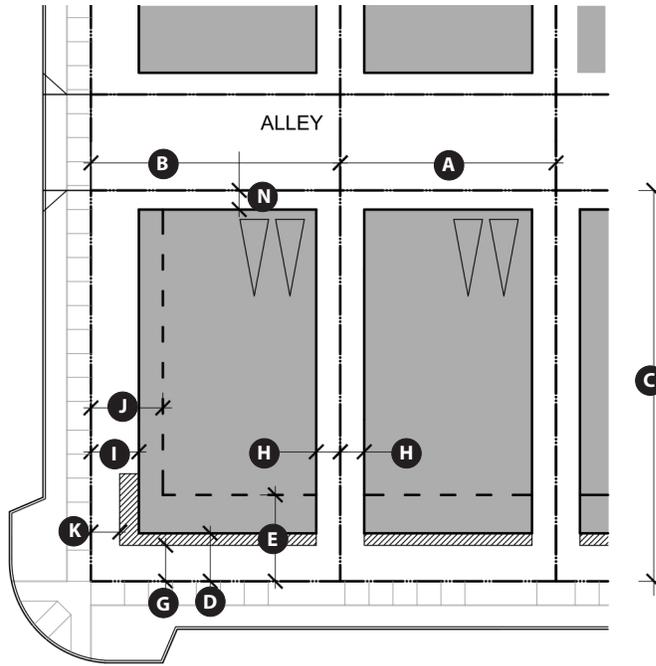
1For single family cluster type residential products, and single family bungalows designed as part of condominium project lot sizes and required setbacks shall be determined on a case by case basis through the design review process.

2Minimum front and rear setbacks shall be 25 ft. from arterials, 15 ft. from minor collectors.

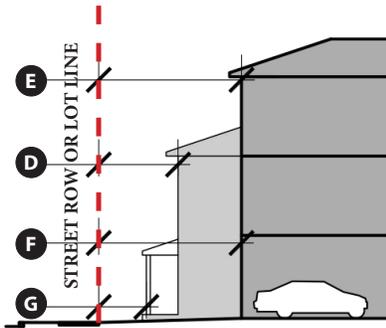


Medium Density Residential (MDR-16)

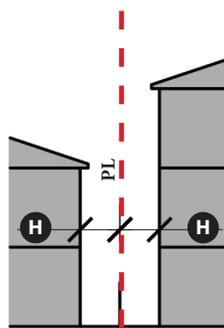
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-  PROPERTY LINE



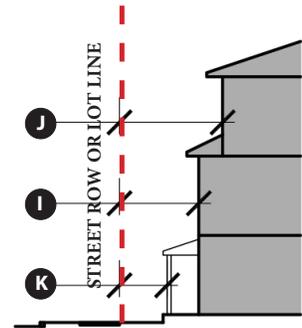
Medium Density Residential (MDR-16) - Detached, Alley Loaded



Front setbacks



Side setbacks at internal lots



Side setbacks at corner

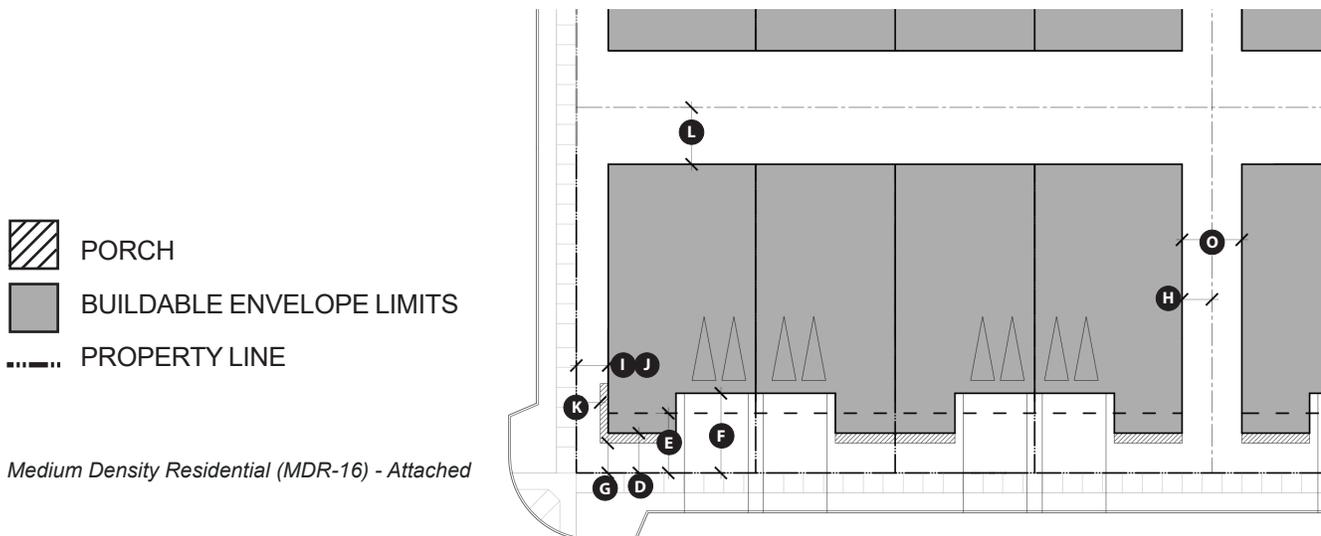


Table 6.2.3: Medium Density Residential (MDR-16) - Development Regulations, Attached Dwellings

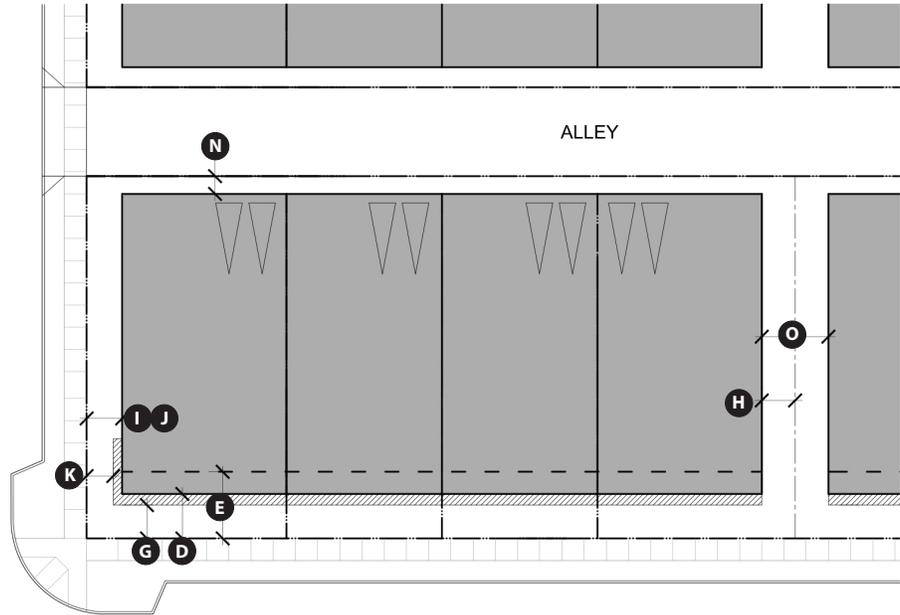
Minimum Lot size¹	800 sf	
Minimum lot width, interior	16 ft.	
Minimum lot width, corner	25 ft.	
Minimum lot depth	50 ft.	Lot lines may extend to the center line of the alley if alleys are designed as private streets
Setbacks²		Chimneys, fireplaces, accent walls or pilasters, bay windows, eaves or similar architectural projections may encroach as per City of American Canyon zoning ordinance.
D Front, 1 st & 2 nd stories	10 ft.	
E Front, 3 rd story	15 ft.	Balconies and decks may encroach 5' into the required setback.
F Front, garage	20 ft.	
G Front, covered porch	10 ft.	
H Side, internal lot	10 ft.	Only applicable to the end of rows of attached units.
I Side, corner lot 1 st & 2 nd stories	10 ft.	
J Side, corner lot 3 rd story	10 ft.	
K Side, corner lot covered porch	6 ft.	
L Rear	10 ft.	Porches and stoops may encroach 5' into the required setback.
N Rear, alley	4 ft.	
O Minimum distance between main buildings	15 ft.	
Maximum Height	35 ft.	
Maximum FAR	1	FAR calculated over gross acreage of proposed attached development and does not include covered garages
Maximum Lot Coverage	55%	Walkways, driveways, porches (covered or uncovered) are excluded from lot coverage calculations
Minimum Usable Open Space	300 sf per unit	<ul style="list-style-type: none"> Usable open space can be provided as group or private open space. Private open space shall count as usable open space and the actual measurement of an area which meets the criteria of private usable open space, shall be doubled, and the resulting figure applied toward the total usable open space requirements. Exterior usable group open space shall be not less than 400 square feet in area, and a rectangle inscribed within it shall have no dimension less than 20 feet. Exterior private open space shall be not less than 100 square feet in area, and a rectangle inscribed within it shall have no dimension less than 10 feet. Above-ground open space shall not be less than 60 square feet in area, and a rectangle inscribed within it shall have no dimension less than 6 feet

¹Minimum lot sizes are applicable where attached units are designed on individual lots as fee simple units

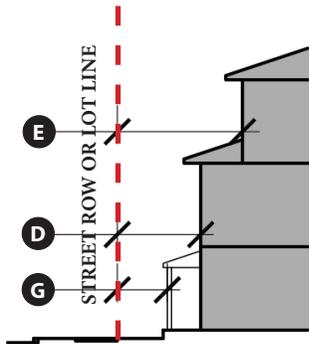
²Minimum front and rear setbacks shall be 25 ft. along arterials, 15 ft. along minor collectors.



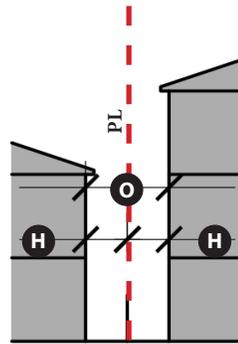
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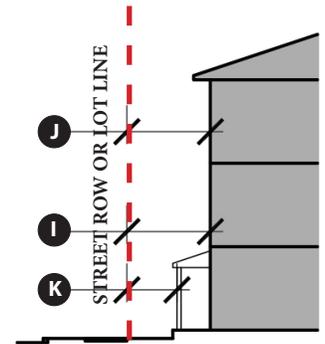
Medium Density Residential (MDR-16) - Attached, Alley Loaded



Front setbacks



Side setbacks at internal lots



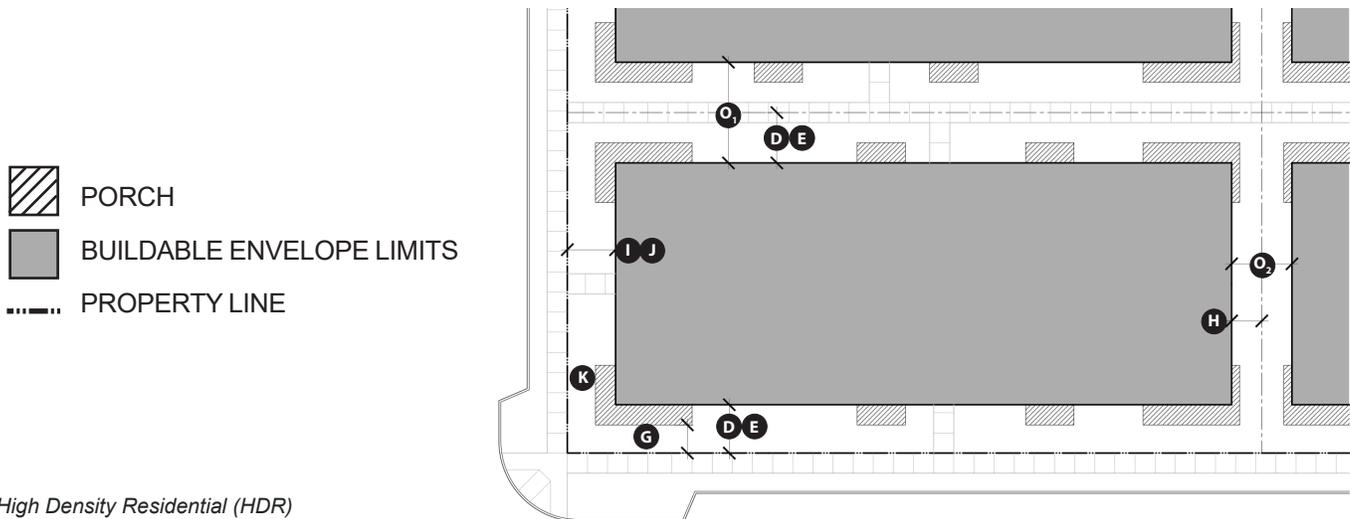
Side setbacks at corner



Table 6.2.4: High Density Residential (HDR) Development Regulations

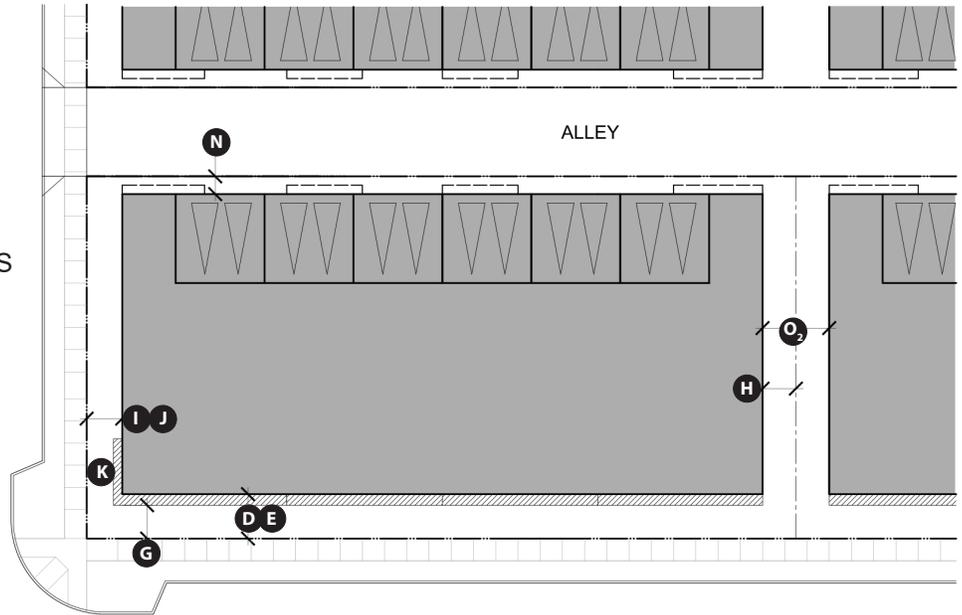
Table 6.2.4: High Density Residential (HDR) Development Regulations		
Minimum Lot size	20,000 sf	
Minimum lot width, interior	100 ft.	
Minimum lot width, corner	100 ft.	
Minimum lot depth	100 ft.	Lot lines may extend to the center line of the alley if alleys are designed as private streets
Setbacks¹		Chimneys, fireplaces, accent walls or pilasters, bay windows, eaves or similar architectural projections may encroach as per City of American Canyon zoning ordinance.
D Front, 1 st & 2 nd stories	15 ft.	
E Front, 3 rd story	15 ft.	Balconies and decks may encroach 5' into the required setback.
G Front, covered porch	10 ft.	
H Side, internal lot	10 ft.	
I Side, corner lot 1 st & 2 nd stories	15 ft.	
J Side, corner lot 3 rd story	15 ft.	
K Side, corner lot covered porch	10 ft.	
L Rear	10 ft.	Porches and stoops may encroach 5' into the required setback.
N Rear, alley	4 ft.	For all alley loaded units, rear setbacks are measured from the property line to the face of the garage door
O Minimum distance between main buildings	1. Front: 30 ft. 2. Side: 20 ft. 3. Rear: 30 ft.	
Maximum Height	40 ft.	Maximum height may be exceeded by 10' by tower rooms less than 200 sf, and distinctive architectural features such as towers, spires or cupolas. The total square footage of all projections that exceed the maximum height shall not exceed 15% of the roof area.
Maximum FAR	1	FAR does not include covered garages
Maximum Lot Coverage	50%	Walkways, driveways, porches (covered or uncovered) are excluded from lot coverage calculations
Minimum Group Open Space	300 sf per unit	<ul style="list-style-type: none"> Usable open space can be provided as group or private open space. Private open space shall count as usable open space and the actual measurement of an area which meets the criteria of private usable open space, shall be doubled, and the resulting figure applied toward the total usable open space requirements. Exterior usable group open space shall be not less than 400 square feet in area, and a rectangle inscribed within it shall have no dimension less than 20 feet. Exterior private open space shall be not less than 100 square feet in area, and a rectangle inscribed within it shall have no dimension less than 10 feet. Above-ground open space shall not be less than 60 square feet in area, and a rectangle inscribed within it shall have no dimension less than 6 feet

¹Minimum front and rear setbacks shall be 25 ft. along arterials, 15 ft. along minor collectors.

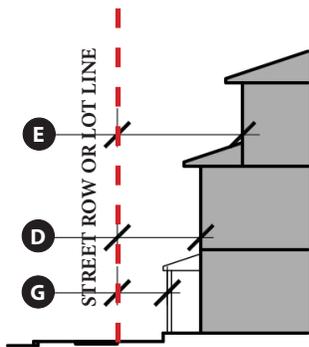


High Density Residential (HDR)

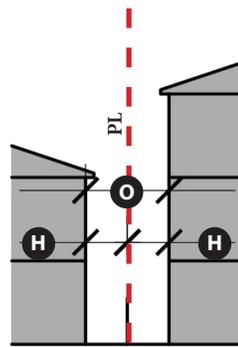
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-  BUILDABLE ENVELOPE LIMITS
-  PROPERTY LINE



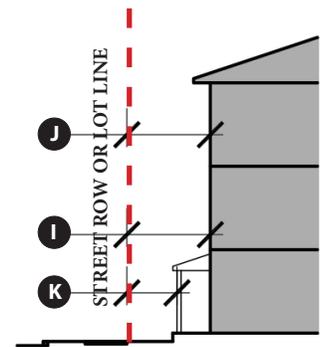
High Density Residential (HDR) - Alley Loaded



Front setbacks



Side setbacks at internal lots



Side setbacks at corner



6.3 Napa Valley Ruins & Gardens Development Regulations

The following development regulations apply to all non-residential development within the Napa Valley Ruins & Gardens Mixed Use (NVRGMU) land use designation.

Given the unique nature of the NVRG, development regulations need to provide flexibility while steering new development towards good design.

6.3.1 SETBACK AND HEIGHT REGULATIONS

REQUIRED SETBACKS

Required setbacks are as determined through the design permit review process set forth in the Municipal Code Section 19.41.010 et. seq. The ruins within the NVRG create a mass and scale that will form the backdrop for new construction. As buildings will be worked in and around the ruins, setback requirements must be flexible to respond to this context. Refer to Table 6.3 for building setbacks.

MAXIMUM BUILDING HEIGHT

The maximum building height for new buildings is 55 feet. Exceptions to this maximum may be considered and permitted as a means of re-establishing a landmark statement within the NVRG reflective of the historic silos on site. Refer to Table 6.3 for building heights.

6.3.2 SITE DESIGN REGULATIONS

VEHICULAR ACCESS

Vehicular access to buildings within the NVRG area is intended to provide for access to parking and service uses, and provide for emergency vehicular access while in a subordinate role to the pedestrian-oriented environment. To preserve the pedestrian nature of the NVRG, vehicular access may be provided via shared pedestrian ways, or woonerfs, or via plaza hardscape, and controlled through

Requirements	NVR&G	Other Commercial
Building Setbacks	Flexible to accommodate existing ruins	Per city of American Canyon Zoning Code
Building Heights	55'	Per City of American Canyon Zoning Code

Table 6.3: Allowable Building Setbacks and Height

the use of removable bollards or other devices to control access and timing of vehicular access.

STREET-LEVEL INTEREST

In order to support successful businesses within this area, activation and diversity of retail and commercial options are important at the street level to make them attractive as a destination for shoppers and visitors.

GROUND FLOOR USES

In order to facilitate a vibrant, pedestrian-oriented streetscape, buildings with street or publicly oriented frontage shall provide street-level pedestrian-oriented uses on all street-facing frontages.

NEW BUILDING DESIGN

New buildings shall be designed to complement the pedestrian orientation of their context, integrate into the adjacent ruins structures and community, and contribute to the creation of a vibrant and interesting mixed-use environment. Buildings located at street corners shall have the same level of design, detail and transparency for both frontages to maintain continuity and pedestrian interest.

PARKING LOCATION

Off-street surface parking shall be located such that it does not interrupt pedestrian flow between various uses. Curb cuts shall be minimized and located in areas least likely to impede pedestrian circulation (see Section 6.4 for parking regulations).

GROUND-LEVEL SERVICE FACILITIES

Loading, delivery and trash removal elements have the potential to present and impose an adverse impact on the pedestrian environment and to create hazards for



Trash enclosures shielded from public view

both people and automobiles if not given proper attention. Therefore, planning in the early stages of development must consider the placement of service facilities and their effects on the public environment so that they do not create unsightly conditions. In order to minimize the negative visual and physical impacts of service facilities, the following treatments shall be applied:

- Trash storage, loading, and truck parking shall be located to minimize visibility from streets, pedestrian ways and building entrances and minimize interference with commercial or retail activities. Service and loading areas shall not be located along important pedestrian or view corridors.
- Service entrances shall not face primary or secondary retail and commercial streets. All service entrances and associated loading docks and storage areas shall be located to the side or rear of a building.
- Loading docks and truck parking shall be screened from public view using building mass, freestanding walls, and/or landscaping. Structural screening shall be made of materials and colors consistent with that of the principal structure(s).
- Roll-up security doors at service entrances shall be detailed to conceal door housings and tracks and provide an attractive, finished appearance for all exposed components.

All exterior trash receptacles shall be screened from public view on three sides, and on the fourth shall be screened by a gate that also obscures views. The enclosure shall be made of materials and colors compatible with that of the principal structure(s).

BUILDING LIGHTING

Moving, blinking, or strobe lights are prohibited.

Illuminating a franchise building as a form of advertising is prohibited.

6.4 Parking Regulations

6.4.1 PARKING REQUIREMENTS

REQUIRED OFF-STREET PARKING SPACES

Each land use identified below shall be provided at least the number of off-street parking spaces listed. Uses not listed are subject to the parking requirements of Chapter 19.21 of the City of American Canyon Municipal Code.

NVRG area: 1 space for 400 sf of enclosed covered area, and 1 space for 200 sf of outdoor seating area. Overflow parking in parking orchards may be counted towards fulfilling parking requirements. A Parking Management Plan (PMP) shall be submitted as part of the NVRG Master Plan/Design Permit (see section 6.4.2.1).

Hotel: 1 parking space per room plus 1 space for every 10 rooms.

Residential: Off-street parking requirements shall be in accordance with Chapter 19.21 of the City of American Canyon municipal Code.

RV parking is not allowed in driveways, front yards, or on streets.

MINIMUM DIMENSIONS, UNCOVERED PARKING

Where parking stalls are located adjacent to landscaped areas, the paved depth of such stalls may be decreased by two feet provided a vehicle overhang area exists. The vehicle overhang area may not encroach into a public sidewalk unless that sidewalk is widened by the depth of the overhang. Parking area parking spaces and drive aisle dimensions shall be as listed below.

Standard space: 9 ft x 18 ft.

Compact space: Up to 15% of the total number of required parking spaces may be designed as compact spaces with minimum dimensions of 8.5 ft x 16 ft.

Drive aisle widths for parking lots with perpendicular parking is 24' minimum.

MINIMUM DIMENSIONS, COVERED PARKING

Garages shall be constructed to meet the following minimum dimensions:

Single car: 12 ft x 20 ft

Two car standard: 20 ft x 20 ft

Two car tandem: 12 ft x 36 ft. (Tandem parking is allowed)

for all residential types as long as both spaces are assigned to the same unit/residence, and in non-residential parking areas where they are valet parked.

Garages shall provide additional area outside the vehicle parking area to accommodate all trash, garbage, green waste, and recyclables containers. The Community Development Director may waive this requirement if an alternative location is provided on site.

6.4.2 NAPA VALLEY RUINS & GARDENS SHARED PARKING PROGRAM

Shared parking is permitted within the NVRG area allowing for a reduction of minimum parking requirements for individual uses, as per the City of American Canyon Municipal Code Section 19.11.060 (C) Shared Parking. On-street parking may be counted towards required off-street parking spaces.

POLICY 6.4.2.1 PARKING MANAGEMENT PLAN.

A Parking Management Plan (PMP) shall be prepared and submitted as part of the Master Plan/Design Permit for the NVRG area. The Parking Management Plan (PMP) will include, but not be limited to:

- Designated parking areas
- Parking circulation
- Special signage
- Parking fees



Examples of bike racks

- Parking time limits
- Ride sharing incentives
- Biking and walking options
- Shuttle service

6.4.3 BICYCLE FACILITIES

A comprehensive system of bicycle parking facilities shall be provided within the WRSP Area. Where bike parking and storage facilities are provided for specific uses, they shall be located at safe and easily accessible locations, in close proximity to the building entrances that they serve. The bike parking facilities shall be provided per the following minimum standards:

NAPA VALLEY RUINS & GARDENS:

Bike parking shall be appropriately located within the NVRG to provide easy access to the trail connections and the retail and commercial uses within the village: A minimum of 10 percent of the number of required automobile parking spaces, with a minimum of one two-bike capacity rack.

MULTI-FAMILY RESIDENTIAL

A minimum of 10 percent of the number of required automobile parking spaces, with a minimum of one two-bike capacity rack.

PARKS AND OPEN SPACE:

A minimum of 10 percent of the number of required automobile parking spaces, with a minimum of two four-bike capacity racks.



6.5 Signage Regulations

These regulations are intended to result in functional, attractive signage incorporating a high level of design, graphics and efficient maintenance throughout the WRSP Area. All signs shall meet the Signage Guidelines in the WRSP. A master signage plan shall be prepared for the NVRG area as part of the first Design Permit entitlement.

6.5.1 PROHIBITED SIGNS

In addition to the signs prohibited pursuant to Section 19.23.060, Prohibited Signs, of the American Canyon Municipal Code, the following signs are prohibited.

- Decal signage on glazing.
- Internally illuminated awnings.
- Plastic-faced monument signs.
- Conventional plastic-faced box or cabinet signs.
- Formed plastic or injection molded plastic signs.
- Signs with luminous vacuum-formed letters.
- Cloth, paper, cardboard or foam signs or decals.
- Blinking, flashing, animated or moving signs.
- Advertising displayed on vehicles to attract attention to a specific business location or sale.

6.5.2 COMMERCIAL TENANT IDENTIFICATION AND STOREFRONT SIGNAGE

Tenant signs shall be limited to the identification of building tenants. Tenant signage must comply with the Design Guidelines contained herein and the master signage program prepared for the NVRG Mixed Use area.

6.5.3 LIVE-WORK SIGNAGE

The following signs are prohibited for live/work developments:

- Franchise signage
- Internally lit acrylic or plastic signs



Examples of creative signage appropriate at the Napa Valley Ruins & Gardens

6.6 Landscape Regulations

6.6.1 GENERAL LANDSCAPE REGULATIONS

LAYOUT AND DESIGN

Planting layout and design should be conducted with knowledge of plant material habits and varieties as a guide for proper spacing and combinations. The ultimate size and growth speed of plant materials should be considered to achieve an interesting, diverse landscape effect.

LOCATION

Shrubs shall not be planted within two feet (2') of residential walks, curbs, driveways, or architectural elements.

Trees shall not be planted within two feet (2') of residential walks, curbs, driveways or architectural elements. Trees planted within five feet (5') of walks, curbs, or pavement shall require continuous root barriers.

PLANT SELECTION

Residential Local Street trees shall be chosen from an approved Street Tree List.

Common Area landscaping (both private and public) shall comply with the City of American Canyon's water efficient landscape ordinance (see Section 6.6.3 below).

Shrubs, grasses, vines, groundcovers and perennials shall be chosen from an approved plant list and shall constitute a minimum of 30% of the plant palette.

Trees in private common areas shall consist of at least 30% from an approved plant list and shall be located appropriately for the available planting space.

PLANT SIZE

Plants shall be of the following minimum size and spacing at the time of installation:

Shrubs: A minimum of 35% of shrubs shall be 5 gallon size.

Ground Cover: Ground cover planting shall be installed at sizes and spacing to provide complete cover within one year of installation.

Vines: Vines shall be a minimum size of five gallons.

Trees: See Section 6.6.2 below.

6.6.2 STREET TREES

Street trees shall be a minimum size of 15 gallons, and all street trees shall be installed with root barriers where necessary to prevent sidewalk uplift. 24-inch box trees shall be planted in areas of special interest such as key intersections, neighborhood entries, or focal points.

A minimum of three street trees shall be planted for every 100 linear feet of street frontage. Street trees shall be spaced a maximum of 30' on center and be in accordance with the following minimum setbacks:

- 30' setback from street corners for sight line visibility (greater setback may be required at specific conditions, as determined by the City Engineer)
- 10' setback from commercial driveways
- 5' setback from residential driveways
- 15' setback from streetlights and traffic control signals
- 5' setback from water, gas and fire service laterals
- 5' setback from sewer and gas laterals

6.6.3 MODEL WATER EFFICIENCY LANDSCAPE ORDINANCE

In September 2009, the State adopted the Water Conservation in Landscaping Act of 2006 (AB1661). As of January 1, 2010 all local jurisdictions in California are required to implement this law, with the aim of reducing water consumption by landscaping.

The State Department of Water Resources (DWR) has prepared a Model Water Efficient Landscape Ordinance (MWELo) for use by local jurisdictions. The city of American Canyon has adopted a MWELo for all new development which is found in the Zoning Ordinance (19.22.010 - 19.22.080).

WRSP must comply with the City's MWELo requirements in all landscapes.

6.7 Walls and Fencing Regulations

Fences, walls, and dense hedges shall comply with the following regulations:

- Front Yards: Fences and walls within a required front yard shall not exceed 36 inches in height.
- Corner Intersections: Fences and walls at a corner intersection may not exceed 30 inches within the Public Works Standard for a Controlled Area at an intersection.
- Alleys: Fences and walls located within five feet of the edge of paving of an alley shall not exceed 36 inches in height.
- Where private side yards are adjacent to a street, privacy fencing should only enclose the area that is consolidated into usable yard space; privacy fencing to enclose minimum side setbacks adjacent to a street is not permitted.
- Sidewalks. Fences and walls within three feet of a sidewalk shall not exceed 36 inches in height.
- Continuous vandal resistant, safety fencing shall be required along the railroad right of way to prevent pedestrian and bicycle access into the railroad right of way. Fencing shall be a minimum of 6 feet in height and shall be designed to comply with the Design Guidelines in Appendix A, Section A.4.3.1 and the “view fencing” in section A.7. Where homes are located closer than 50 feet to the railroad right of way, a solid fencing design constructed of masonry or metal must be utilized.

See Figure 6.2 for an illustration of wall and fencing regulations.

6.8 Utility Regulations

All utility transformers and pedestals shall be located below ground in vaults to minimize their appearance in the community.

- ① Side yard at corner lots
- ② Front yard with fence/ low wall
- ③ Front yard without fence/ low wall

Front Yard Fence
Good Neighbor Fence

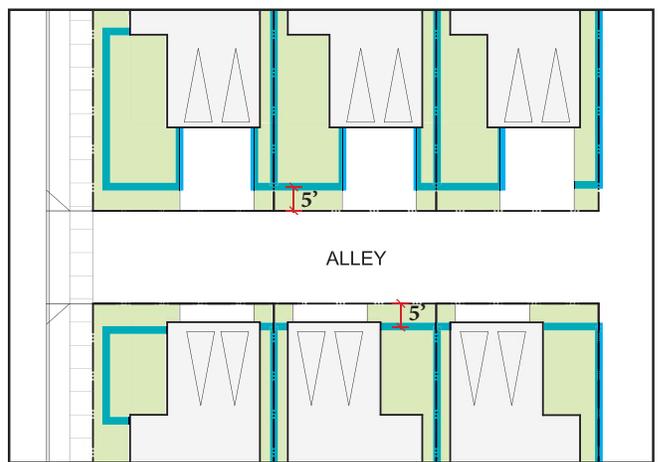
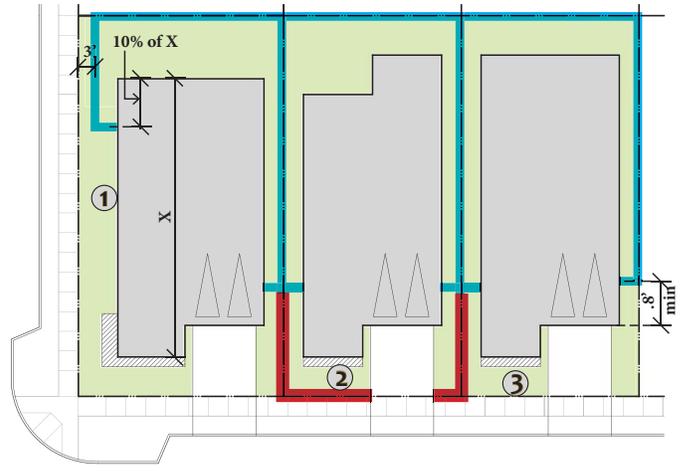


Figure 6.2: Fencing Regulations

6.9 Resource Management Guidelines

The sustainability of a community depends on creating and maintaining its economic and environmental health. It is important that for communities to be successful, they develop principles and a collective vision for the future that applies an integrative approach to environmental, economic, and social goals. The WRSP aims to achieve a high level of sustainability incorporating a three-tier approach to sustainability through an effective management of resources at all levels of planning, design, and construction.

TIER 1 DESIGN GUIDELINES

The first tier involves using sustainable principles in community design and overall master planning. An integral relationship exists between how a community is planned and developed through its form, configuration, and use along with its capacity to meet its social, environmental, and economic needs. Community form, which represents the needs and priorities of the community, directly influences community capacity to sustain itself into the future. The sustainability design guidelines used in the overall planning of the WRSP Area include:

- Vehicle miles traveled should be reduced through the provision of a mixed-use community that accommodates a wide range of uses.
- A wide range of housing types should be provided that allow for varied housing opportunities.
- Non-vehicular circulation should be emphasized through the creation of compact, interconnected walkable blocks.
- A healthy lifestyle should be promoted through an integrated system of trails and pedestrian connections.
- Narrow street sections should be provided to reduce surface runoff and reduce urban heat island effect.
- Environmentally sensitive areas should be preserved where possible.
- Existing structures and materials at the NVRG should be reused where possible.
- Features such as electric charging stations and solar panels should be provided at key community destinations.

- Incorporate horizontal mixed-use into the community to create a walkable, pedestrian friendly community with less need for automobiles.
- If possible, accommodate a public transit stop centrally located near the core activity hub.

TIER 2 DESIGN GUIDELINES

The second tier involves a commitment to using sustainable green building techniques. The technology exists to create buildings that are smarter, more energy efficient, and healthier than those of the past. Homes and commercial buildings should be built incorporating the finest building and energy saving techniques available. The sustainable green building techniques that might be used in the built form include:

- Passive energy should be used to reduce dependency on mechanized energy systems.
- Recycled, refurbished or sustainability farmed building products should be incorporated.
- A lifecycle analysis should be used when choosing building materials.
- Homes and buildings should meet strict energy performance standards by incorporating ENERGY STAR compliant products such as appliances, building, heating and cooling, and lighting products in an effort to reduce greenhouse gas emissions.
- Reduce the energy consumption in homes by the use of energy efficient design and construction methods. These could include, but are not limited to:
 - High Performance Attics
 - High Performance Walls
 - High Performance HVAC Systems
 - High Performance Windows
 - Tankless Water heaters
 - Cooling Ventilation and house fans
- Consider incorporating a “cool roof” which reduces the amount of heat conducted to the building. This can be achieved by using highly reflective or “cool color” roofing products.
- Outdoor lighting should incorporate fixtures that produce light using light-emitting diodes (LEDs) which use at least 75 percent less energy than incandescent lighting.

- Residences and public/commercial buildings should be designed to accommodate electric vehicle charging stations.
- Incorporating signage on the sustainable design features on the build environment, specifically LEED certified buildings.
- Homebuilders providing buyers with a system option for the homes to include photovoltaic (solar) and solar water-heating systems. Being solar-ready may include:
 - Having a designated solar zone
 - Adequate electric capacity
 - Designated conduit and plumbing paths
- Commercial parking incorporating photovoltaic (solar) panels over the parking, or solar turbines located on the light poles to harvest energy.
- Low impact development principles should be implemented to reduce the impact of the development on the natural environment.
- Incorporate infiltration beds, swales, and basins into the design of a project to allow water to collect and soak into the ground, as required by local agency.
- Utilize pervious or porous surfaces (permeable pavers or blocks) to minimize runoff.
- Encourage the use of rain harvesting or catchment technologies (rain gardens, canisters, etc.)

TIER 3 DESIGN GUIDELINES

The third tier involves a commitment to sustainable landscape techniques. Projects shall incorporate The City of American Canyon Model Water Efficient Landscape Ordinance (MWELo) into the design (See Section 6.6.3 above). The desire to create community in accordance with green building principles influences the decisions regarding the landscape design, recreational programming, and physical layout of each development area. The sustainable landscape techniques that may be used include:

- Drought tolerant plants should be selected to reduce water consumption based upon:
 - Their adaptability to the natural conditions of the site.
 - Protection and preservation of native species and natural vegetation
 - Disease and pest resistance
 - Water-conserving plant and turf species
 - Selection of plants from the East Bay Municipal Water District’s “Plants and Landscape for Summer-Dry Climates” as the primary reference.
- Plant materials should be selected that promote a diversity of flora and fauna.
- Recycled water for landscape should be considered to reduce the consumption and demand of potable water.
- Pursuant to Zoning Ordinance Chapter 19.22.040, the use of turf in new residential projects is limited to the rear yard.

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CHAPTER 7 - CIRCULATION

7.1 Introduction

The Watson Ranch Specific Plan (WRSP), consistent with the City of American Canyon's General Plan Circulation Element, incorporates an array of transportation modes. This is achieved by including a network designed for pedestrians, bicyclists, and motor vehicles. Streets are not only functional systems that allow vehicles to navigate smoothly and safely within the community, but they are also an important building block in creating a rich and vital pedestrian environment. The Circulation Plan (Figure 7.1) provides access to residential, educational, commercial, and recreational destinations. In addition, the circulation plan promotes "complete streets", a transportation policy and design approach that requires streets to be planned, designed, operated, and maintained to enable safe, convenient, and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation. In doing so, the circulation plan provides a safe, functional, and pedestrian-friendly environment for residents and visitors of the WRSP Area community.

The WRSP Area is connected to the city on its western edge by Rio Del Mar East, which is proposed to be extended to serve as an east-west connector between Highway 29 and Newell Drive. From the southeast corner of the WSRP Area, Newell Drive is proposed to extend along the eastern edge of the WRSP Area, enabling a future northern connection of Newell Drive to SR 29 at Green Island Road. From a "roundabout" intersection connecting to the existing Newell Drive, the Newell Drive extension will continue to the north/northeast, while a small local road (Mallen Way) heads north connecting Rio Del Mar East and providing access to the hotel site.

In keeping with the General Plan goal of creating a well-connected community, the WRSP provides connections along its southern edge to Vintage Ranch. Respecting the existing development pattern of Vintage Ranch, connections are made to both existing Summerwood Drive and Rolling Hills Drive. The Rolling Hills Drive connection provides a connection to and from Vintage Ranch to Rio Del Mar East while Summerwood Drive connects the southern residential development parcels within the WRSP Area to Vintage Ranch. Complying with the American Canyon Fire Protection District, the WRSP Area will provide for two means of access during all phases of construction and at build-out.

The main circulation network within the WRSP Area is a modified grid with Rio Del Mar East forming the southern edge of the grid. The interconnected nature of the block pattern disperses traffic so that local streets are pleasant to live on and walk or bike along.

7.2 Goals, Objectives, and Policies

The following goals, objectives, and policies establish a framework to create a circulation network that considers the movement of pedestrians, bicyclists, and vehicles.

Goal 7A – Create a balanced transportation system that moves a variety of users through the community.

Objective 7.1 – Design pedestrian-friendly streets with appropriate traffic calming measures.

Policy 7.1.1 – Sidewalks within residential neighborhoods shall be a minimum of five feet wide.

Policy 7.1.2 – Standard crosswalks shall use accentuated markings, changes of material, pedestrian warning indicators, flashing bollards, flashing lane indicators and/or other flashing warning signs near parks, the NVRG, or major trail crossings.

Policy 7.1.3 – Bulb-outs shall be used at intersections subject to approval of the City of American Canyon and the American Canyon Fire Protection District.

Policy 7.1.4 – All streets, intersections, alleyways and other circulation improvements shall preserve public safety access throughout the WRSP Area.

Policy 7.1.5 – At-grade railroad crossings shall be subject to California Public Utility Commission (CPUC) jurisdiction.

Policy 7.1.6 – All improvements located within the SR 29 State right of way are subject to approval of an encroachment permit by the California State Department of Transportation (Caltrans).

Policy 7.1.7 – The “Tie-in” at Rio Del Mar East and SR 29 (see section 7.7 – Off-Site Improvements) is intended to be consistent with the Caltrans District 4 Bike Plan project referred to as –Nap-29-X12, which will add Class II bike lanes on SR 29 and implement signal/intersection improvements to support bicyclist left turns and movements through the intersection in general.

Goal 7B – Create a street network that enhances the sense of community.

Objective 7.2 – Design streets that are compatible with the character of their neighborhoods.

Policy 7.2.1 – In more-urban conditions, sidewalks may widen and become curb-adjacent. Tree wells with tree grates should be used in these situations.

Policy 7.2.2 – Use of alleyways is encouraged. Specific location of alleyways shall be reviewed and approved as a part of the tentative map and design permit entitlement process for individual neighborhoods.

Policy 7.2.3 – Modified Street sections reflected in the WRSP are approved for use; however, specific location and application will be subject to review and approval as part of subsequent tentative map and design permit entitlement processes.

Goal 7C – Create a pedestrian and bicycle trail network.

Objective 7.3 – Provide bicycle and pedestrian amenities throughout the WRSP Area.

Policy 7.3.1 – Short-term and long-term bicycle parking for non-residential uses shall be provided as required in the City of American Canyon Municipal Code, or as otherwise required in the WRSP.

Policy 7.3.2 – The Napa Valley Vine Trail and River to Ridge Trail segments within the WRSP Area shall incorporate street furniture, wayfinding signage, interpretive elements, high-visibility

crossing treatments, and other amenities where appropriate.

Objective 7.4 – Increase connections through a network of trails, sidewalks, and bike facilities to promote walking and bicycling.

Policy 7.4.1 – The River to Ridge Trail shall be integrated into the WRSP Area along the south side of the Rio Del Mar East alignment fronting the Napa Valley Ruins & Gardens (NVRG) mixed use area, intersecting with the Napa Valley Vine Trail, fronting Newell Park and the Community Plaza/Center, and connecting to Newell Drive.

Policy 7.4.2 – The River to Ridge Trail shall have an improved minimum width of 10 feet for the segment passing through the WRSP Area.

Policy 7.4.3 – The Napa Valley Vine Trail shall be aligned generally along the PG&E gas line easement running north to south through the WRSP Area.

Policy 7.4.4 – The Napa Valley Vine Trail shall have a minimum improved width of 12 feet for the segment within the WRSP Area.

Policy 7.4.5 - Class IV bikeways (cycle tracks) may be implemented along Newell Drive and Rio Del Mar East in lieu of class II bike lanes.

Policy 7.4.6 – An area-wide looped trail system in the WRSP Area shall be established through multiple secondary trail and sidewalk loops provided along open space areas and through neighborhoods.

Objective 7.5 – Promote safety and security on all trail networks.

Policy 7.5.1 – Posting of rules related to bicycle speed limits and trail use etiquette shall be provided along the trail system.

Policy 7.5.2 – When provided, lighting on multi-use trails shall be downward facing.

Goal 7D – Integrate and connect to regional public transportation.

Objective 7.6 – Provide connections to regional networks for a variety of transportation modes.

Policy 7.6.1 – Transit stops shall be provided at key destinations within the WRSP Area.

Goal 7E – Design “Complete Streets.”

Objective 7.7 – Develop a street hierarchy and street system design that considers all potential users and establishes a Complete Streets network in the WRSP Area.

Policy 7.7.1 – Street design shall be consistent with Resolution 2012-72, “Complete Streets Policy of the City of American Canyon.”

Policy 7.7.2 – Class II bike lanes (delineated by on-street striping) shall be provided on all arterial and collector level streets throughout the WRSP Area except where Class I or Class IV bikeways are provided.

Policy 7.7.3 – Sidewalks shall be provided on at least one side of all streets.

Policy 7.7.4 – Approaches to signalized intersections should include bicycle detection devices that are operational and properly marked.

Policy 7.7.5 – Safety and access for bicyclists should be improved at at-grade railroad crossings by providing appropriate enhancements such as proper track structure, safe crossing angles, track fillers, lighting, and adequate warning and guidance information, among other features.

Policy 7.7.6 – Street design shall be consistent with the Circulation Element. Modifications not approved as part of this Specific Plan (see Section 7.3) shall be subject to review and approval through the tentative map and design permit approval process.

Goal 7F – Provide an open street network with connectivity to adjoining neighborhoods.

Objective 7.8 – Wherever possible, local street connections to adjoining neighborhoods should be made to provide local access to the Napa Valley Ruins and Gardens (NVRG) Area.

Policy 7.8.1 – Rolling Hills Drive shall be connected as a local street to Rio Del Mar East. Where necessary, traffic calming features shall be utilized to avoid “cut-through” traffic.

Policy 7.8.2 – Preserve the potential for the City to achieve a future at-grade crossing connection at the terminus of South Napa Junction Road until such time the City has completely abandoned the option as a secondary access into the NVRG.

7.3 Street Sections

This section describes the range of street classifications that make up the components of the overall circulation system. The Circulation Plan (Figure 7.1) delineates the location of the backbone street layout for the WRSP Area. The backbone circulation plan shows the general alignment of the primary streets. The locations of local streets and smaller streets will be determined at time of tentative map approval. Final street layouts may vary from street configurations illustrated below in this section, and may be determined at time of tentative map approval. The street sections described below include a series of “modified” street sections that are permissible to use within the WRSP Area.

In keeping with the sustainability goals of the WRSP, Low impact Development (LID) techniques may be used to reduce impacts of development on the natural environment. To meet this goal, planting strips or landscape medians may be used to treat stormwater runoff. The use of parkway strips or medians to treat stormwater may help in meeting the City’s MS4 Permit requirements, which is the regulatory framework to address appropriate source control, site design, and stormwater treatment measures in new development and redevelopment projects to address pollutant discharges and prevent increases in runoff flows.

In the following Street Section Graphics certain text may be abbreviated to fit graphically. These are:

- FOC delineates Face of Curb Dimensions
- BOC delineates Back of Curb Dimensions
- ROW delineates the Right of Way

Street sections illustrated represent the range of proposed streets within the WRSP Area. Specific locations and dimensions may be further refined or modified through the tentative map process (See Policy 7.7.6).

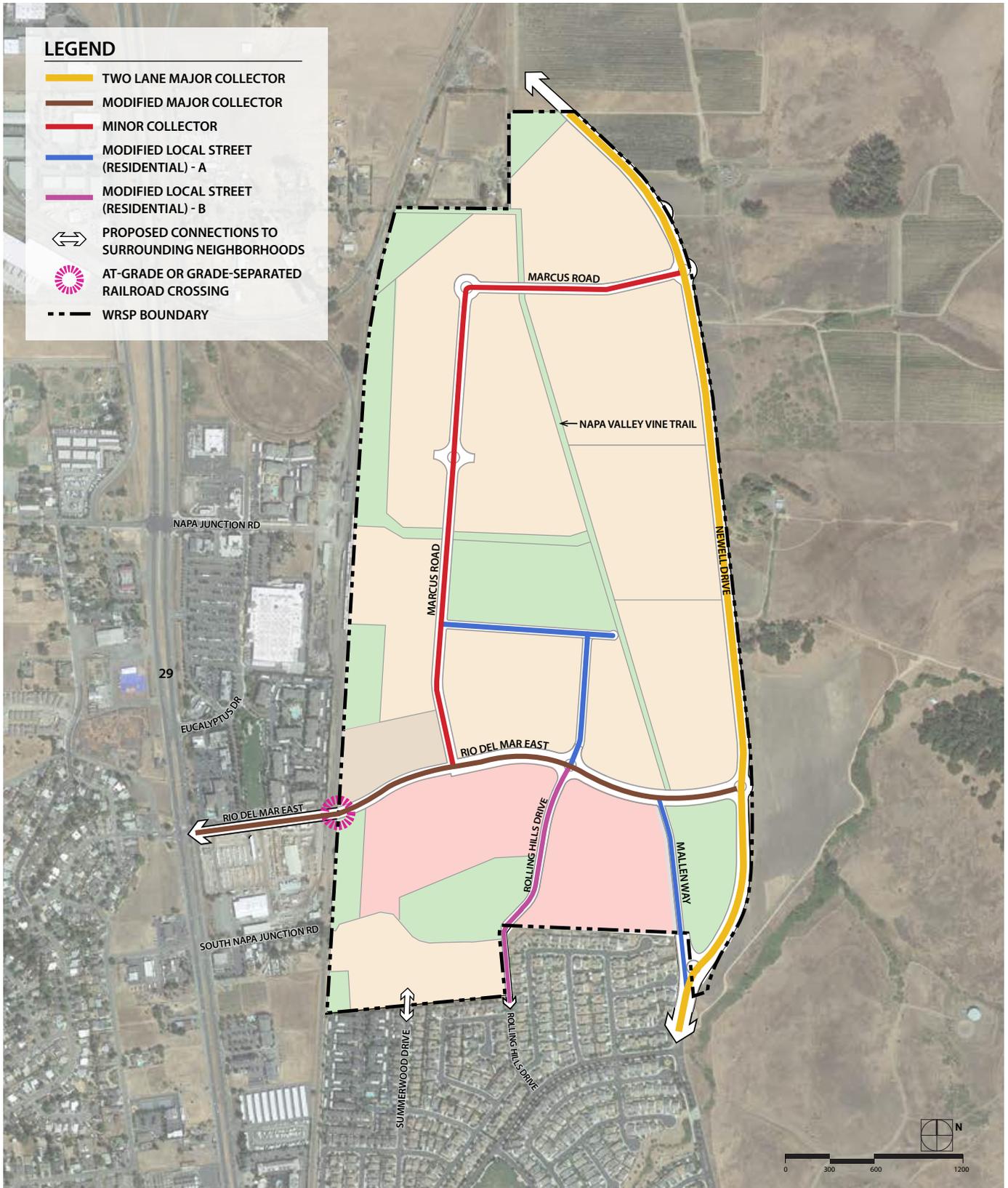


Figure 7.1: Circulation Plan

7.3.1 TWO LANE MAJOR COLLECTOR (NEWELL DRIVE)

This major collector consists of two 12-foot-wide lanes of travel, a 14-foot-wide median, 5-foot-wide Class II bike lanes, 8-foot-wide parkways, and 8-foot-wide sidewalks.

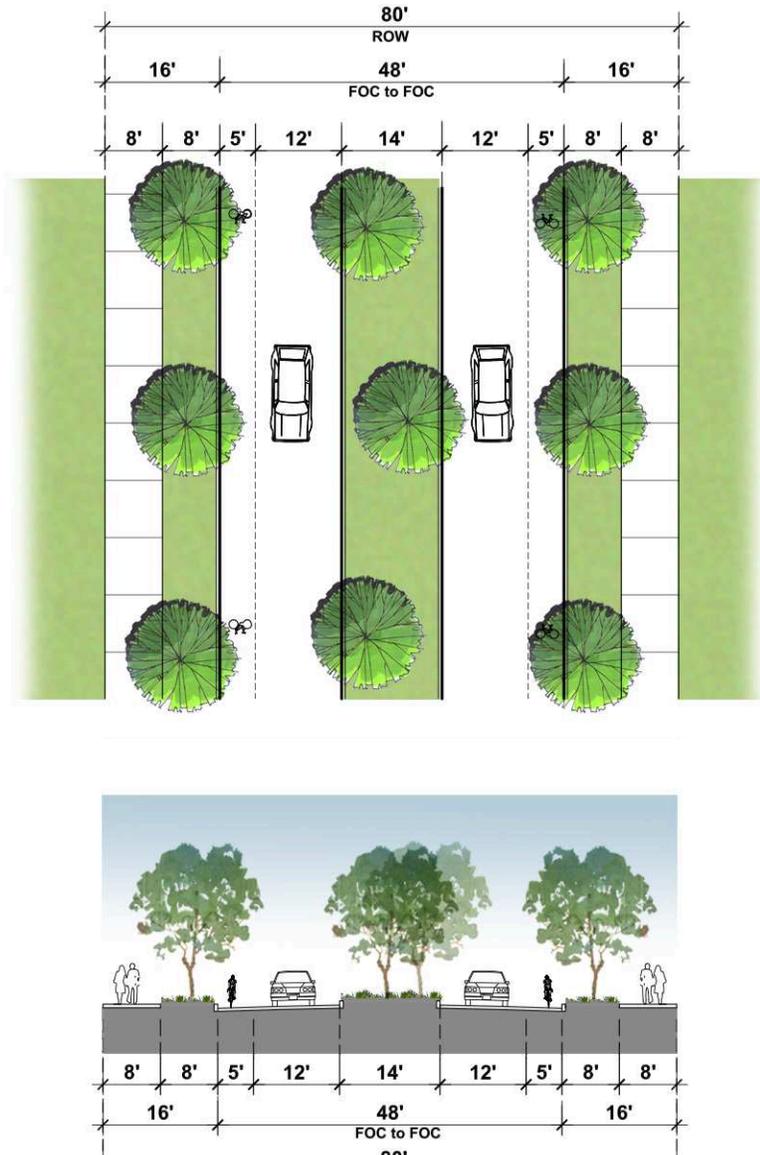


Figure 7.3.1: Two Lane Major Collector (Newell Drive)
Subject to modification

7.3.2 MODIFIED MAJOR COLLECTOR

This two lane modified collector is proposed as an east-west extension from the existing Rio Del Mar linking to the extension of Newell Drive. The modified major collector street consists of a 69.5-foot-wide road section within a 108.5-foot-wide right-of-way. The paved section will accommodate two 12-foot-wide travel lanes and a 14-foot-wide median that accommodates turn lanes. The roadway has a vertical curb and gutter, and curb returns have a 20-foot radius. On the north side, there is a 5-foot-wide on-street Class II bike lane and 8-foot-wide parking bays. Diagonal parking is provided on the south side along

the frontage of the NVRG. Alternately, “back-in” diagonal parking may be considered along the NVRG frontage. An 8-foot-wide sidewalk is provided on the north side of the street and separated from the travel lanes by an 8-foot-wide parkway strip. The River to Ridge Trail runs along the south side of the road as a 12-foot-wide multi-purpose trail for pedestrians and bicycles, separated from the parking areas by a minimum 8-foot-wide parkway strip. Where this street runs along the NVRG, the paving may extend to the curb, thus eliminating the parkway strip and forming more of a promenade atmosphere with street trees in tree grates. (See Figure 7.3.2a and Figure 7.3.2b. Note that

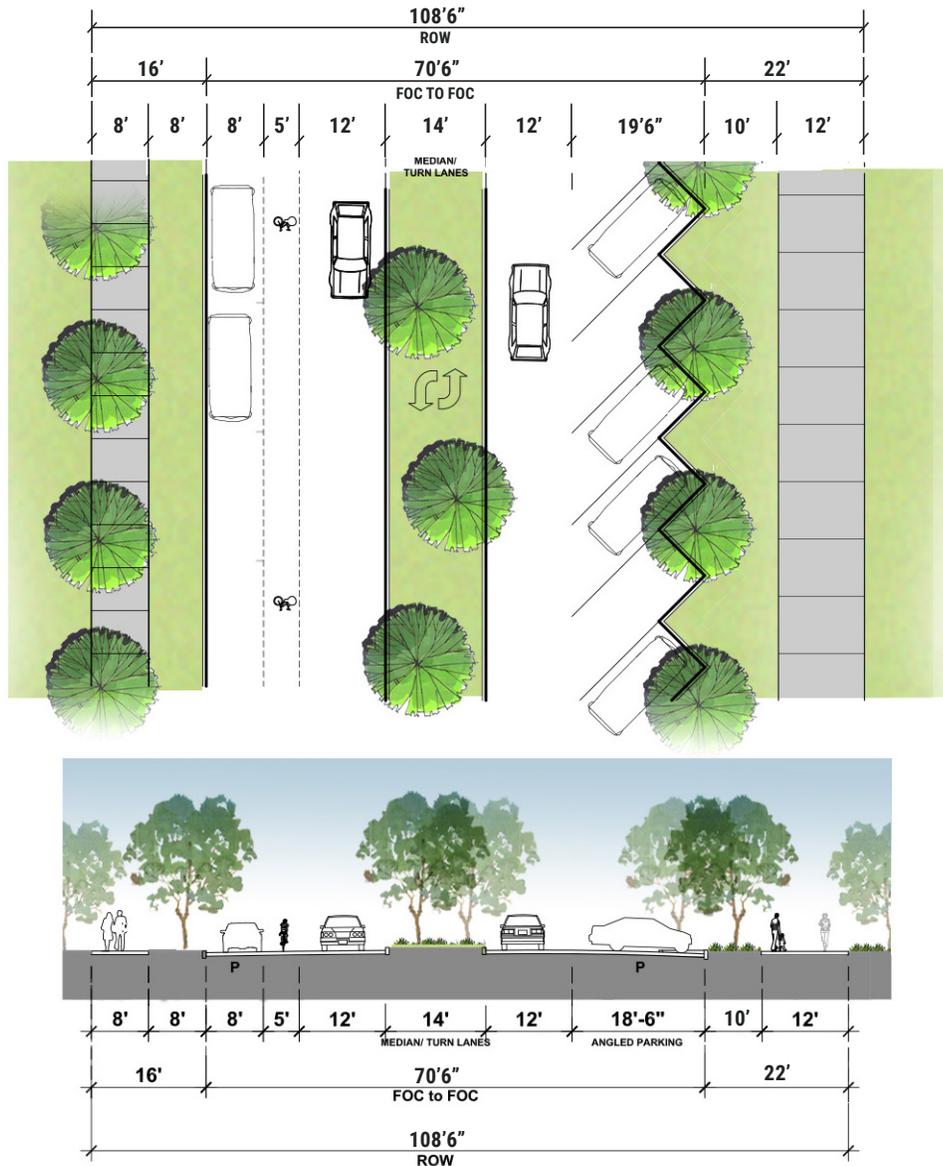


Figure 7.3.2a: Modified Collector (Rio Del Mar extension - in front of NVRG)
Subject to modification

these figures represent typical street configurations; they are not indicative of the ultimate configuration of approved or future-approved modified major collector roadways such as Rio Del Mar East.)

Along the western edge of the site, Rio Del Mar East crosses the existing railroad. The Rio Del Mar East railroad crossing will be an at-grade or grade-separated crossing. Parking bays are eliminated for this portion of the roadway. Along the western edge of the site where not adjacent to the NVRG, the diagonal parking will become parallel spaces.

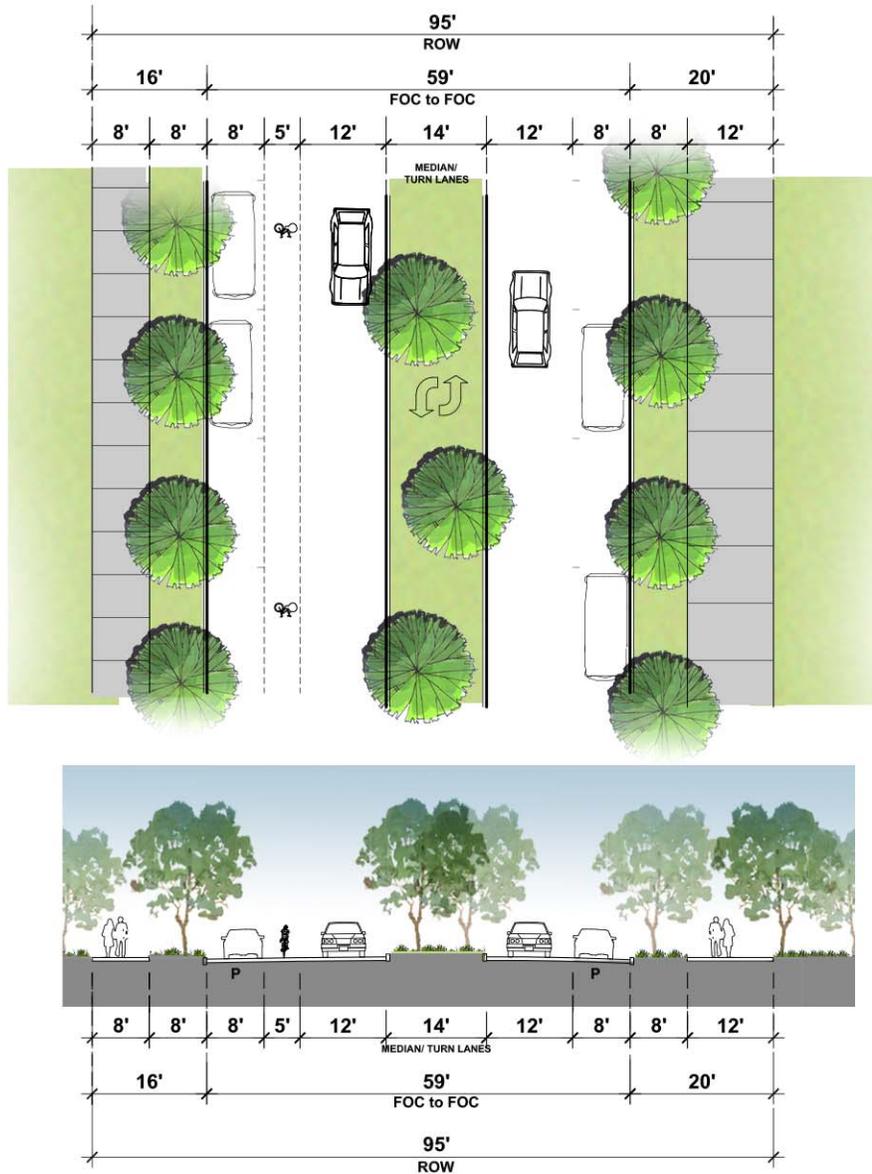


Figure 7.3.2b: Modified Collector (Rio Del Mar extension - from Napa Valley Vine Trail crossing to Newell Drive)
Subject to modification

Figure 7.3.2c shows the conceptual at-grade crossing for Rio Del Mar East, and Figure 7.3.2d shows the grade-separated concept. Figure 7.3.2e shows the conceptual layout for the off-site portion of Rio Del Mar East from the western property boundary to SR 29. Lane configurations and alignments are conceptual and will be refined at the time of final design.

Rio Del Mar East railroad crossing at grade or grade separated is subject to C.P.U.C. approval. Figure 7.3.2c and 7.3.2d are conceptual only and may be subject to change.

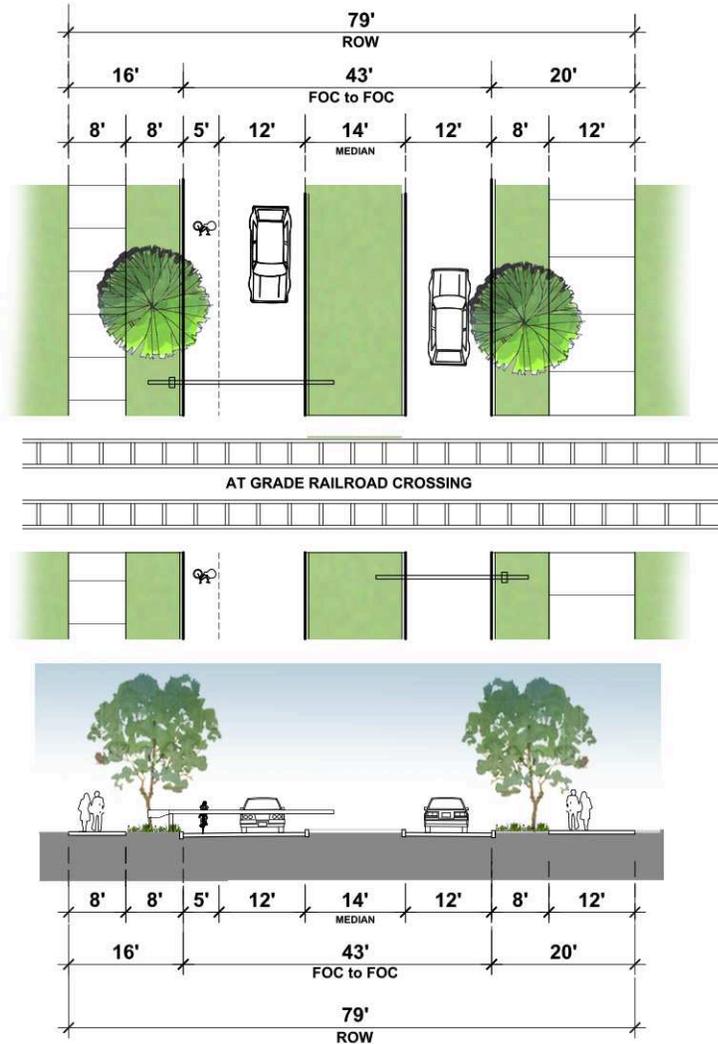


Figure 7.3.2c: Modified Collector (Rio Del Mar extension - at grade crossing)

Final grade crossing design is subject to C.P.U.C. approval and must meet C.P.U.C. regulations for vehicular, bicycle and pedestrian safety crossing features. Figure 7.3.2c is conceptual only and may be subject to change.

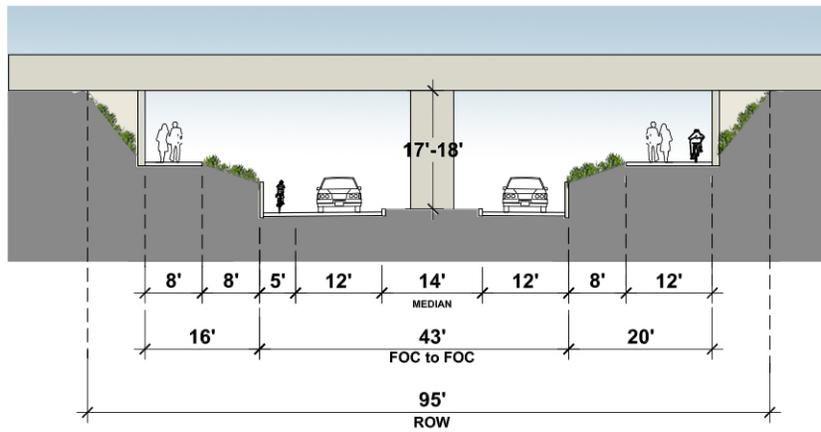
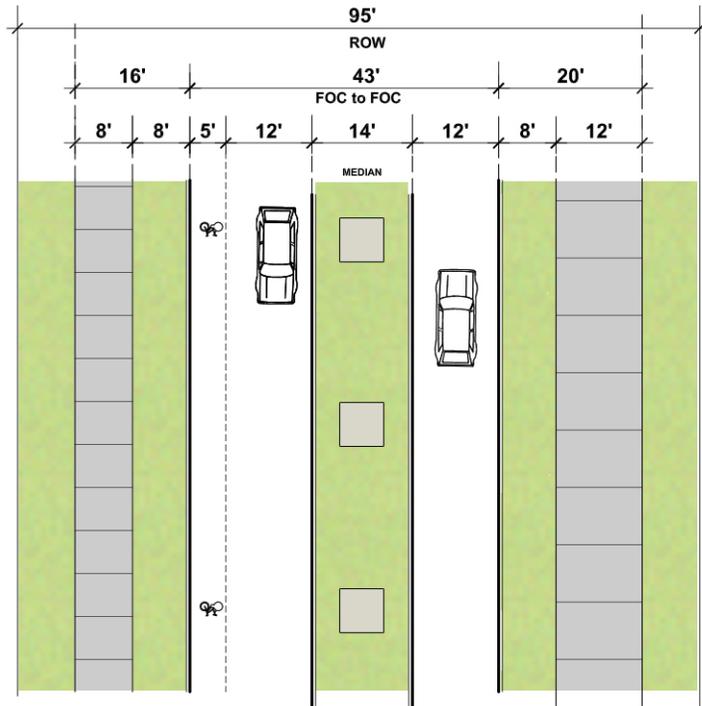


Figure 7.3.2d: Modified Collector (Rio Del Mar extension - RR underpass)

Final railroad crossing is subject to C.P.U.C. approval and must meet C.P.U.C. regulations for vehicular, bicycle and pedestrian safety crossing features. Figure 7.3.2d is conceptual only and may be subject to change.

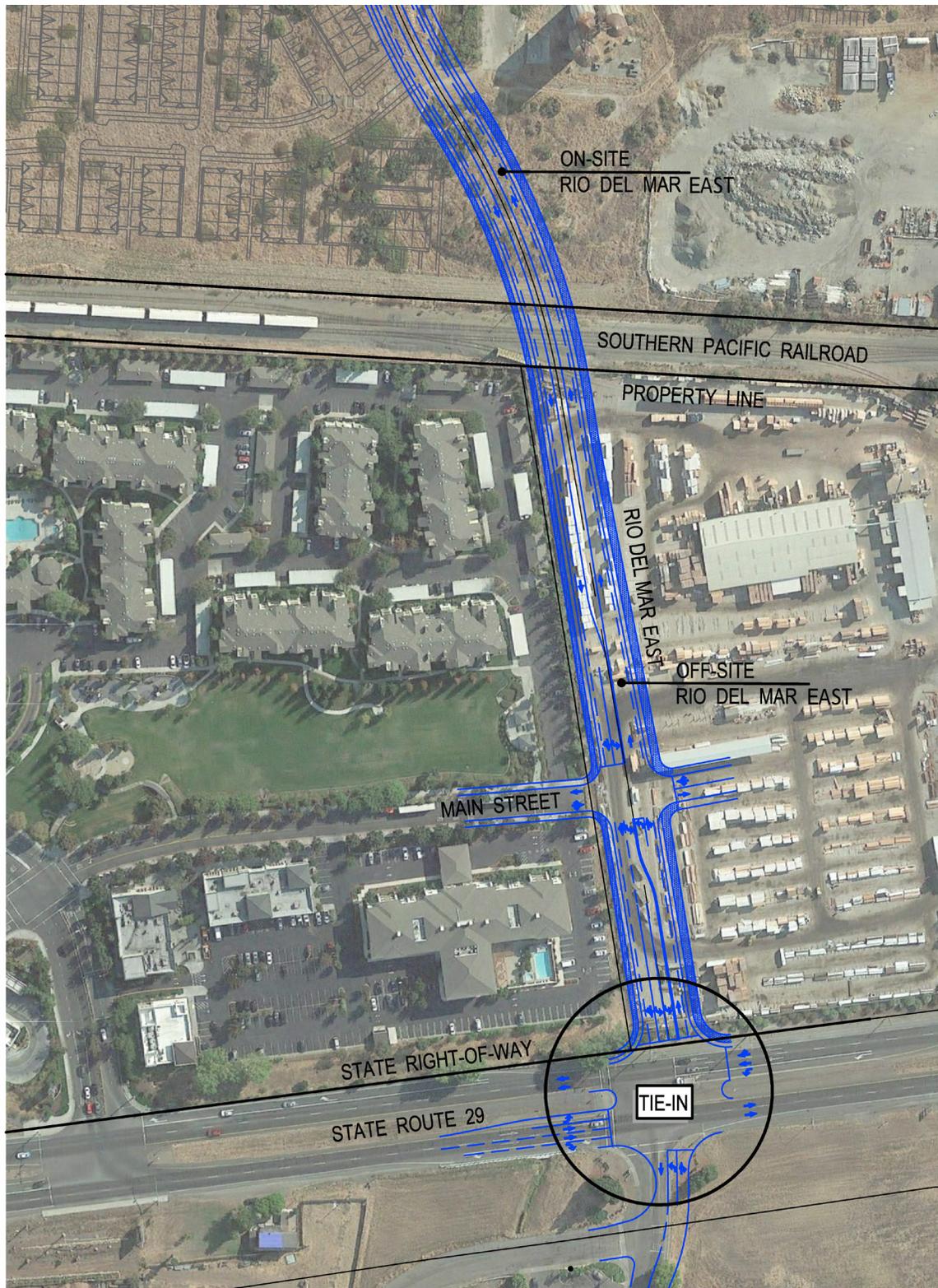


Figure 7.3.2e: Modified Collector (Rio Del Mar extension - from SR 29 to western property boundary)
 Subject to modification

7.3.3 MINOR COLLECTOR

Marcus Road is a proposed two lane Minor Collector providing a primary connection through the project area. Marcus Road runs northward from Rio Del Mar East and then curves eastward to a terminus at Newell Drive.

The standard Minor Collector street section consists of a 50-foot wide road section within a 74-foot wide right-of-way. The paved section will accommodate two 12-foot-wide travel lanes, 5-foot wide on-street bike lanes, and 8-foot-wide parking bays on either side. The roadway has a vertical curb and gutter and curb returns have a 20-foot radius.

Six-foot-wide sidewalks are provided on both sides of the minor collector and are each separated from travel lanes by a 6-foot-wide parkway strip. Where the parkway strip might be utilized for stormwater treatment, an increase in width may be necessary.

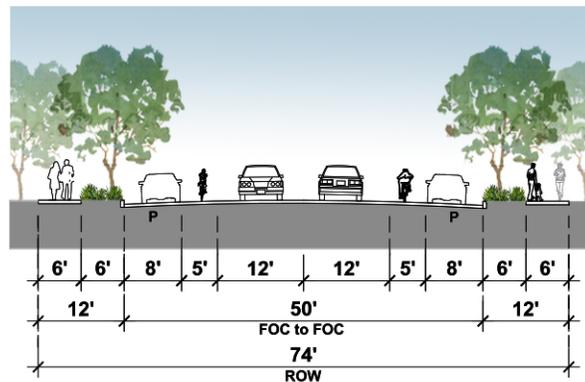
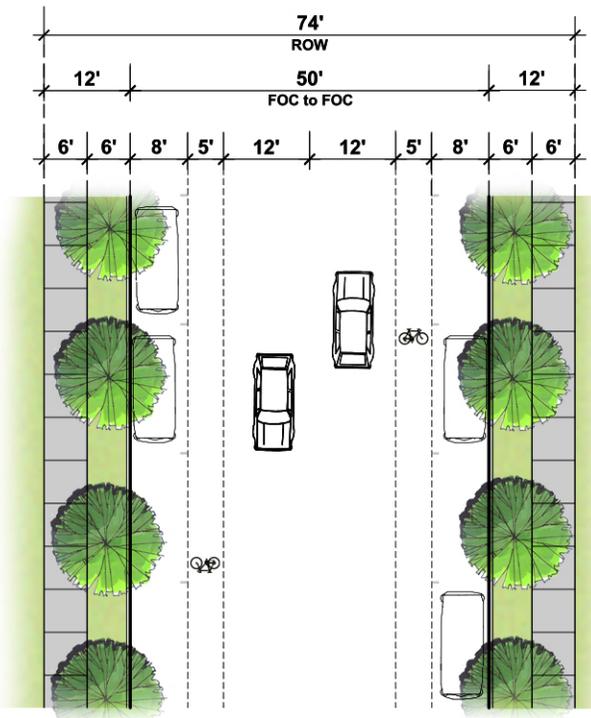


Figure 7.3.3: Minor Collector
Subject to modification

7.3.4 LOCAL STREETS (RESIDENTIAL)

The Local Street (Residential) classification consists of a 40-foot-wide road section within a 64-foot-wide right-of-way. The paved section can accommodate two travel lanes and on-street parking on both sides.

The roadway has a vertical curb and gutter, and curb returns have a 20-foot radius. 6-foot-wide sidewalks provided on both sides and are separated from parking areas by a 6-foot-wide parkway strip.

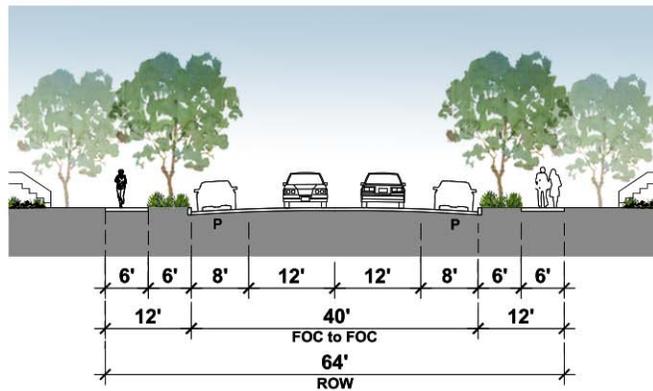
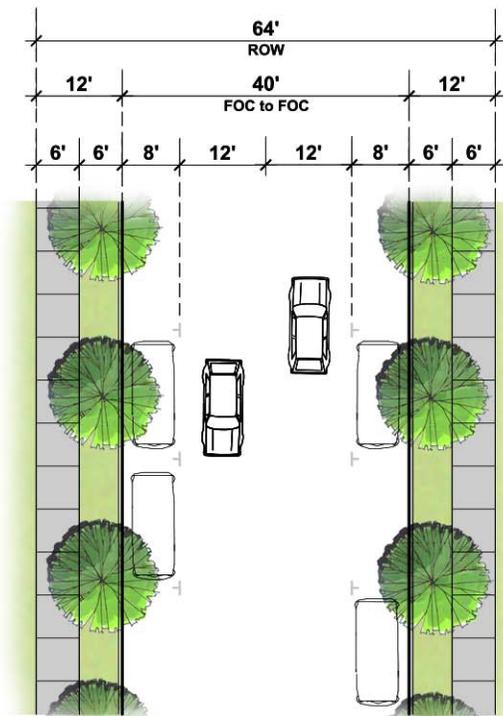


Figure 7.3.4: Local Streets (Residential)
Subject to modification

7.3.5 MODIFIED LOCAL STREET (RESIDENTIAL) - A

The Modified Local Street (Residential) - A is a street that is unique to the WRSP Area and provides access within residential neighborhoods. This street has a 36-foot-wide paved section within a 58-foot-wide right-of-way. The paved section accommodates two travel lanes with on-street parking on both sides.

The roadway has a vertical curb and gutter, and curb returns have a 20-foot radius. 5-foot-wide sidewalks are provided on both sides and are separated from the parking areas by a 6-foot-wide parkway strip.

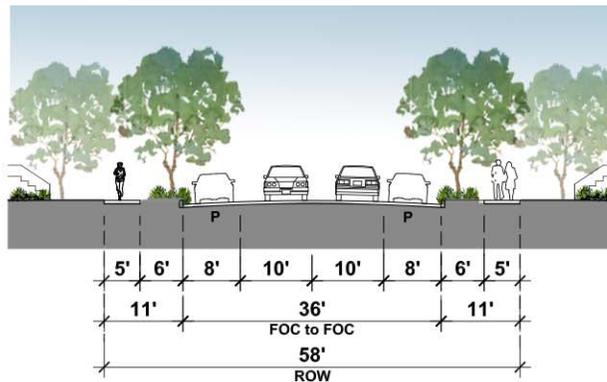
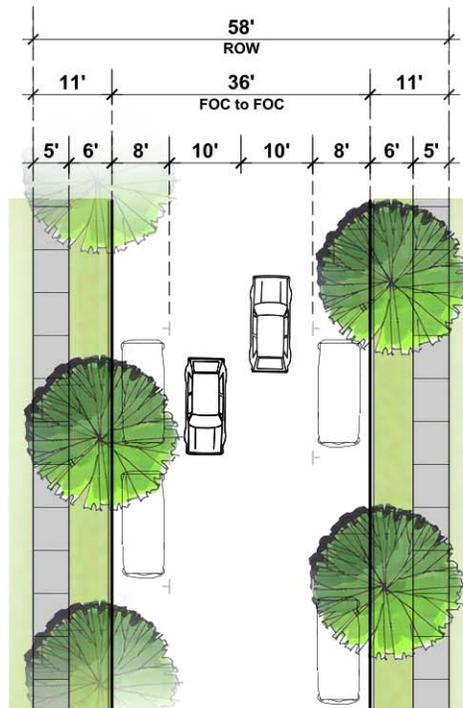


Figure 7.3.5: Modified Local Street (Residential) - A
Subject to modification

7.3.6 MODIFIED LOCAL STREET (RESIDENTIAL) - B

The Modified Local Street (Residential) - B is a street that is unique to the WRSP Area and provides connection from development parcels along the Southern boundary to Vintage Ranch and from Vintage Ranch to Rio Del Mar East. The street has a 32-foot-wide paved section within a 58-foot-wide right-of-way. The paved section accommodates two travel lanes with on-street parking on one side.

A 10-foot wide sidewalk is provided on one side separated from the travel lane by a 6-foot wide parkway strip. A 10-foot wide landscape parkway is provided opposite the side containing the 10-foot sidewalk.

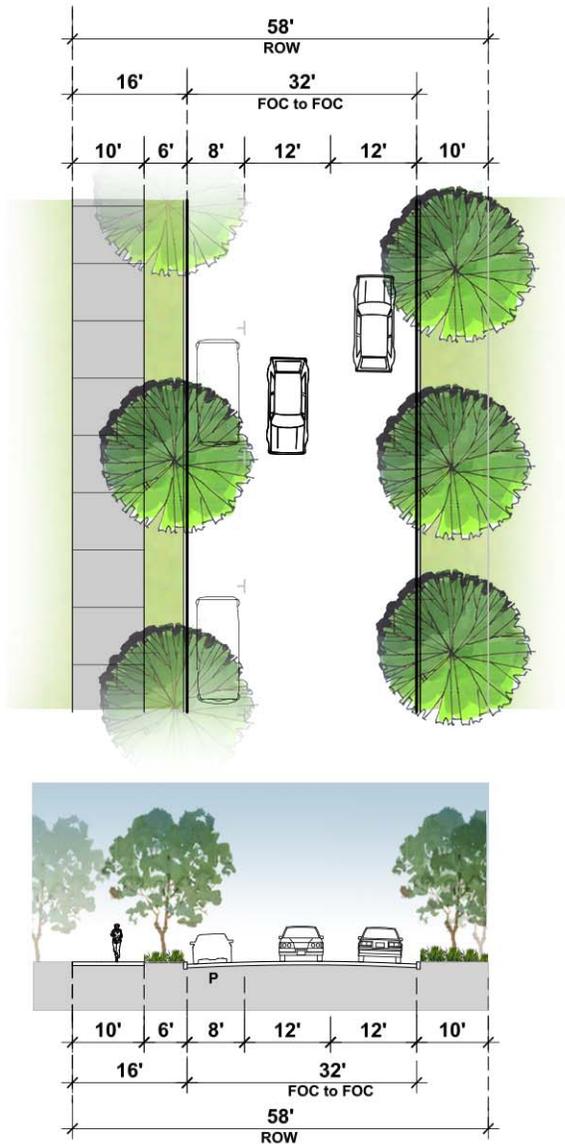


Figure 7.3.6: Modified Local Street (Residential) - B
 Subject to modification

7.3.7 CUL-DE-SAC

The cul-de-sac is utilized at the end of a residential street primarily to serve as a turnaround.

The bulb of the cul-de-sac is located in a right-of-way which is 114-feet wide in diameter. The paved driving surface is 90-feet wide in diameter. The curb radius to the connecting neighborhood street is 17-feet. No parking is permitted within the cul-de-sac. A 6-foot-wide sidewalk that is separated by a 6-foot-wide planting strip is provided.

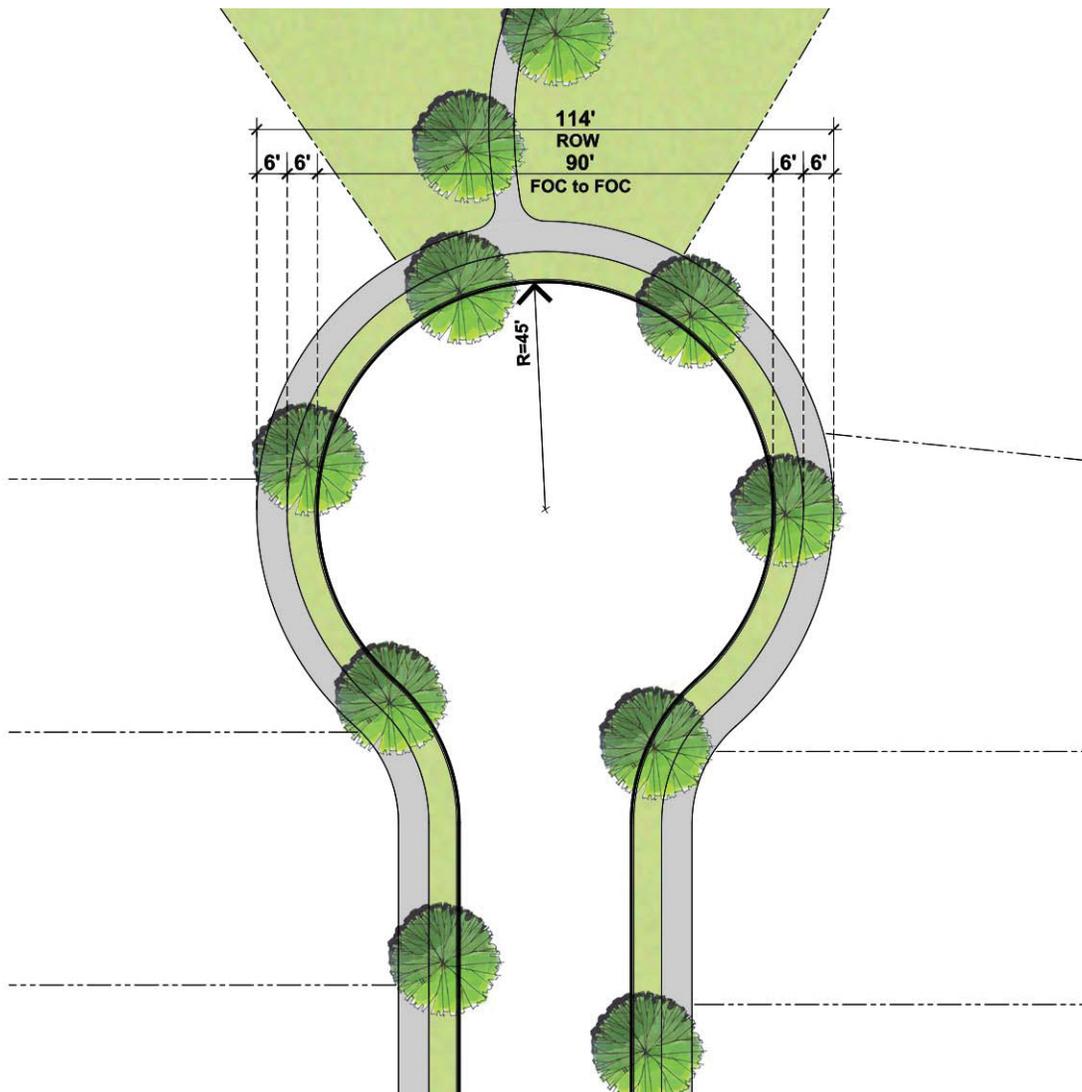


Figure 7.3.7: Cul-de-sac
Subject to modification

7.3.8 RESIDENTIAL ALLEY

Alleys are generally located behind residential lots, providing service access and resident vehicular access to garages. Alleys have a minimum travel way width of 20 feet within an access easement or right-of-way. Garage doors must be set back a minimum of 14 feet from the centerline of the paved surface. No resident or guest parking is allowed within the alley except in designated parking spaces or full length driveway aprons. Alleys are encouraged to be used extensively to minimize the garage dominant streetscape and encourage pedestrian/bicycle activity.

Points of access and certain segments may vary from this minimum standard to accommodate public safety vehicles and garbage trucks.

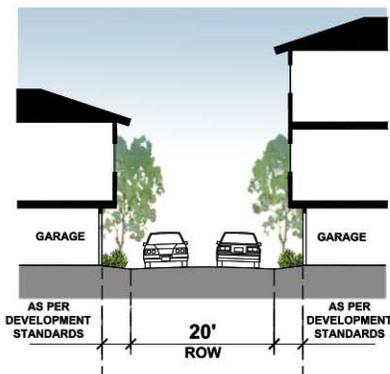
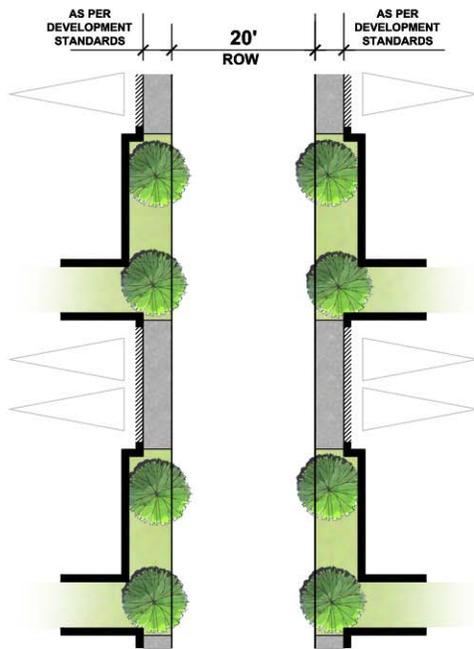


Figure 7.3.8: Residential Alley
Subject to modification

7.4 Traffic Calming

The Institute of Transportation Engineers defines traffic calming as “Traffic calming involves changes in street alignment, installation of barriers, and other physical measures to reduce traffic speeds and/or cut-through volumes, in the interest of street safety, livability, and other public purposes.” To promote walkability and to provide a safe pedestrian travel route within the community, various traffic calming measures could be adopted. The WRSP implements bulb-outs at intersections and narrowing at important pedestrian/vehicular intersection points, such as mid-block crossings for trail crossings, as shown in Figure 7.4.

In addition, the following traffic calming measures may also be considered, and their review and approval will be considered at tentative map review.

1. Vertical Deflection (Speed Humps; Speed Tables; Raised Intersections) Small changes in elevation can be used to slow vehicles. A speed hump is typically 12-14 feet in length and a height less than 4 inches, while a speed table is at least 22 feet in length rising up to 6 inches, with a flat central area at least 10 feet in length. Speed humps and tables work well in residential neighborhoods and are often used in series. Speed tables are sometimes used at midblock pedestrian crossings, with the crosswalk located across the flat area. Speed humps and speed tables are not used near intersections; rather, the entire intersection may be raised, with bollards to delineate sidewalks and pedestrian zones, making the entire intersection pedestrian-friendly. Special modifications must be made for drainage in the case of vertical deflections. Vertical deflections are not preferred on streets with significant (>8%) grade.
2. Horizontal Shifts (Traffic Circles; Chicanes) Traffic circles are raised islands installed at intersections of local or collector streets with one lane each direction entering the intersection; they should be adequately sized and configured to accommodate turning conditions for large vehicles such as buses or trucks. Chicanes are a series of at least three alternating curb extensions used at midblock locations only. Careful design must ensure that speeding cannot occur by drivers cutting straight paths across the center line. Chicanes can be created using on-street parking, but are often difficult to design due to driveway access. Horizontal shifts are sometimes preferred by emergency response teams over vertical deflections, and can provide opportunities for landscaping.

3. Other (Change in Material / Texture; Pedestrian Actuated Signals) Stamped asphalt, brick pavers, and rumble strips can be used to capture the attention of motorists. They are successfully used in conjunction with crosswalks or raised intersections / speed tables, and provide additional emphasis to mixed-use or commercial areas where there is also an increase in pedestrian activity. For especially dangerous geometries or high-volume / high-speed roads, pedestrian-actuated signals with flashers can be used at intersections or at mid-block locations.

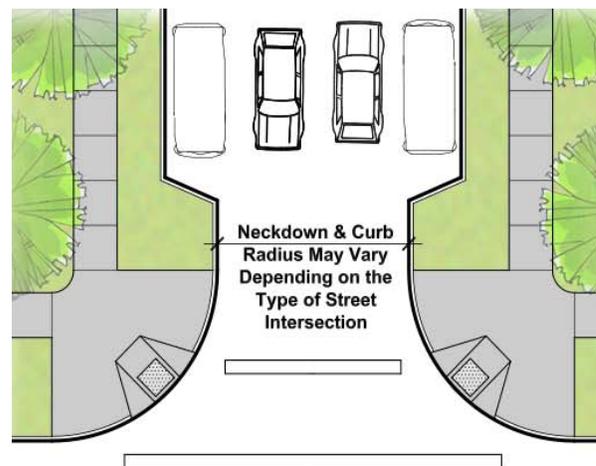


Figure 7.4: Intersection Neckdown/Bulb-out
Subject to modification

7.5 Pedestrian and Bike Circulation

The pedestrian and bikeway network is an important component in ensuring connectivity and promoting non-vehicular travel within the WRSP Area. The network has been designed as a truly integral part of the community, providing easy and efficient access to the NVRG, parks, and other amenities. This network of bicycle, multi-purpose trail, sidewalk and paseo connections within the community links neighborhoods together and provides connections to surrounding areas. The WRSP Area is locally and regionally connected to the rest of the City of American Canyon by the crossing of two regional trails. Pedestrian connection opportunities are provided to adjacent uses and allow for easy access into the regional trail and bicycle network that is currently existing or planned in the City. Figure 7.5 illustrates the proposed bike and pedestrian circulation system.

7.5.1 SIDEWALKS

Sidewalks form day-to-day linkages throughout the community. All streets within the WRSP Area will have sidewalks on at least on one side of the street. The character of the sidewalks should reflect the adjacent land use. For example, sidewalks within residential neighborhoods will be a minimum of 5 feet wide and separated from the street by a landscaped parkway strip. In locations where a more-urban character is desired such as in front of row town homes, or adjacent to retail or commercial uses, sidewalks may widen and become curb-adjacent with street trees located in tree grates. This flexibility will create projects where the pedestrian experience responds to its context.

7.5.2 PEDESTRIAN PATHS AND BIKEWAYS

Pedestrian and bicycle circulation forms the backbone of the WRSP. This network creates linkages to all portions of the WRSP Area as well as connection opportunities to the regional trail system. The proposed pedestrian and bicycle network contains the following components:

Community Trails System – The Community Trails network system provides safe and convenient access to parks, natural open spaces, neighborhoods, the NVRG, and regional trails. The Community Trails system is comprised of a series of trails that provide different experiences within the WRSP Area. Loops provide route choices to destinations, offer differing experiences along their alignments and allow users to fit a route to the time they have available and their

recreational needs. The primary Community Trail systems within the WRSP Area are:

Napa Valley Vine Trail - The Napa Valley Vine Trail is a proposed major regional multi-use trail from Calistoga to Vallejo. A portion of this proposed trail route passes through the WRSP Area thus providing a valuable regional non-motorized connection. The proposed route runs north-south along the entire length of the property. Portions of the Vine Trail utilize the existing gas easement line that traverses the WRSP Area. Other local community trails will connect to the Napa Valley Vine Trail thus providing opportunities for various loops within and outside the WRSP Area. Figure 7.6 shows the Napa Valley Vine Trail section.

River to Ridge Trail – The River to Ridge Trail is the proposed trail connecting the western part of the City to the open space areas to the east of Newell Drive. The multi-use trail will connect to the WRSP Area at its western boundary on Rio Del Mar East and traverse east-west through the entire width of the site. The trail will provide connections to the NVRG, Newell Park, the Napa Valley Vine Trail, and local multi-use trails. Figure 7.7 shows the River to Ridge Trail alignment.

Multi-Use Trails – In addition to the regional trail systems the WRSP Area also has a network of multi-use trails that connect various open space and community gathering spaces. The multi-use trails run along the western edge of the WRSP Area linking dual use basins, wetlands, parks, and other destinations within the WRSP Area. Refer to Figure 7.8 and Figure 7.9 for multi-use trail alignments.

Class II Bike Lanes – Major streets within the WRSP Area have Class II bike lanes. Class II bike lanes are designated 5-foot-wide (minimum) on-street bike lanes demarcated with a solid white stripe, allowing for a safe and unrestricted passage for bicyclists.

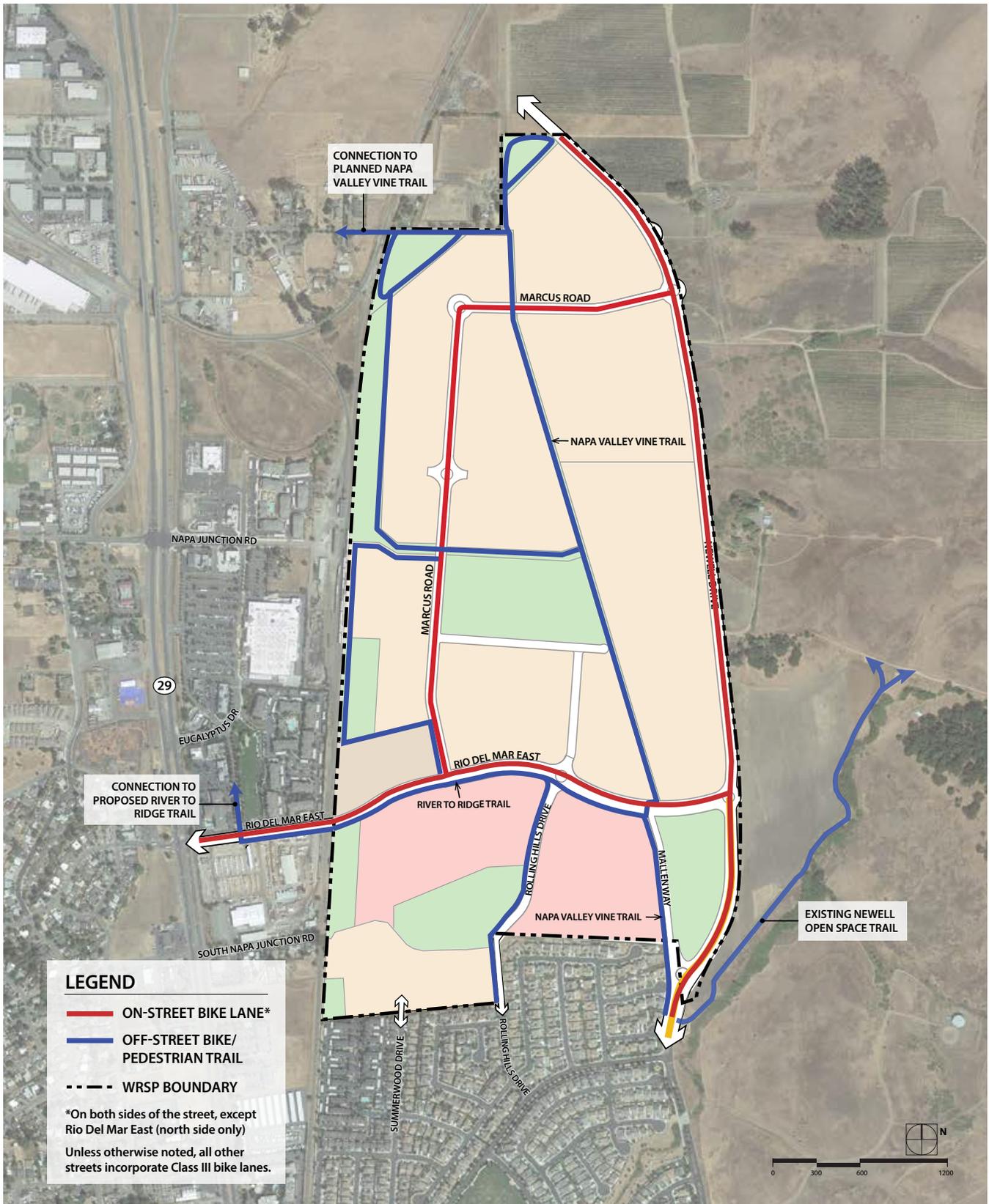


Figure 7.5: Pedestrian and Bicycle Circulation

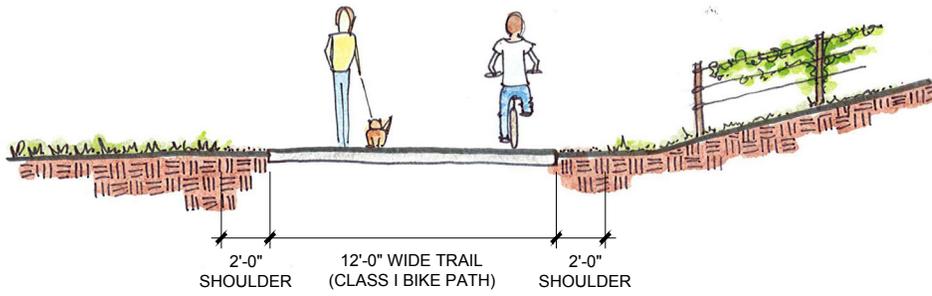


Figure 7.6: Napa Valley Vine Trail Section
Subject to modification

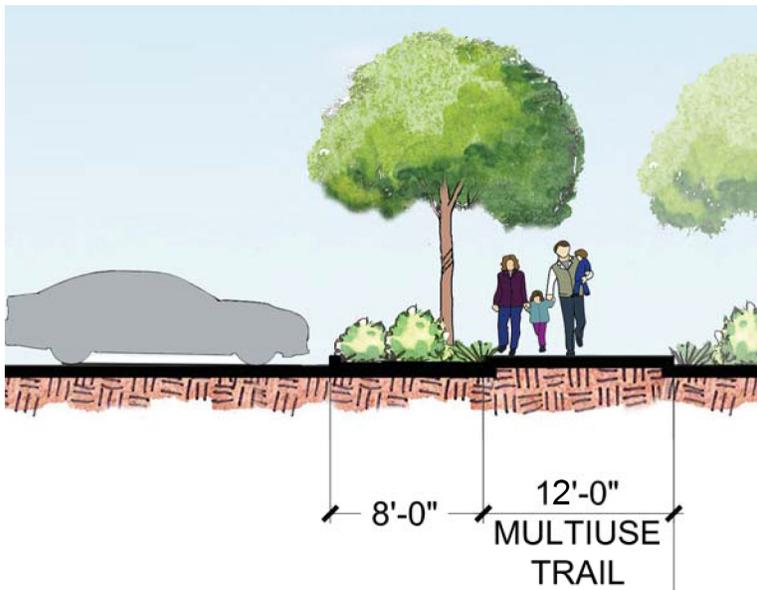


Figure 7.7: River to Ridge Trail Section
Subject to modification

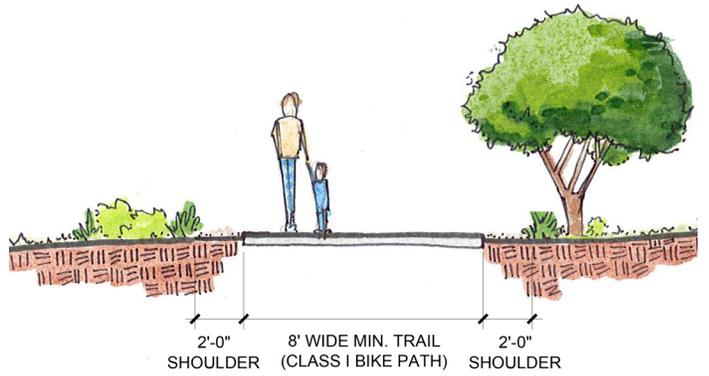


Figure 7.8: Multi-Use Trail Section
Subject to modification

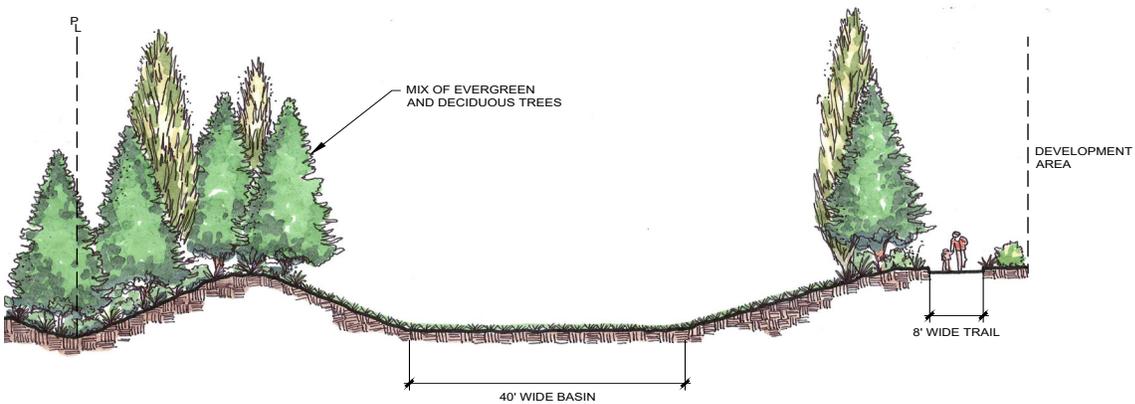


Figure 7.9: Multi-Use Trail Section along Detention Basins
Subject to modification

7.6 Public Transit

American Canyon Transit (ACT) provides a deviated fixed route bus service aimed at getting local residents to shopping and healthcare facilities within American Canyon. ACT also connects to the Napa VINE Routes 11 and 29 allowing American Canyon residents and visitors to travel to Napa, Vallejo, the Vallejo Ferry and even BART in the East Bay. The VINE system serves all the cities in the Napa Valley and connects with SolTrans, Sonoma County Transit, Petaluma Transit, Lake County Transit, BART, the Vallejo Ferry, Amtrak, and Greyhound.

While the WRSP Area is not currently served by ACT, there is the potential for the extension of routes to include key destinations within the WRSP Area. Potential transit stops within the WRSP Area could be located in proximity to the NVRG, hotel site, Newell Park, and the Community Center. To preserve this potential opportunity, the major roads within the WRSP Area are based on the City of American Canyon's street standards and have sufficient lane widths and right-of-way to accommodate transit service and transit stops. Potential future routes could include extending existing bus routes or new routes through the WRSP Area on Rio Del Mar East and/or along Newell Drive. Figure 7.10 shows possible future bus routes and potential transit stop locations.

As noted in Section 1.3.4 of the WRSP, the WRSP Area is proximal to the Broadway District Specific Plan (BDSP) Area, which is a Priority Development Area (PDA) as designated by the Association of Bay Area Governments (ABAG) and Bay Area communities as areas for new growth. PDAs are considered foundational for sustainable regional growth and Plan Bay Area. PDAs emphasize housing near transit. The WRSP represents an extension of that growth being located less than a quarter mile from the SR 29 corridor. Although separated from the PDA by the Railroad line and commercial properties fronting SR 29, the WRSP provides a key point of access to the BDSP Area and SR 29 through the extension of Rio Del Mar East. This dramatically enhances connectivity to the primary transit corridor in American Canyon. Further, the potential transit stop shown at Rio Del Mar East and the NVRG frontage is a quarter mile from approximately 46% of the units (+/- 500) within the WRSP Area as well as the NVRG mixed uses and the hotel site.

7.7 Off-Site Improvements

Off-site improvements include major roadway connections from the WRSP Area to existing American Canyon roadways and several local street connections to enhance local connectivity. A connection to the WRSP Area at the existing Highway 29/Rio Del Mar East intersection will include surface improvements, roadway widening, and an at-grade or grade-separated railroad underpass to access the WRSP site.

The associated off-site improvements for the connection of Rio Del Mar East to SR 29/Broadway will include improvements to Rio Del Mar East from the western project boundary up to SR 29 and the intersection of Rio Del Mar East and SR 29 referred to as the "Tie-in" (see Figure 7.3.2e). Improvements at the Tie-in are intended to comply with California Department of Transportation (Caltrans) Encroachment Permit requirements, which may include the construction or financial contribution toward construction of the bicycle and pedestrian improvements at this intersection, identified in the Caltrans District 4 Bike Plan (2018) as NAP-29-X12. All improvements within the SR 29 State right-of-way requires an encroachment permit that is issued by Caltrans.

Newell Drive and Mallen Way will be connected to the south where Newell Drive currently terminates just north of Donaldson Way. Rolling Hills Drive will be connected to the south where it currently terminates just north of Terrazzo Lane.

The off-site improvements are shown on Figure 7.11.

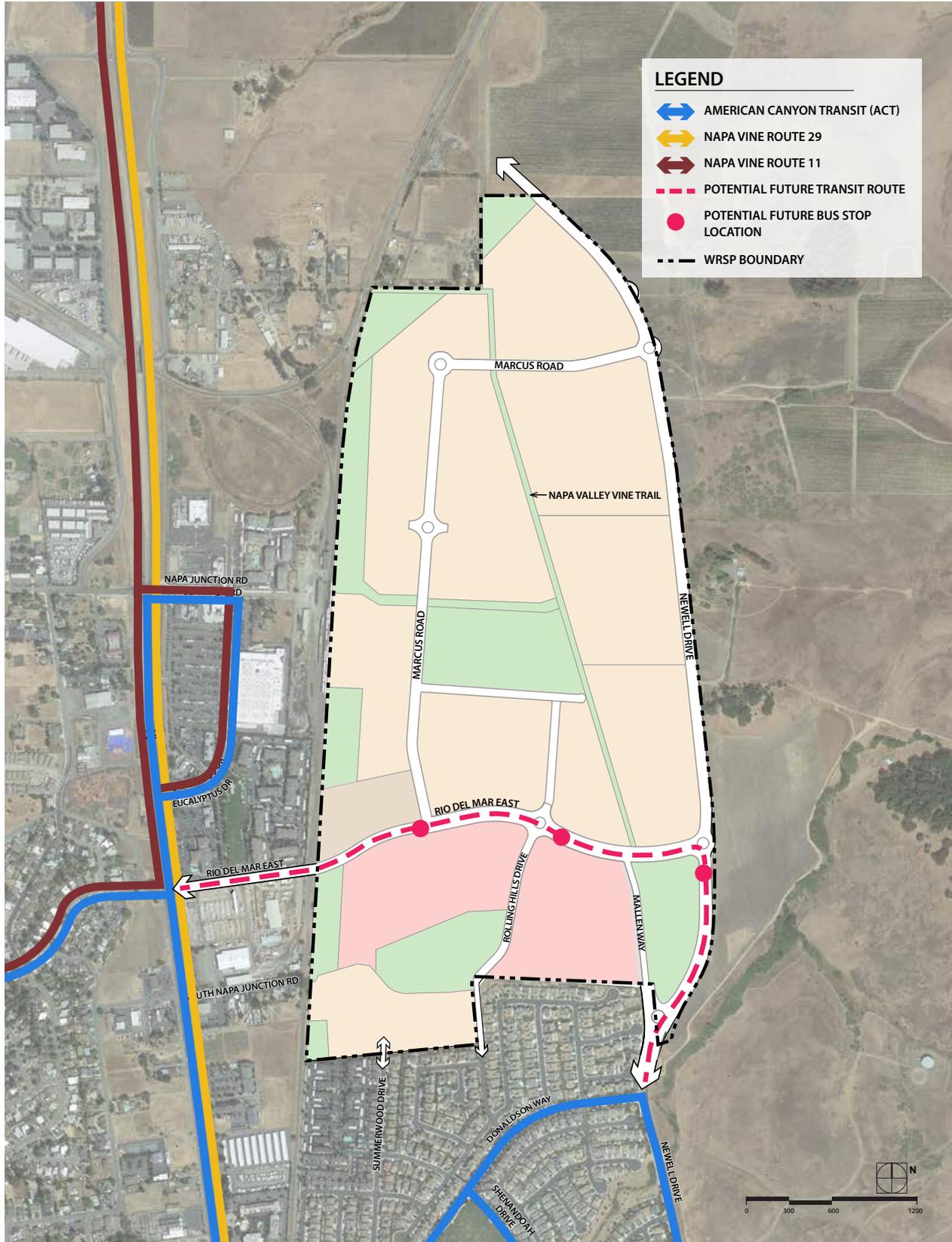


Figure 7.10: Local and Regional Transit

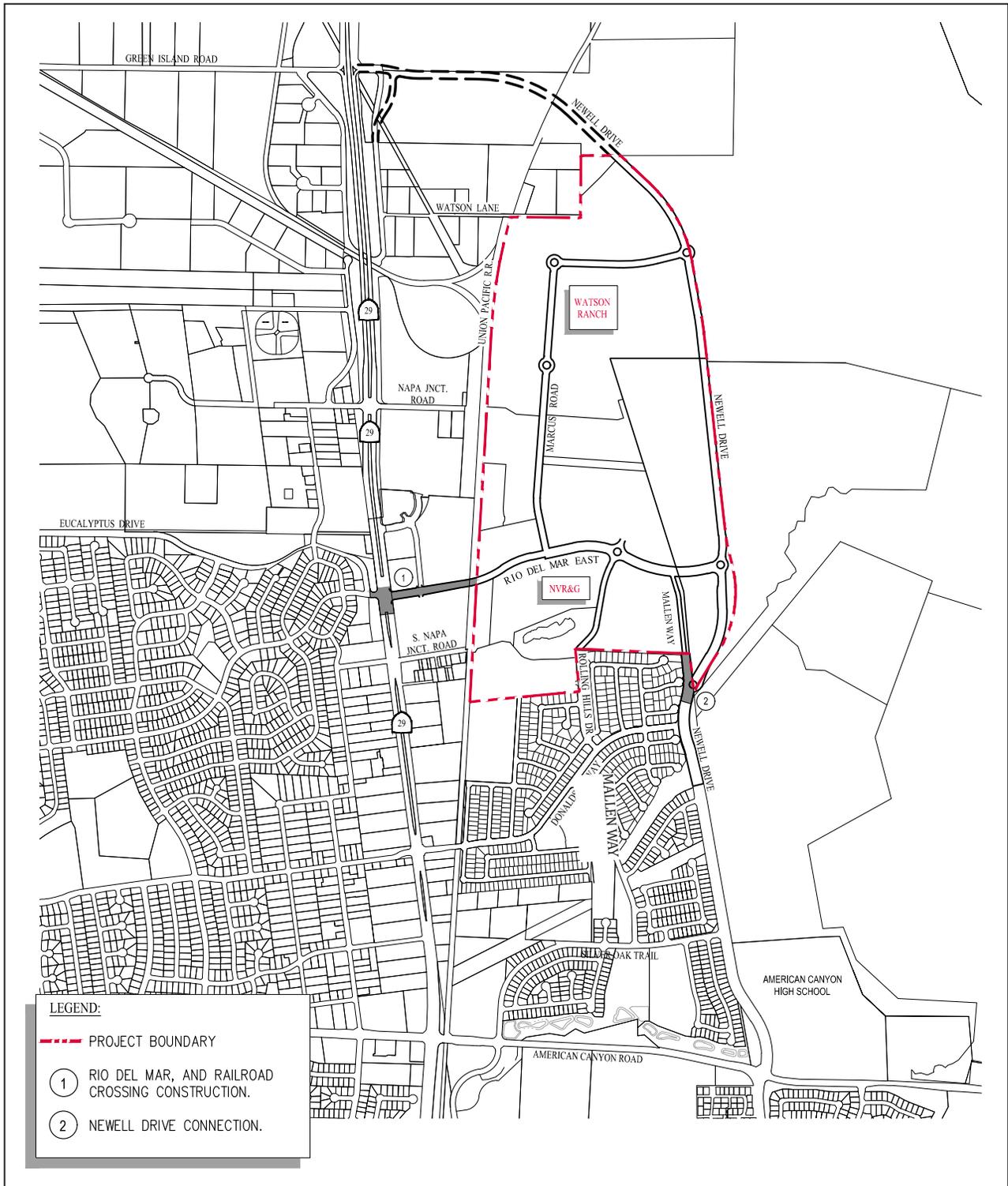


Figure 7.11: Summary of Off-Site Road Improvements

7.8 South Napa Junction

PRIMARY ACCESS OPTION

The City's General Plan Circulation Element (see Figure 3, Circulation Map in General Plan) shows a Major Collector designation and Major Intersection designation connecting SR 29 to Newell Drive. It allows for one of two options to connect to SR 29 from the Watson Ranch Specific Plan Area (Rio Del Mar East or South Napa Junction Road). The final determination of which option will be used is to be as set forth in the Specific Plan (Chapter 9 - Implementation).

The Western edge of the Watson Ranch Specific Plan is bound by the Union Pacific Railroad. Primary project access from SR 29 must cross this rail line. Any crossing of the rail line is subject to review and approval by the Public Utilities Commission (PUC).

The preferred planned access to the site is the Rio Del Mar East access alignment with an at grade crossing (see Figure 7.1). An alternative to the at grade crossing at Rio Del Mar East is the grade separated crossing along the Rio Del Mar East alignment (see Figure 7.3.2d). A final option for primary access to the WRSP from SR 29 is South Napa Junction with an at-grade crossing.

The final selection of the Major Collector alignment and connection to SR 29 from the WRSP site will be determined based upon legal, technical, economic, and other factors as provided for in the AC-1 Development Agreement as set forth in Chapter 9 - Implementation (see 9.3.4 (9)). The South Napa Junction alignment is also considered consistent with both the General Plan Circulation Element and the WRSP.

CHAPTER 8 - INFRASTRUCTURE AND PUBLIC SERVICES

8.1 Introduction

This chapter addresses the proposed infrastructure system and public services for the Watson Ranch Specific Plan (WRSP) Area. Infrastructure includes the water, wastewater and storm water runoff systems. The Public Services refer to public facilities and services and utilities necessary to serve the area. These include some citywide system improvements that will benefit more than the properties within the WRSP Area. The majority of infrastructure improvements will be privately funded. When warranted, reimbursement or assistance in financing may be provided when improvements exceed the project requirements and pro-rata share of responsibility for system improvements (see Implementation Chapter).

8.2 Goals, Objectives, and Policies

Goal 8A - To provide the infrastructure and public services required to support the WRSP Area without negatively affecting existing neighborhoods and residents of the City of American Canyon.

Objective 8.1 - Fully comply with the City of American Canyon “Zero Water Footprint Policy” in the development of the water supply and system for the WRSP Area.

Policy 8.1.1 - New potable water sources shall be identified, or existing water supply lines or uses shall be retrofitted to reduce water consumption so that the proposed development shall comply with the Zero Water Footprint Policy.

Policy 8.1.2 - New development shall comply with the City of American Canyon’s Model Water Efficient Landscape Ordinance and with the new water rules established on May 2015.

Objective 8.2 - Address on-site storm water drainage and retention through Low Impact Development (L.I.D.) standards and compliance with BASMAA and City of American Canyon requirements.

Policy 8.2.1 - On-site detention basins shall be efficiently sized to accommodate storm water flows from the project without adversely affecting downstream capacities.

Objective 8.3 - Provide improvements necessary to ensure sufficient downstream capacity in wastewater system.

Policy 8.3.1 - The proposed wastewater system shall be designed and sized accordingly to meet the project needs without adversely affecting downstream capacities.

Objective 8.4 - Provide solid waste recycling opportunities to reduce dependence on landfilling of solid waste.

Policy 8.4.1 - The existing solid waste collection and recycling service that exists within the City of American Canyon shall be extended to serve the project area.

Objective 8.5 - Provide state of the art telecommunication and technology infrastructure (e.g., fiber – optic cable) throughout the WRSP Area.

Policy 8.5.1 - City and developer shall work with telecommunications and cable television providers to provide the most advanced, market ready infrastructure for telecommunications and television. Necessary infrastructure shall be installed in the WRSP Area to facilitate upgrades in service as the technology becomes available.

8.3 Infrastructure

This section identifies the infrastructure required to serve the WRSP Area and addresses the overall infrastructure improvements for the WRSP. Each component will be designed and constructed to accommodate build out in a phased approach.

8.3.1 POTABLE WATER

The WRSP Area will require approximately 460 acre-feet per year of potable water. The total potable water demand will be reduced significantly by the use of recycled water for all commercial uses and public landscaping, resulting in a reduction of approximately 250 acre-feet per year of potable water. The WRSP Area is subject to the City's Zero Water Footprint policy. Studies address the policy by a combination of (1) bringing additional sources of water to the City, (2) facilitating potable water savings elsewhere in the City through water conservation improvements, and/or (3) an even more comprehensive use of recycled water. Through a Development Agreement, the AC-1 portion of the WRSP Area has satisfied the City's Zero Water Footprint policy.

The City of American Canyon obtains nearly all of its municipal water supply from the North Bay Aqueduct. This facility is managed by the Napa County Flood Control and Water Conservation District. The Sacramento River Watershed is the aqueduct's supply source. The aqueduct supply is supplemented with connections to the City of Vallejo and the City of Napa.

The WRSP Area is located within two potable water pressure zones: zone 1 and zone 3.

- Zone 1 provides service to areas within the City ranging in approximate elevation from 50 to 100 feet. This zone is supplied by a 2.5-million-gallon tank at the water treatment plant, which is also connected to the 2.0-million-gallon Oat Hill Tank #1.
- Zone 3 provides service to areas within the City ranging in approximate elevation from 100 to 160 feet. The existing Zone 3 tank is the 0.2-million-gallon Oat Hill Tank #2.

Figure 8.1 includes the preliminary water service zones for the proposed development. The service limits will be based upon final site grading and elevations.

The following facilities are adjacent to the WRSP Area:

- 6-inch water line (Zone 1) located in South Napa Junction Road.

- 18-inch water line (Zone 1) located on the west side of State Route 29.
- 18-inch water line (Zone 1) located in Newell Drive.
- 8-inch water line (Zone 1) located in Rolling Hills Drive.
- 8-inch water line (Zone 1) located in Summerwood Drive.
- 18-inch water line (Zone 3) located in Newell Drive north of the tank access road.

To provide for adequate potable water to the WRSP Area there will be off-site and on-site improvements to provide the required infrastructure.

OFF-SITE WATER IMPROVEMENTS

Off-site improvements may include the construction of two water tanks (Zone 1 and Zone 3) to provide additional supply (Figure 8.6), which would be the responsibility of the City of American Canyon and funded by the Capital Improvement Program (C.I.P.). These tanks have approved plans from the City. The Zone 1 tank (approximately 2.5 million gallons) has been designed to be constructed adjacent to the existing recycled water tank, north of the American Canyon High School and southeast of the WRSP Area. The Zone 3 tank (2.0 million gallons) will be constructed at a higher elevation to achieve required water pressure located southeast of the existing recycled water tank and Zone 1 tank sites. Zone 1 and Zone 3 water lines were installed from Newell Drive to the Recycled and the Zone 1 potable water tank sites. An interconnecting water line will be installed between the new Zone 1 and Zone 3 water tanks and an access road of approximately 15 feet in width will be constructed to provide access to the new Zone 3 tank.

Additional off-site improvements include installing a Zone 3 water main that connects the WRSP Area to the existing 18" Zone 3 water main in Newell Drive located north of the tank access road. This Zone 3 water main will serve the upper elevations of the WRSP Area and additional properties within the City of American Canyon.

See Figure 8.6 for existing (at the time of adoption of the 2018 WRSP) and proposed off-site water improvements.

ON-SITE WATER IMPROVEMENTS

Proposed on-site backbone improvements include water lines ranging in size from 6-inch to 12-inch in diameter. The on-site improvements connect to the following infrastructure lines:

- Existing 12-inch water line (Zone 1) within Watson Lane.
- Existing water line (Zone 1) within the east side of Highway 29, at a location approximately 480 feet south of S Napa Junction Road.
- Existing 8-inch water line (Zone 1) within Summerwood Drive.
- Existing 8-inch water line (Zone 1) within Rolling Hills Drive.
- Existing 18-inch water line (Zone 3) within Newell Drive located north of the tank access road.

All of the facilities will be designed and constructed to meet the City of American Canyon Standards.

See Figure 8.2 – Water Infrastructure – On-Site Improvements.

8.3.2 SANITARY SEWER

A Sewer Flow Monitoring Capacity Study was prepared for the WRSP Area in August 2014 (V&A Report). Its findings presented two methods for estimating peak sewer flow demand. Using existing City criteria, the peak flow demand is estimated to be 1.28 mgd (million gallons per day). Using criteria that recognizes the reduced sewer flow that would occur as a result of the planned use of recycled water, the sewer flow demand is estimated to be 0.86 mgd. Both sewer flow results can be accommodated in the existing sewer main in American Canyon Road, if the existing sewer main in State Route 29 is upsized. The replacement and upsizing of the old sewer main in State Route 29, could significantly reduce the existing Inflow/Infiltration burden on the existing sewer system, and would offset most of the WRSP Area's increased burden on the City's wastewater treatment plant.

The City of American Canyon provides collection and treatment of the wastewater throughout the City. Wastewater is collected and directed to the Wastewater Treatment Plant on the east side of the City for treatment.

The following facilities are adjacent to the WRSP Area:

- 15-inch sanitary sewer line running behind the southern lots on South Napa Junction Road (east of State Route 29).
- 10-inch sanitary sewer line located along the west side of State Route 29.

- 6-inch sanitary sewer line running under State Route 29 that connects the 10" sanitary sewer (west side of State Route 29) to the 15-inch sanitary sewer (east side of State Route 29).
- 6-inch sanitary sewer line in Napa Junction Road.
- 8-inch sanitary sewer line in Summerwood Drive.

The WRSP Area connects to the existing 10" sanitary sewer in State Route 29. A portion of the WRSP Area also connects to the existing 8-inch sanitary sewer in Summerwood Drive. Based on existing flow information, pipe upsizing and replacement is necessary in State Route 29, replacing the existing 10" sewer line to 15" from approximately 500 feet south of South Napa Junction Road to American Canyon Road. The segment of this line from Donaldson Way to American Canyon Road is a City C.I.P. project, the cost of which to be borne by the C.I.P. A sanitary sewer lift station will be required on-site to help maintain a gravity sewer system throughout the WRSP Area.

Backbone on-site facilities will include gravity sewer lines ranging from 8 inches to 15 inches in size. Sewer facilities will be designed and constructed in accordance with City of American Canyon Standards.

See Figure 8.3 for the backbone sanitary sewer improvements.

8.3.3 RECYCLED WATER

It is estimated that the WRSP Area's use of recycled water will be approximately 250 acre-feet per year. The City currently has the capacity to produce 1,000 acre-feet per year of recycled water and given the current usage, there is sufficient capacity in the existing City recycled water treatment plant.

The City of American Canyon provides recycled water service. At present, limited recycled water facilities are located throughout the City and not all portions of the City contain recycled water infrastructure. There is a 12-inch recycled water line within Newell Drive just south of the WRSP Area that was installed with the Vintage Ranch development. This 12-inch mainline is served by a one-million-gallon recycled water tank located southeast of the site. A recycled water system in the WRSP Area will be tied in to the following facilities adjacent or proximate to the WRSP Area:

- 12-inch recycled water line in Newell Drive
- Recycled Water line in State Route 29
- 6-inch recycled water line in Watson Lane

As part of the on-site improvements, a 12-inch backbone recycled water system will be extended into the WRSP

Area. Alley landscaping, median landscaping, frontage landscaping, City-owned landscape areas, and commercial landscape areas will utilize recycled water.

See Figure 8.4 for the existing and proposed recycled water improvements.

8.3.4 STORMWATER DRAINAGE

The City of American Canyon is responsible for stormwater drainage facilities. The drainage pattern within American Canyon is generally in a western direction originating in the hills of the Sulfur Spring Mountains. The WRSP Area is located within two of the five primary watershed areas within the city: North Slough (northern portion of the site) and Rio Del Mar (southern portion of the site). Currently, the Union Pacific Railroad track embankment acts to detain the drainage coming off the eastern hills. The drainage is discharged under the railroad tracks through several culverts and box culverts.

Within the WRSP Area, the site is divided into drainage areas, each with a detention storage basin. On-Site storm drain facilities will collect site drainage and direct it to a specific detention basin. These detention areas will provide storage capacity to allow flows leaving the site to mimic the existing conditions. The detention areas will also incorporate storm water quality features to maximize their benefit. Storm water quality features will conform to the City of American Canyon, and Bay Area Storm Water Management Agencies (BASMAA) quality requirements.

The southern portion of the site is within the Rio Del Mar watershed. Downstream of the site, the existing City storm drain infrastructure has documented existing capacity constraints. All stormwater will be directed to detention basins on-site, which are primarily located downslope at the western edge of the WRSP Area. To help provide relief to this watershed, the existing quarry lake will also be utilized as a detention component of the WRSP storm drain system, helping to minimize the ultimate drainage volumes in the downstream stormwater system.

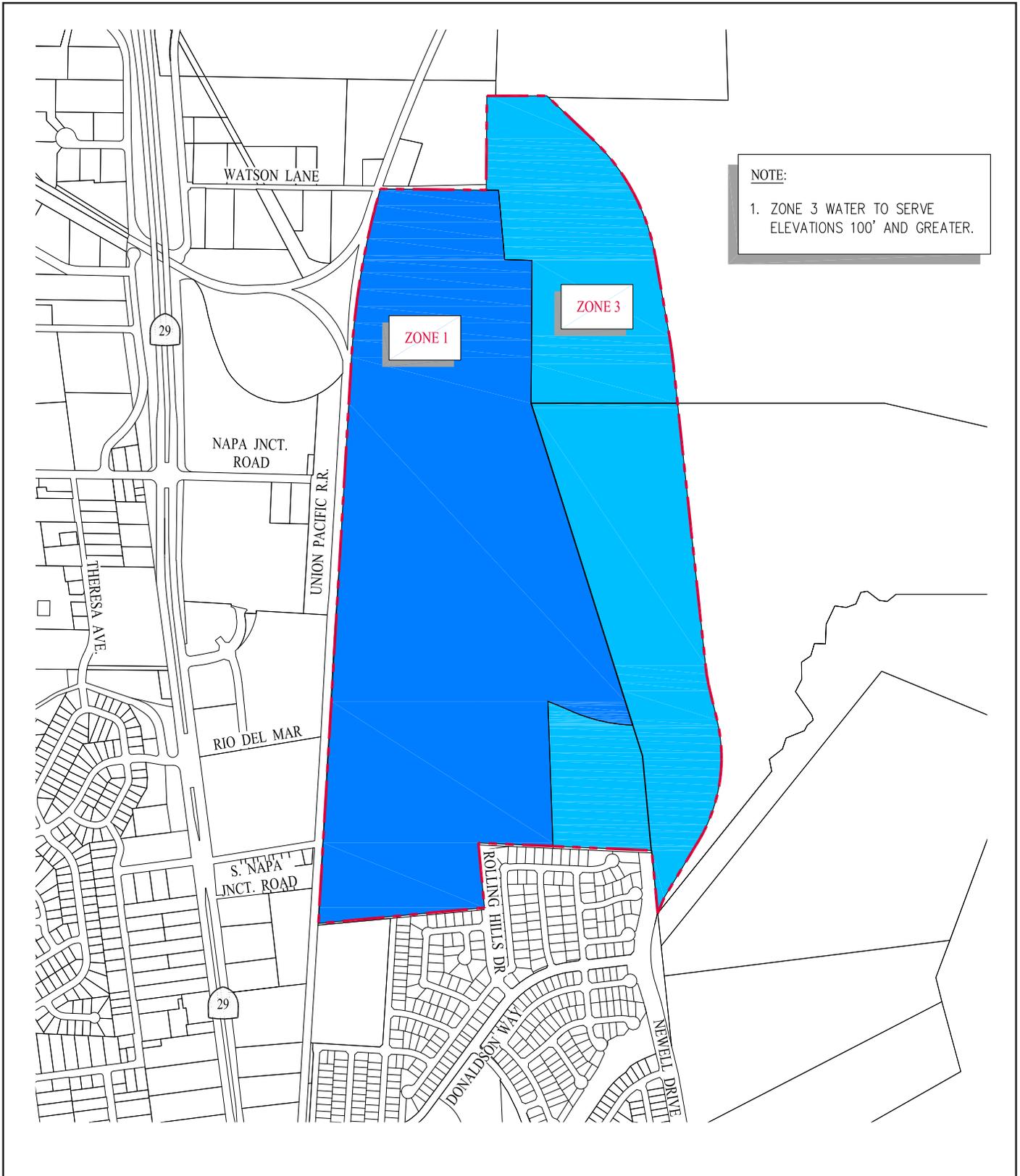


Figure 8.1: Water Service Zones

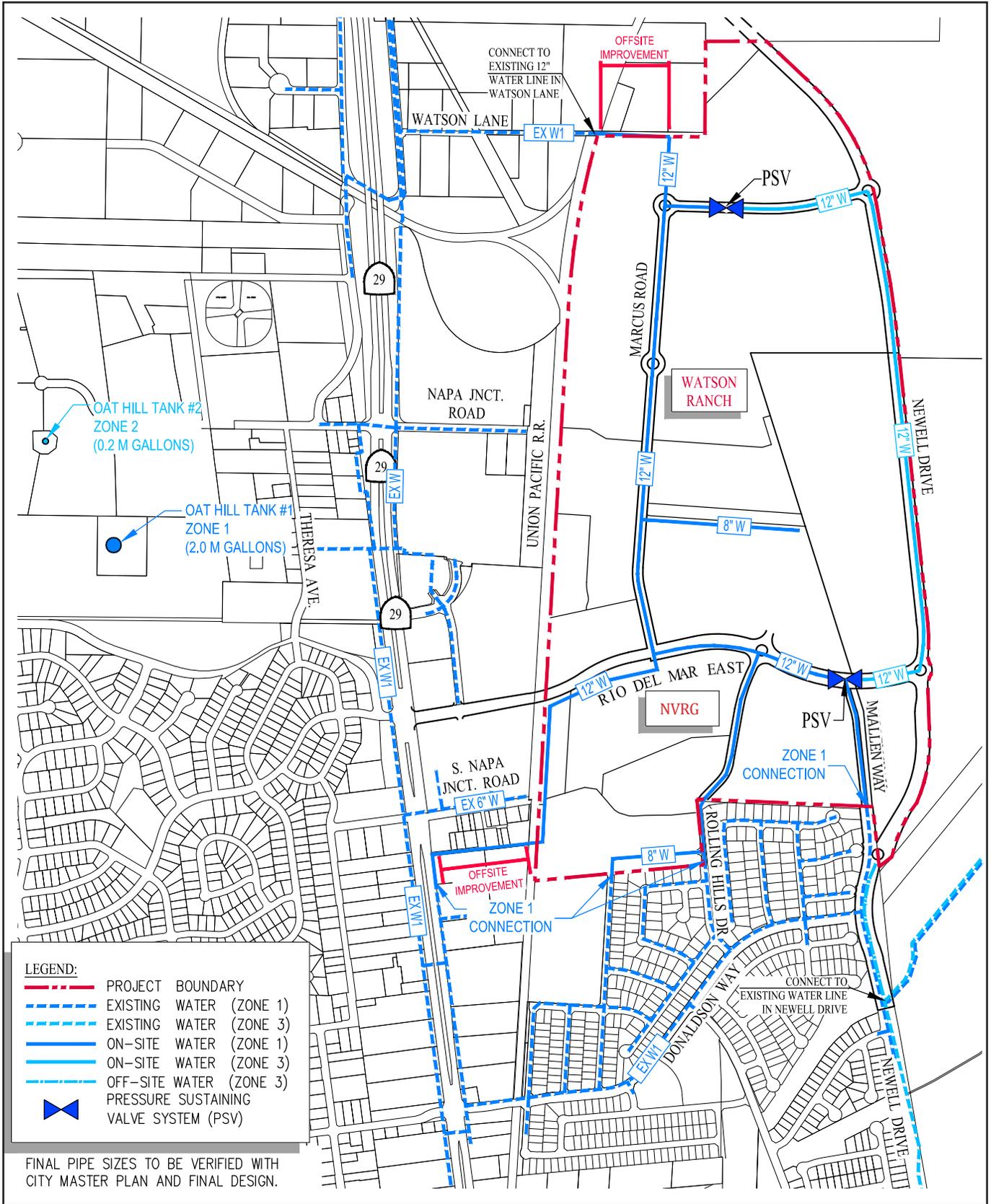


Figure 8.2: Water Infrastructure - On-Site Improvements

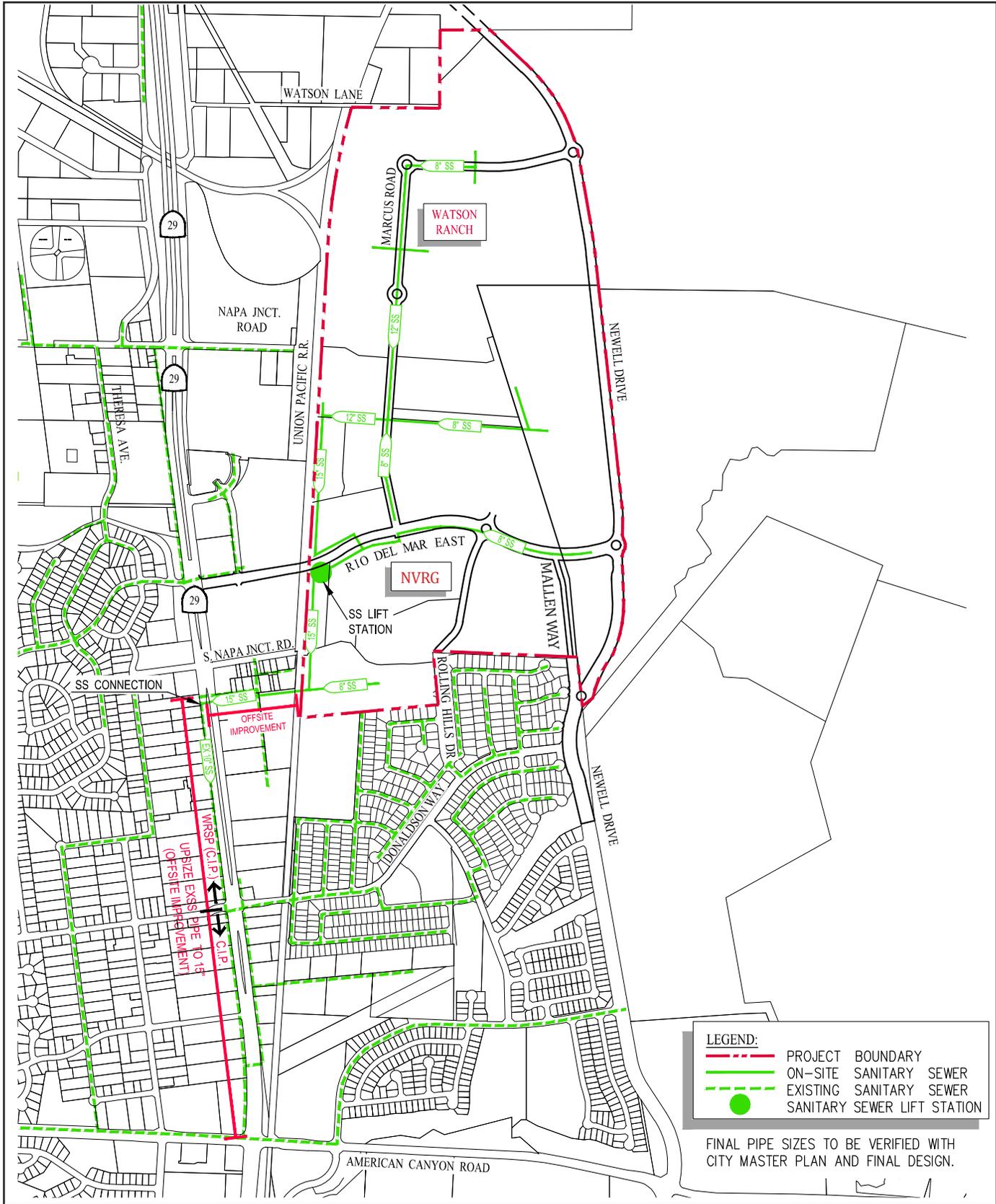


Figure 8.3: Sanitary Sewer Infrastructure Improvements

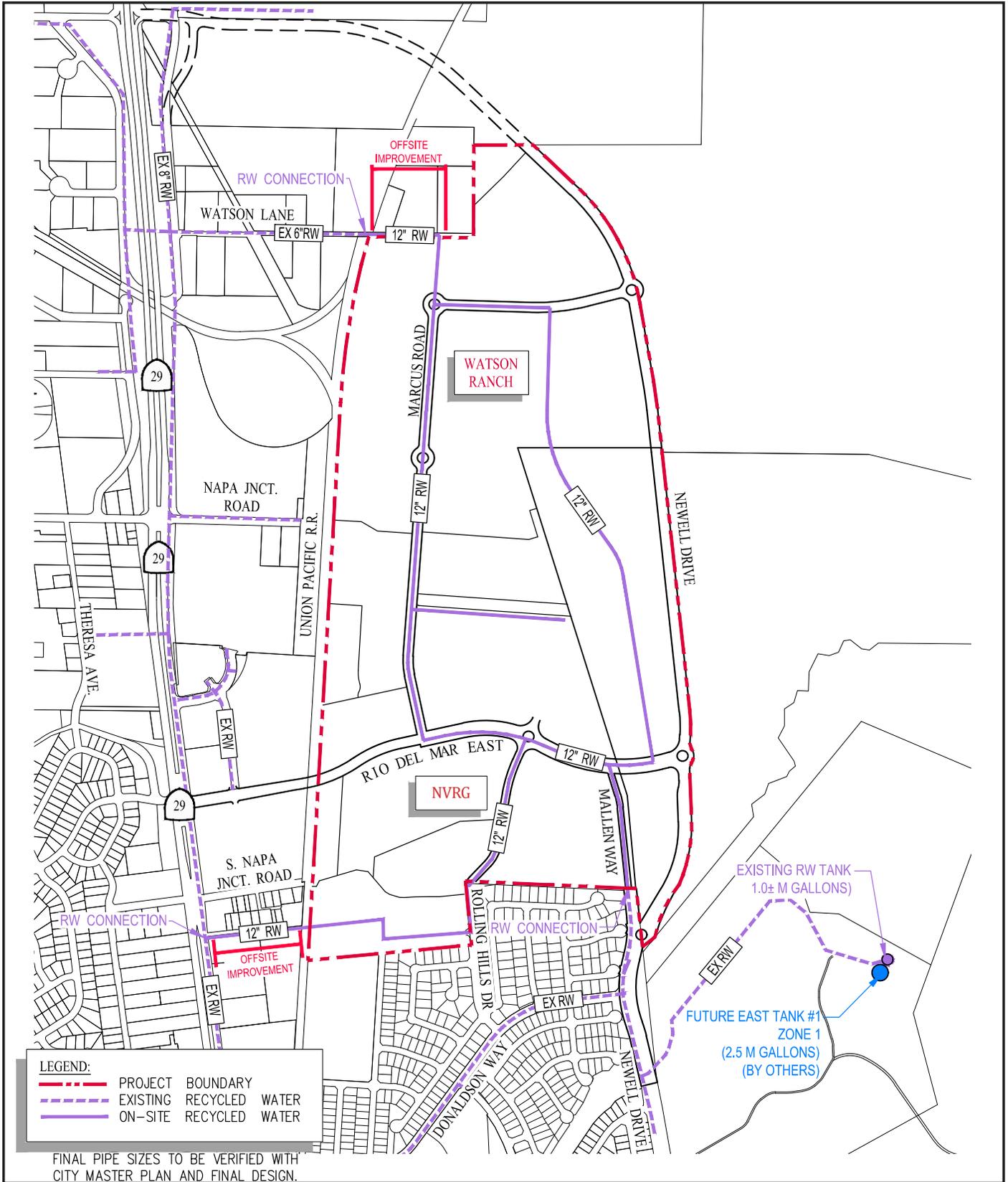


Figure 8.4: Recycled Water Infrastructure Improvements

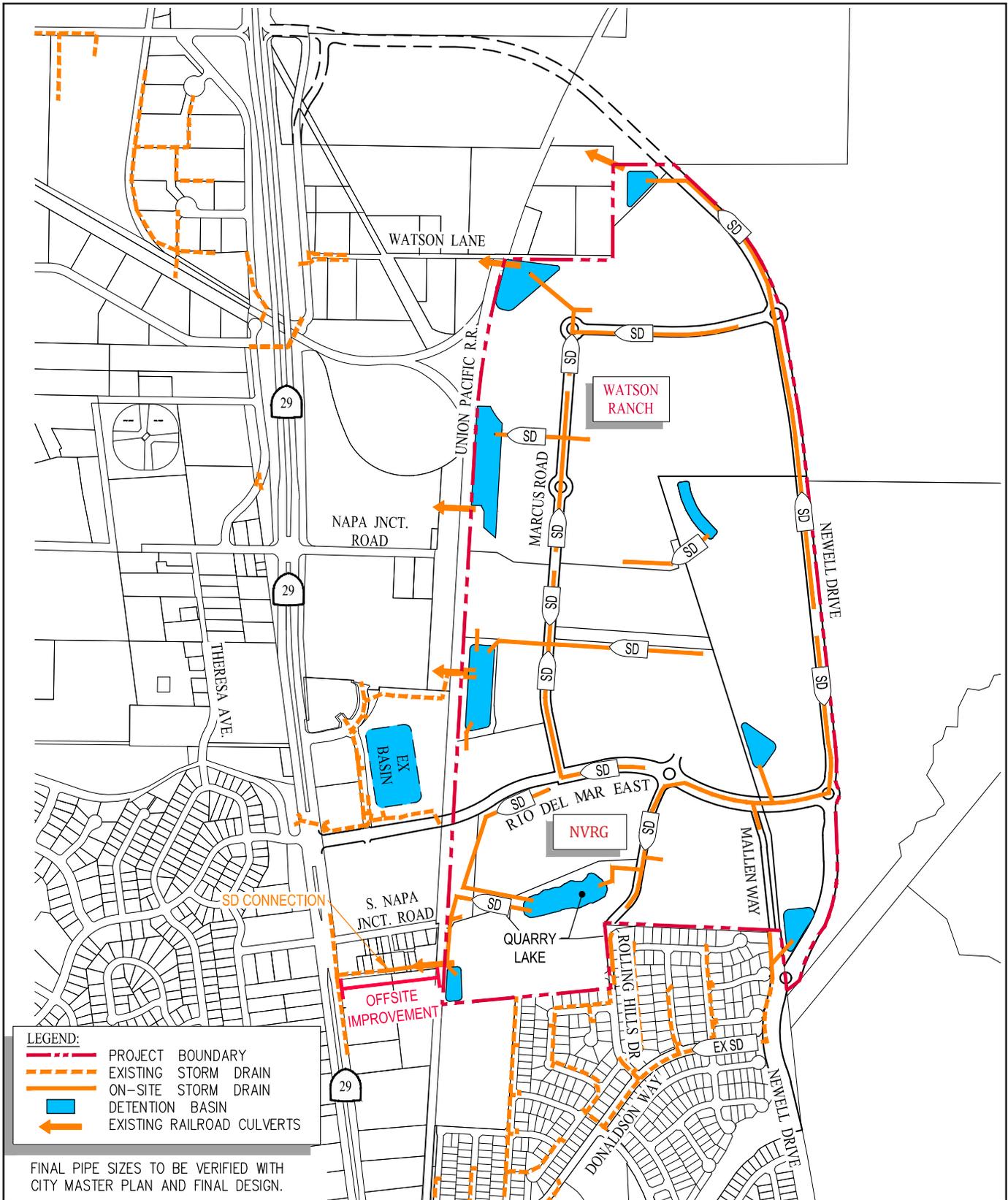


Figure 8.5: Storm Water Drainage Infrastructure

8.4 Public Services

The following is an overview of the public services necessary to meet the needs of the WRSP Area residents in accordance with the policies of the City's General Plan.

8.4.1 FIRE AND EMERGENCY SERVICES

The American Canyon Fire Protection District will provide fire suppression, rescue, and emergency medical services to the WRSP Area. The closest fire and emergency services facility is Station 11, located at 911 Donaldson Way East within the Vintage Ranch neighborhood to the south.

8.4.2 POLICE PROTECTION

The American Canyon Police Department will serve the WRSP Area. The closest police station is located at 911 Donaldson Way East within the Vintage Ranch neighborhood to the south.

8.4.3 LIBRARY

The City of American Canyon operates the American Canyon branch of the Napa County Library located at 300 Crawford Way. No additional library facilities are planned within the WRSP Area.

8.4.4 ELECTRICITY AND GAS

The Pacific Gas and Electric Company currently provides electric and gas service within the City of American Canyon, and will do so for the WRSP Area.

There is an existing 12" high pressure PG&E gas main that traverses the site along the proposed location of the Napa Valley Vine Trail. Site grading and infrastructure will need to be designed and constructed in respect to the existing gas main. Infrastructure and facilities will be coordinated with PG&E.

Smaller gas pipe and services will be installed with the backbone and in-tract infrastructure to serve the WRSP Area.

8.4.5 TELECOMMUNICATIONS

The telecommunications provider will provide telecommunications services to the WRSP Area. They will determine the required facilities to adequately serve the site.

8.4.6 CABLE TELEVISION

The cable service provider will provide cable television and related services to the WRSP Area. The provider will determine the required facilities to adequately serve the site.

8.4.7 SOLID WASTE

Recology will provide solid waste and recycling collection services to the WRSP Area. Land use and development type will dictate trash service requirements. Solid waste and recycling services will be designed to meet the standards and requirements of Recology and the City of American Canyon.

8.4.8 SCHOOLS

The WRSP Area is located within the Napa Valley Unified School District boundary (NVUSD). There are five schools in American Canyon that will accommodate students from the WRSP area, based on existing and projected capacity. Development in the WRSP Area will mitigate its school impacts through the payment of school impact fees per the current published rates on the NVUSD website.

8.5 Off-Site Improvements

8.5.1 OFF-SITE DOMESTIC WATER IMPROVEMENTS

Off-site improvements will include the construction of a new Zone 3 (upper pressure zone) water tank to provide adequate supply, which is the responsibility of the City and funded by the C.I.P. This tank, along with a proposed Zone 1 tank have approved plans from the City. The Zone 1 tank (approximately 2.5 million gallons), which is not a part of the project, would be constructed next to the existing recycled water tank, north of the high school and southeast of the WRSP Area. The Zone 3 tank (2.0 million gallons), which is necessary to serve the portion of the WRSP generally east of the PG&E gas line alignment (see figure 8.1), will be constructed at a higher elevation to achieve required water pressure in a location southeast of the existing recycled water tank and Zone 1 tank site. The Zone 3 water tank will provide service to the upper pressure zone within the WRSP as well as other existing and future projects within the City. Zone 1 and Zone 3 water lines were installed from Newell Drive to the Recycled and the Zone 1 potable water tank site.

A 12" Zone 3 water line will be installed in Newell Drive from the southern boundary of the project to the existing 18" Zone 3 water line in Newell Drive located north of the tank access road. A 12" Zone 3 water line was also installed parallel to – and approximately 480 feet south of – South Napa Junction Road.

8.5.2 OFF-SITE SANITARY SEWER IMPROVEMENTS

The WRSP Area will upsize the existing 10" sanitary sewer in State Route 29 between South Napa Junction Road and American Canyon Road to a 15" sanitary sewer line. The segment of this line from Crawford Way to American Canyon Road is a City C.I.P. project, to be funded by the C.I.P.

8.5.3 OFF-SITE RECYCLED WATER IMPROVEMENTS

Off-site improvements include a 12" recycled water line south of – and running parallel to – South Napa Junction Road. A 12" recycled water line will be installed east of Watson Lane (east of the Railroad), connecting the WRSP Area to the existing 6" recycled water line in Watson Lane (ends at the Railroad), which is the responsibility of the City and funded by the C.I.P. Both of the WRSP Area's offsite recycled water connections will provide better redundancy in the existing City system.

8.5.4 OFF-SITE STORM DRAIN IMPROVEMENTS

Off-site improvements include a connection from detention basins in the southwest corner of the WRSP Area to the existing storm drain located just east of State Route 29 and south of South Napa Junction Road, which is the responsibility of the City and funded by the C.I.P.

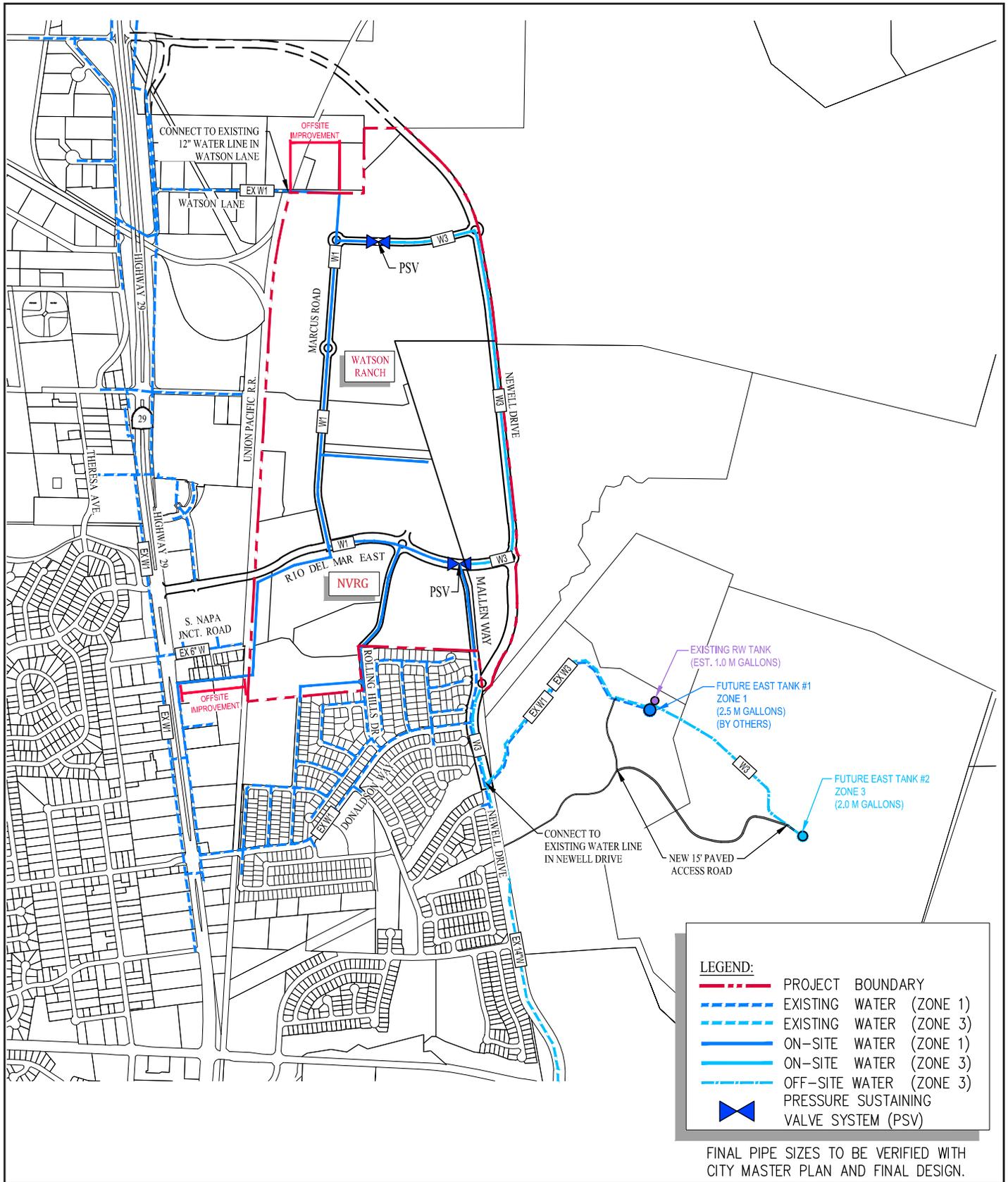


Figure 8.6: Water Infrastructure - Off-Site Improvements

CHAPTER 9 - IMPLEMENTATION

9.1 Introduction

This chapter fulfills the requirements of California Government Code Section 65451(a) (4), which requires that a Specific Plan include a program of implementation measures. This chapter shall serve as the program of implementation measures for the Watson Ranch Specific Plan (WRSP). It includes administrative procedures, public works projects, phasing and proposed methods of financing needed to carry out the WRSP.

The purpose of the implementation chapter is to ensure that:

- Public infrastructure and services are provided concurrently with private development.
- Adequate financing is in place to pay for the construction of necessary public facilities and improvements as defined in the Backbone Infrastructure.
- Adequate financing is in place to pay for the operation and maintenance of privately maintained facilities.
- Construction of public infrastructure by the city is coordinated with that constructed by the development.
- Consistency is achieved between the provisions of the WRSP and Development Agreement(s).
- Administrative procedures are in place to regulate development, in accordance with the standards and policies of the WRSP.
- Appropriate flexibility is provided to allow the project to respond to changing market conditions.

9.2 Land Use Regulations

9.2.1 TOWN CENTER (TC) ZONING AND WRSP LAND USE - ZONING

Consistent with the City of American Canyon's Town Center (TC) Zone (Municipal Code 19.12.020), supported by the City of American Canyon General Plan, the WRSP General Plan amendment and the WRSP, the regulations within the WRSP shall guide development in the WRSP area. The WRSP has established a WRSP Land Use Map (see Figure 4.2) that delineates the different Land Use – Zoning categories in the WRSP. These include Medium Density Residential (MDR-12 and MDR-16), High Density Residential (HDR), Napa Valley Ruins & Gardens Mixed Use (MU), Parks (PARK), and Open Space (OS). Within the context of the WRSP and as authorized by the City of American Canyon Municipal Code Section 19.12.010 – 050, these designations shall serve as combined Land Use-Zoning designations for purposes of implementing the WRSP and related regulations.

9.2.2 DEVELOPMENT REGULATIONS

The WRSP contains development regulations and guidelines, including those found within Appendix A to govern architectural design character, site planning, setbacks, architectural form, and landscape selection for both residential and nonresidential uses. These regulations are intended to supplement the City of American Canyon Zoning Ordinance and serve as the regulations for the WRSP Area; and supersede conflicting standards in the City of American Canyon Zoning Ordinance and subdivision regulations. Unless otherwise provided for in approved Development Agreement(s), where the WRSP standards do not provide express direction, the City of American Canyon Zoning Ordinance and subdivision regulations shall be used as appropriate.

9.2.3 ENTITLEMENT PROCESSING

Unless otherwise provided for in approved Development Agreement(s), applications for subsequent entitlements within the WRSP shall be processed in accordance with the City of American Canyon Municipal Code Sections 19.41.010 – 060 (see Subsequent Entitlements, Section 9.7 below). Development Plans within the WRSP shall integrate the design and development objectives, policies, regulations and guidelines contained in approved Development Agreement(s), the WRSP, and specific Design Guidelines that may be approved with specific development. Where conflicts arise with City ordinance,

the policies, guidelines, and regulations contained in the WRSP and other city regulations, the City ordinances, policies, guidelines, and regulations contained in approved Development Agreement(s) shall prevail.

9.3 Phasing

Actual phasing of development will be at the discretion of the developer(s). The phasing concept includes four phases, although the location and extent of each phase and the pace of the phased development may vary based upon market conditions and other economic factors. As a result, each phase may have smaller sub-phases which respond to market and financing conditions. For purposes of environmental review, the Project began construction in 2021 with each phase time frame being 2 to 3 years. The conceptual phasing program also assumes some overlap in phases resulting in a total projected build-out of 8 years. Actual build out may extend over a longer period of time subject to the variations in the market.

Should the pace of project phasing advance significantly faster than the anticipated build-out of 8 years (based on the commencement of construction in July 2021), the developer shall be required to do an updated air quality analysis to identify which additional mitigation requirements may need to be implemented.

9.3.1 RESIDENTIAL DEVELOPMENT

Residential phasing will be a product of economic conditions, market demand, and infrastructure availability. If there is market demand and if infrastructure is constructed in a manner that ensures availability when such residential development comes “on line,” then there shall be no phasing limitations on such residential development. It is necessary that adequate infrastructure is planned, constructed, and available for whatever residential development is supported by economic conditions and market demand.

Further determinations of phasing may be contained in approved Development Agreement(s). For a description of the number and types of residential uses allowed in the WRSP Area, see Chapter 4.

9.3.2 INFRASTRUCTURE PHASING

Infrastructure shall be constructed to serve development with the WRSP Area. As such, infrastructure planning, development, construction, completion, operation and maintenance shall be commensurate with development within the WRSP Area.

Infrastructure improvements are described in Chapter 8.

The following maps (Figures 9.1 through 9.5) depict anticipated phasing of Backbone Infrastructure for the WRSP Area. These figures are for illustrative purposes only to indicate the general progression of infrastructure improvements anticipated through project completion. Actual phasing will be established in the adopted Development Agreement(s).

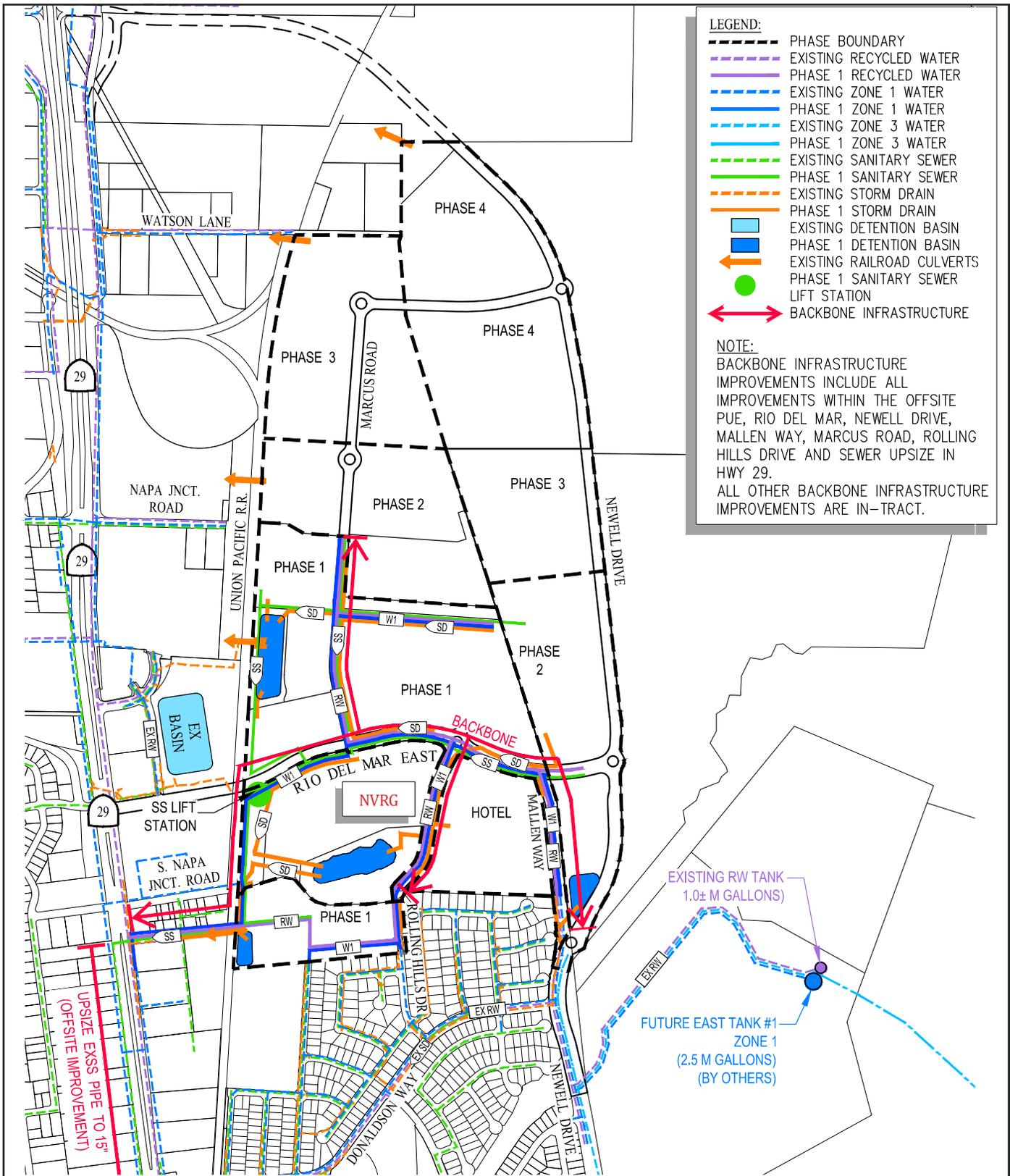


Figure 9.1: Phase 1 - Potential Infrastructure Improvements - Subject to Modification

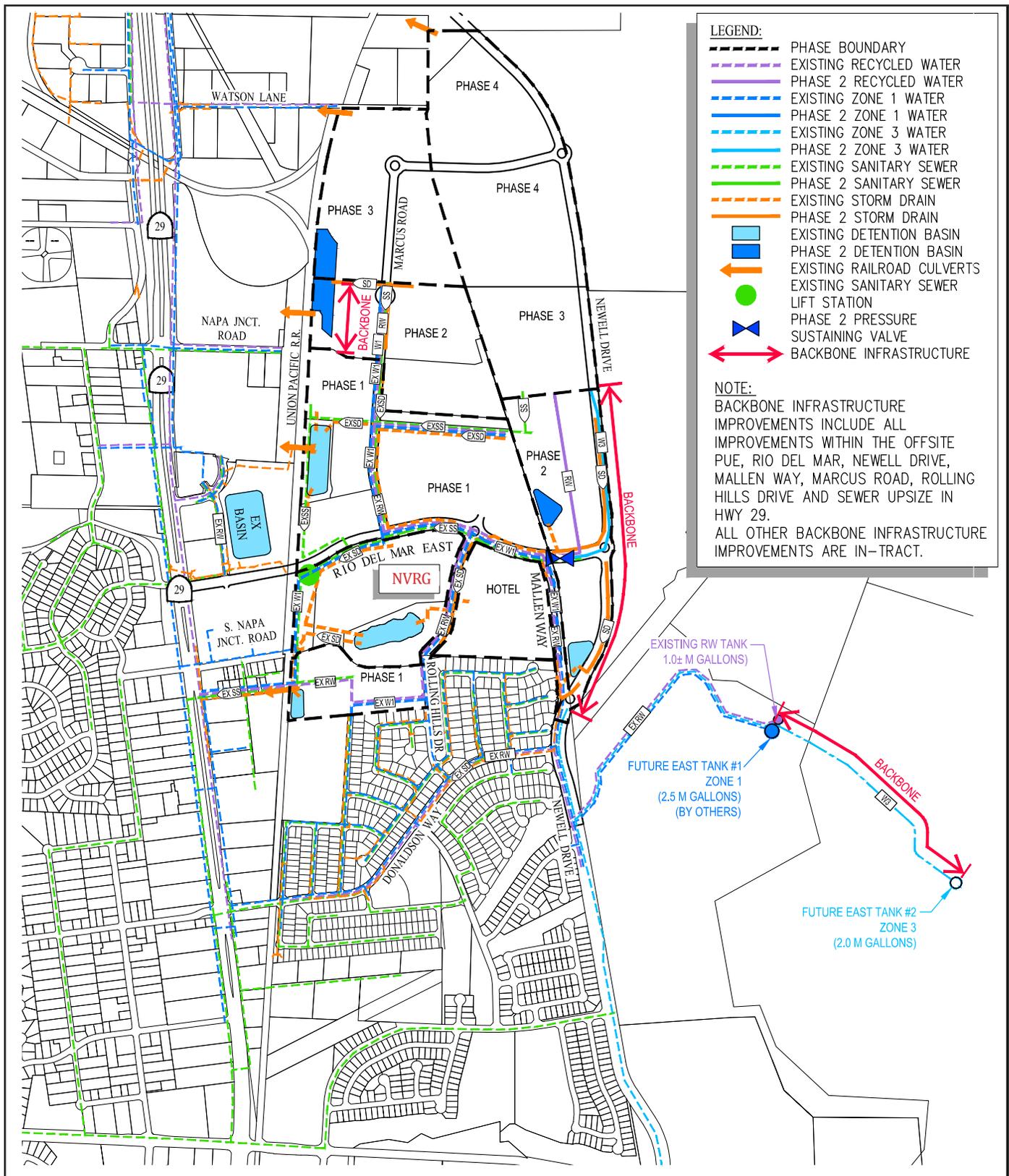


Figure 9.2: Phase 2 - Potential Infrastructure Improvements - Subject to Modification

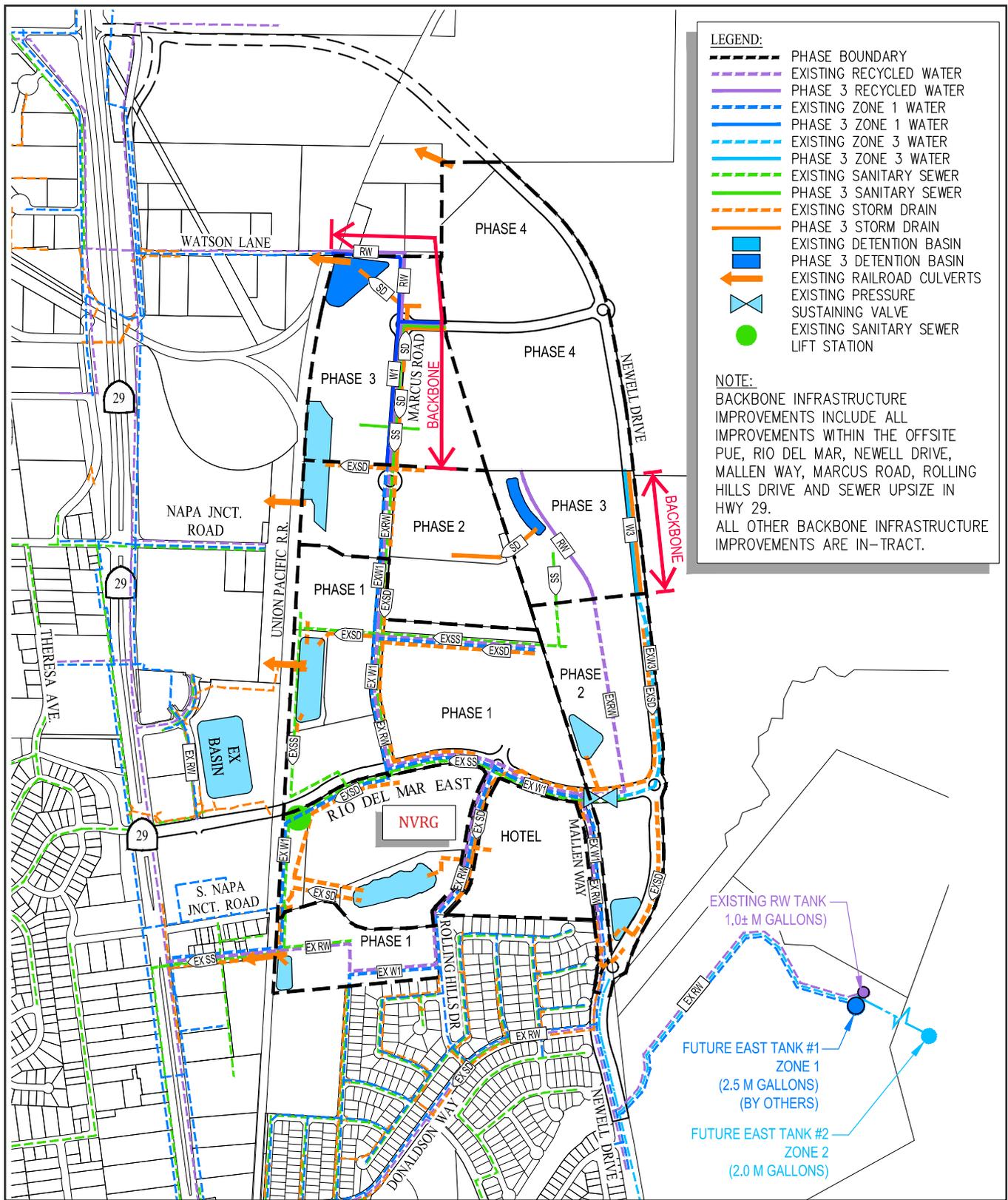


Figure 9.3: Phase 3 - Potential Infrastructure Improvements - Subject to Modification

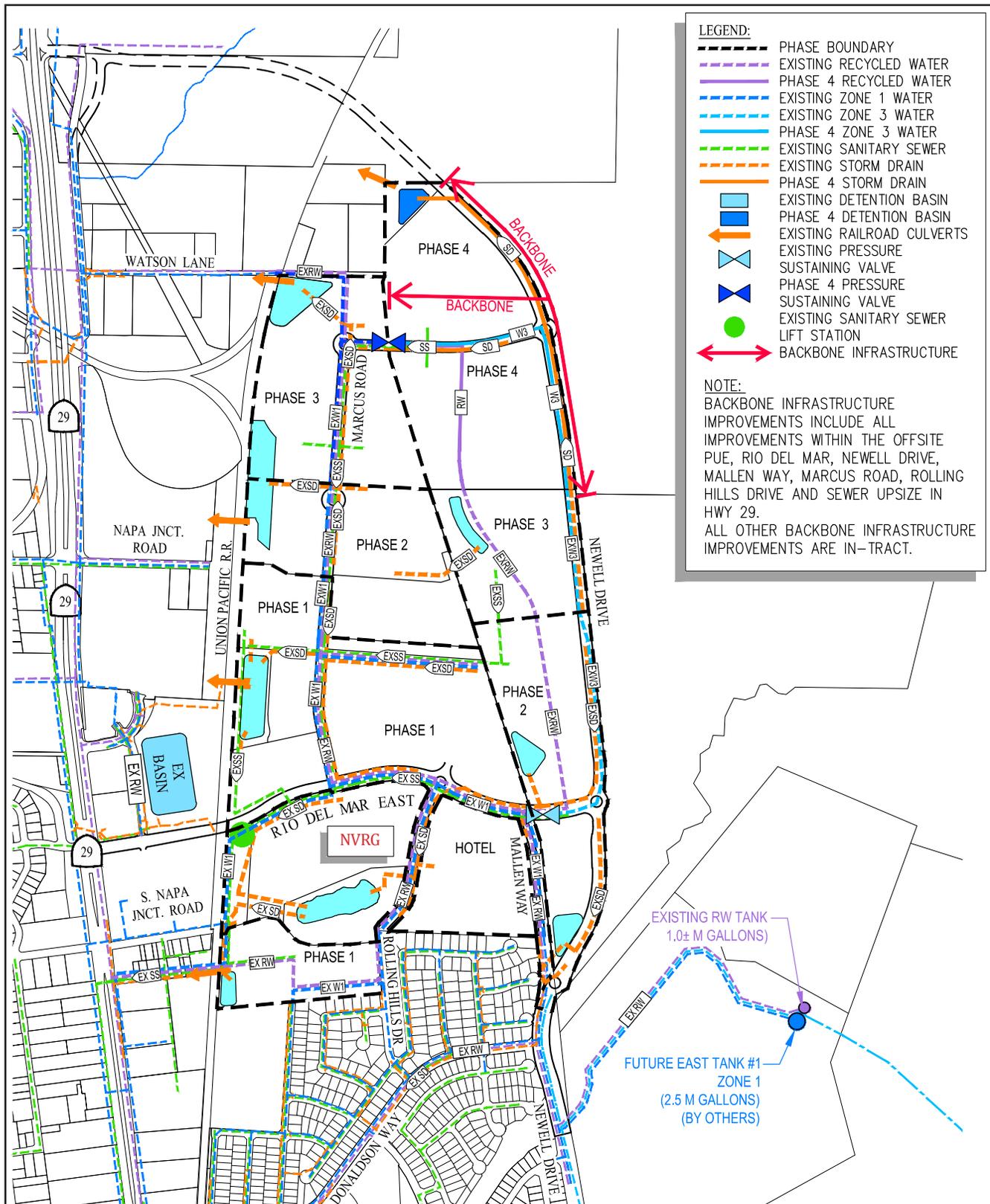


Figure 9.4: Phase 4 - Potential Infrastructure Improvements - Subject to Modification

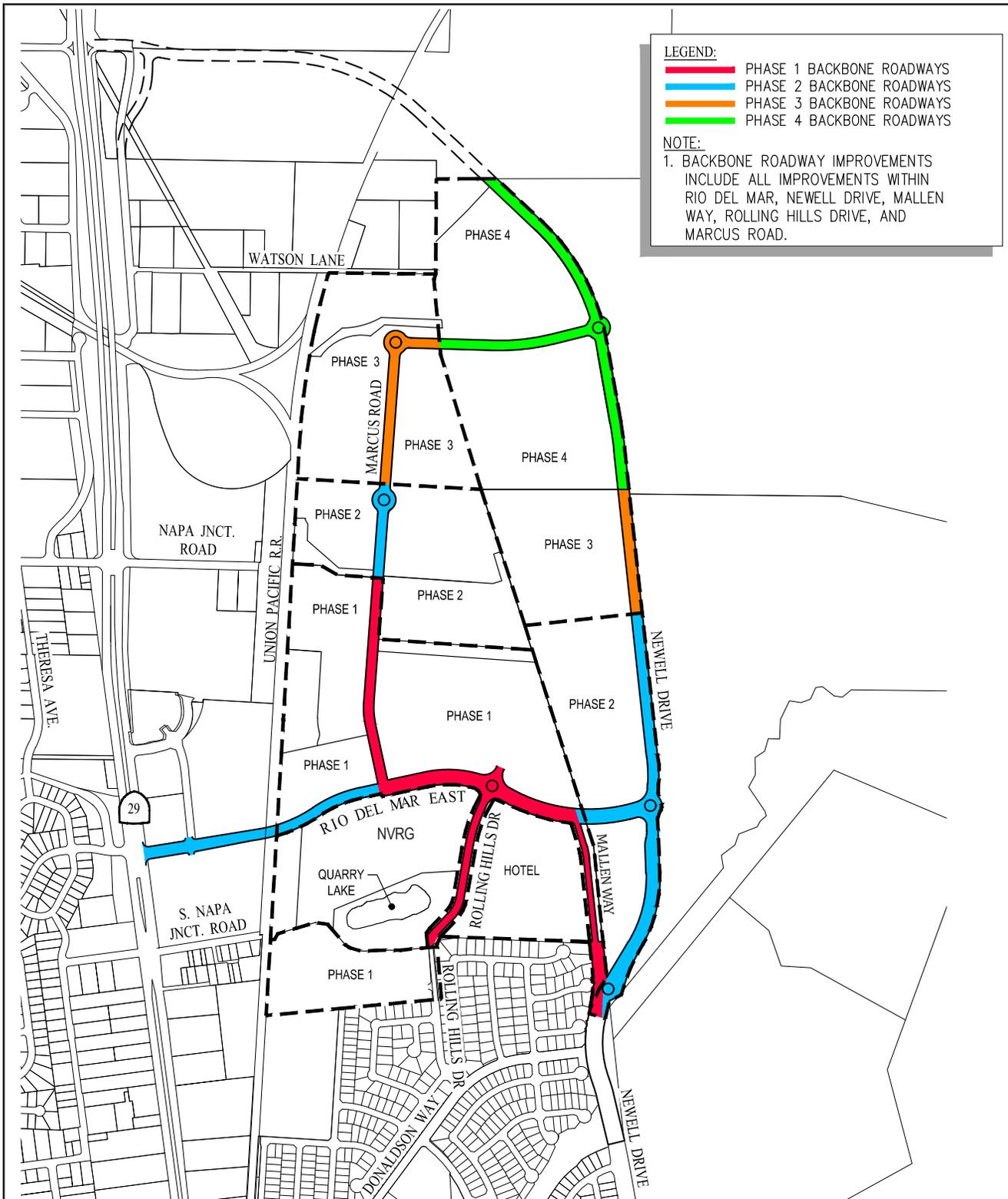


Figure 9.5 : On-Site & Off-Site Backbone Roadway Phasing - Subject to Modification

Backbone Roadway improvements are generally constructed in conjunction with the adjacent phase of residential development. If a portion of any phase of development is delayed, construction of the Backbone Roadway improvements adjacent thereto may also be delayed as established in the approved Development Agreement(s).

9.3.3 PHASING OF LAND DEDICATIONS

1. General

All land necessary for public improvements and facilities as required by the WRSP, shall be offered for dedication to the City of American Canyon in accordance with the timeframes generally described below and more specifically defined in the Development Agreement(s).

All land subject to dedication shall be offered for dedication at no cost to the City, unless otherwise agreed to by City and Owner and shall be free and clear of all liens and encumbrances that would preclude its intended use.

Land dedication requirements of any subdivision map or parcel map approved within the WRSP Area shall not exceed the standards of the Specific Plan or accompanying Development Agreement(s).

Certain required elements of the WRSP's public infrastructure is located outside of the Plan's boundaries, and on lands not owned by the property owner. The general location of such infrastructure elements are indicated in graphics in Chapter 7, Circulation, and Chapter 8, Infrastructure and Public Facilities. The City and the WRSP developers shall work collaboratively to secure such sites and rights-of-way necessary for critical backbone infrastructure.

2. Land Dedication Conditions for First Final Map:

- Newell Drive as described in the approved Development Agreement(s)
- Rio Del Mar East from Newell Drive to the Western Project Boundary
- Associated road rights of way and utilities as described in the approved Development Agreement(s)
- Associated Parks and Open Space as described in the approved Development Agreement(s)

3. Land Dedication Conditions of Subsequent Final Maps

- Associated road rights of way, utilities and associated Parks trails and open space areas as described in the approved Development Agreement(s)

9.3.4 PHASING OF PUBLIC AND QUASI-PUBLIC INFRASTRUCTURE

The public and quasi-public infrastructure in the WRSP Area is comprised of the streets, parks, open space, and trails. The Parks and Open Space areas will be provided in conjunction with the appropriate subdivision map and may include dedication with first subdivision map

and construction of subdivision improvements. Specific conditions and timing of improvements shall be described in the Development Agreement(s).

9.3.5 PHASING OF PRIVATE DEVELOPMENT

General Policies Regarding the Phasing of Private Development

1. NVRG is a development priority in the WRSP. Development of the NVRG may commence pursuant to the phasing plan as part of phase 1. If not initiated during phase 1 of the residential development, the NVRG must commence development in conjunction with the start of phase 2 of the residential development.
2. NVRG Property owner shall have the right to commence development of the NVRG at any time provided Backbone Infrastructure (as designated in the Phasing Plan) has been approved and is under construction.
3. Private development in a subsequent phase of residential development may commence before development of a previous phase is complete provided Infrastructure Backbone (as designated in the Phasing Plan) has been approved and is under construction.
4. Phase boundaries may be modified if necessary as development progresses, subject to the approval of the City and general consistency with the proposed phasing plan.
5. Multiple final maps may be filed with each phase of development when approved as part of the tentative map entitlement stage.
6. Dedication and construction (or the requirement to cause the finance and construction) of Rolling Hills Drive shall occur at the commencement of Phase 2 of the residential development.
7. Phase 1 of the WRSP residential development may commence without the completion of the Rio Del Mar East extension (off site improvement) subject to the approval of at least two points of access to the Phase 1 area. No more than 743 homes may be constructed without completion of the Rio Del Mar East railroad crossing and the connection to SR29. The 186 affordable housing units do not count towards this limit.
8. The Railroad crossing at Rio Del Mar East shall first be pursued for PUC approval as an at grade crossing. Should the at grade crossing not be approved, the grade separated crossing will be pursued.

9.4 Public Facilities and Infrastructure Financing

This section describes how the public facilities and infrastructure needed to support development in the WRSP Area will be financed. Specifically, this chapter:

- Describes an overall financing framework
- Identifies infrastructure requirements for the plan area
- Sets forth principles and policies regarding how these financing obligations will be met
- Identifies potential financing mechanisms
- Proposes action items to implement the financing framework

9.4.1 PRIVATE DEVELOPER FINANCING

The WRSP will be implemented through different public and private financing mechanisms. There may be improvements that in the broader community context, provide benefits to populations or areas beyond the boundaries of the WRSP. This Infrastructure financing framework recognizes four geographic tiers of infrastructure benefit, which help to define conceptually how costs could be allocated. Tiers one through three represent improvements that may be eligible for some level of credit or public financing assistance.

9.4.1.1 CITYWIDE

Costs for infrastructure items that are determined to be of citywide benefit, are included in the City's Capital Improvement Program, and could be funded by impact fees, "in-kind" construction, connection charges, or other citywide funding sources. An example of this category of improvement would be new water storage facilities that provide a citywide benefit.

9.4.1.2 SURROUNDING GROWTH AREA

Costs for infrastructure items that will benefit development proximate to the WRSP Area could be funded by allocating costs to each City sub-area based on "benefit received." An example of this type of infrastructure benefit may be the Rio Del Mar rail underpass.

9.4.1.3 WATSON RANCH SPECIFIC PLAN

Costs for infrastructure items that are directly related to and will benefit development within the WRSP Area shall be funded primarily by private development within those areas.

9.4.1.4 BACKBONE INFRASTRUCTURE COST ALLOCATION

The cost for the Master Backbone infrastructure, which includes backbone roadways (see Figure 9.5) and associated wet and dry utilities, and parks, will be allocated on a prorated basis (\$/acre) between the two primary ownerships (AC-1 and the Newell Family properties) within the WRSP Area. Specific costs and allocations will be determined through Development Agreement(s) and subsequent Tentative and Final Subdivision Map and Improvement Plan approvals.

9.4.1.5 SPECIFIC PLAN PREPARATION COSTS AND COSTS OF STATE AND FEDERAL PERMITS

Pursuant to controlling law (Government Code Section 65456 (a)), the City legally can require the property (and its property owners) within the WRSP to pay for the costs associated with the preparation and adoption of the WRSP.

In this case, the costs associated with the preparation and adoption of the WRSP include, without limitation, the costs associated with environmental review of the WRSP, the costs associated with the planning and financial consultants hired to assist with the preparation and adoption of the WRSP and related General Plan Amendment and Zoning criteria, the costs associated with funding and administering the preparation and adoption of the WRSP, the costs of securing all state and federal permits, and similar direct and indirect costs (WRSP Costs).

Generally, the AC-1 Property shall be responsible for approximately eighty-two percent (82%) of the total WRSP Costs, whereas the Newell Family Property shall be responsible for eighteen (18%) of the total WRSP Costs. The owner of AC-1 portion of the property has already paid for the bulk (if not all) of the WRSP Costs. Therefore, the City, after adopting the WRSP, shall determine the total WRSP Costs, the City shall determine the amount the Newell Family Property owes toward those total WRSP Costs, and the City shall impose a requirement that the Newell Family Property pay its portion of the WRSP Costs before it is approved for any development. Reimbursement for WRSP Costs may also be addressed in Development Agreement(s).

9.4.2 INFRASTRUCTURE AND IMPROVEMENTS

The infrastructure requirements for the WRSP Area are composed of a variety of backbone infrastructure projects including land dedications, roadways, water supply improvements, and sewer infrastructure. The WRSP Area also includes a variety of other public facilities that have community benefits beyond the WRSP Area.

9.4.3 FINANCING PRINCIPLES AND POLICIES

The following principles and related policies will govern the funding of infrastructure and public facilities for the WRSP Area. The principles will guide future decisions regarding formation of financing entities, adoption of financing mechanisms, and project approvals.

Note: The approved Development Agreement between AC-1 and the City of American Canyon establishes precise direction implementing the WRSP principles and policies. In any areas of conflict, the terms and conditions of the Development Agreement shall control.

1. A financial analysis and funding strategy shall be prepared that identifies the private and public infrastructure and facilities (improvements) required for the WRSP Area, the allocation of these costs, and proposed methods for funding.

Policy 1.1: The financial analysis and funding strategy shall take into account the phasing and build out of the proposed development and all improvements needed to accomplish the development and their respective costs, and it should provide a financial analysis of all funding mechanisms proposed.

2. The WRSP shall not be a burden to existing residents and will cover the pro rata cost for development associated with the build out of the WRSP Area.

Policy 2.1: Funding mechanisms shall be established such that new development and property owners within the WRSP and in the City fund the infrastructure and facilities required to provide needed public services and utilities to the WRSP Area.

Policy 2.2: Existing residents within or nearby the WRSP Area should not be burdened with assessments or taxes to pay for new public facilities if no benefit is received. Any new assessments or taxes should be directly related to the benefit received and not place an undue burden on these residents.

Policy 2.3: City development impact fees should be reevaluated to determine the development impact

fee amount that new development outside the WRSP Area should have to pay toward infrastructure improvements and facilities included within the WRSP Area. Additionally, a partial or full impact fee credit may be granted for public infrastructure and facilities that are provided as part of new development in the WRSP Area (as further described in Policy 7 below).

Policy 2.4: Properties outside the WRSP Area that benefit from public improvements provided by the WRSP should contribute their fair share to public improvement funding.

3. Future development within the WRSP Area shall pay the costs of mitigating impacts of the WRSP development on existing facilities and infrastructure in other parts of the City as identified in the WRSP FEIR and Mitigation Monitoring Program and as set forth in the Development Agreement(s). Future development within the WRSP Area shall not pay the cost of addressing existing deficiencies in parks and all other public improvements in the City of American Canyon.

Policy 3.1: WRSP Area development shall mitigate offsite public infrastructure and facility impacts as specified in the WRSP Environmental Impact Report. WRSP Area development shall pay existing impact fees, help fund public improvements, or undertake needed public improvements in order to mitigate impacts on existing facilities and infrastructure in other parts of the City as defined in and determined by approved Development Agreement(s).

4. Infrastructure costs shall be allocated among WRSP Area and surrounding properties based on the principle of benefit received (fair share allocation or rational nexus).

Policy 4.1: The City shall establish, on the basis of the financial analysis, a fair-share cost allocation for required public improvements to be borne by all benefiting new development within and adjoining the WRSP Area. These public improvement cost obligations shall be funded by developer construction and dedication of improvements, as an area development impact fee, and/or included in a local land-secured financing district covering the WRSP Area.

Policy 4.2: The City shall identify land (e.g., right of way) requirements for public improvements and establish a "fair share" (average) allocation to all developers and property owners in the WRSP Area.

5. Total public infrastructure and financing costs in the WRSP Area should not exceed industry standards of financial feasibility or benefit/cost burden on property owners.

Policy 5.1: Develop a detailed infrastructure phasing schedule that links the timing of infrastructure and facility construction to the timing of new residential, commercial/retail, and public facility development to the extent possible.

Policy 5.2: The City should encourage measures that minimize public improvement costs borne by new development in the WRSP Area. Such measures could include cost sharing agreements with other benefiting areas, use of revenue bond funding for utility improvements, and construction and reimbursement agreements with developers, which can lower costs generally associated with public construction projects. The City shall consider credits against City fees to the extent permissible by law and for public facilities constructed by WRSP Area developers that would otherwise be funded by City fees.

6. The City should assist in securing methods of WRSP Area infrastructure financing where feasible through the establishment of necessary financing entities and arrangements.

Policy 6.1: The City should assist in coordinating landowners, developers, and other jurisdictions in the establishment of the appropriate funding mechanisms.

Policy 6.2: At the request of landowners or developers in the WRSP Area, the City shall assist in the establishment of a financing district or districts (e.g., Community Facilities District) to provide necessary land-secured debt financing.

7. Generally, improvements that are to be located on a particular property in the WRSP Area shall be improved by the owner of such property (land). If such a landowner is required to make improvements (e.g., oversizing public infrastructure) with a higher value than the benefits that landowner receives, the excess value shall be reimbursed to such landowner from other benefiting properties.

Policy 7.1: The City shall require dedication of land for road improvements and construction of public improvements consistent with City policies and capital improvement programs.

Policy 7.2: The City shall require development projects in the WRSP Area to fund or support financing for oversizing of facilities if required by the City and as set forth in the Development Agreement(s).

Policy 7.3: The City shall consider mechanisms for future development to reimburse landowners or developers who oversize infrastructure or dedicate

excess land, possibly as part of WRSP fee or related credit and reimbursement agreements.

Policy 7.4: The City shall consider credits against City impact fees to the extent that WRSP Area landowners or developers build public improvements and/or dedicate land that would otherwise be funded by City impact fees and/or provide community benefit and to the extent permissible by law.

8. Mechanisms shall be identified or established to assure ongoing operations and maintenance of public improvements and extension of public services to accommodate new development.

Policy 8.1: Landowners or developers shall consider participating in duly established financing mechanisms that provide funding for operation and maintenance of public facilities and/or municipal services such as public safety services within the WRSP Area, to the extent standard tax revenues are insufficient to support appropriate levels of maintenance.

9.4.4 FINANCING MECHANISMS AND RESOURCES

A number of funding and public financing mechanisms may be used to fund the public services, facilities, and infrastructure associated with the development in the WRSP Area. The ultimate mix of funding mechanisms shall be determined in the implementation process based on final technical analyses of costs, benefits and burdens on existing and future property owners, and on deliberations involving City staff, property owners, developers, elected officials, bond counsel, underwriters and finance experts.

This chapter describes the key features of the funding mechanisms available to finance WRSP Area infrastructure development. The mechanisms discussed in this section fall into four distinct categories:

1. Area-specific fees, dedications, and exactions
2. Assessment and special tax secured financing
3. Citywide sources
4. Federal and State grants
5. Other sources

Potential financing mechanisms including without limitation are discussed in the following section.

WATSON RANCH SPECIFIC PLAN AREA FEES, DEDICATIONS, AND EXACTIONS

WATSON RANCH SPECIFIC PLAN IMPACT FEES

WRSP impact fees may be enacted by a legislative body (i.e., city or county) through adoption of an ordinance. Such fees, if adopted prior to the development of the WRSP, do not require a public vote to be enacted, but they do require public hearings. WRSP fees must be directly related to the benefits received. They do not create a lien against property, but must be paid in full as a condition of approval. The principal use of these fees is to construct backbone infrastructure and facilities essential to the development of the WRSP Area that is not otherwise part of the City-wide impact fee program or other City funding source. WRSP fees are established so that all properties within the WRSP Area pay their fair share at the time they are developed. The WRSP fee effectively replaces a myriad of other funding devices (e.g. Area of benefit assessments, etc.). Proceeds may be used to reimburse property owners who pay up-front costs for facilities benefiting other properties.

DEDICATIONS AND EXACTIONS

Under the Subdivision Map Act, developers may be required to dedicate land or make cash payments for public facilities required or affected by their project (e.g., road right-of-way). Dedications are typically made for road and utility rights-of-way, park sites, and land for other public facilities. Cash contributions are made for other public facilities that are directly required by their projects (e.g., payments for a traffic signal).

LAND SECURED FINANCING

SPECIAL ASSESSMENT DISTRICTS (1911, 1913, 1915 ACTS)

California law provides procedures to levy assessments against benefiting properties and issue tax-exempt bonds to finance public facilities and infrastructure improvements. Assessment districts, also known as improvement districts, are initiated by the legislative body (e.g., city council), subject to majority protest of property owners. Assessments are distributed in proportion to the benefits received by each property as determined by engineering analysis, and form a lien against property. Special assessments are fixed dollar amounts and may be prepaid, although they are typically paid back with interest over time by the assessed property owner. Only improvements with property-specific benefits (e.g., roads, and sewer and water improvements) may be financed with assessments.

MELLO-ROOS COMMUNITY FACILITIES DISTRICTS

California's Mello-Roos Community Facilities Act of 1982 allows for the creation of a special district authorized to levy a special tax and issue tax exempt bonds to finance public infrastructure, facilities and services. A CFD may be initiated by the legislative body or by property owner petition, and must be approved by a two-thirds majority of either property owners or registered voters (if there are more than 12 registered voters living in the area). Special taxes are collected annually with property taxes, and may be prepaid if prepayment provisions are specified in the tax formula. The special tax amount is based upon a special tax lien against the property. There is no requirement that the tax be apportioned on the basis of direct benefit. Because there is no requirement to show direct benefit, Mello-Roos levies may be used to fund improvements of general benefit, such as schools, fire and police facilities, libraries and parks, open space, detention basins, etc. as well as improvements that benefit specific properties. The provision also allows for the allocation of cost burdens to alleviate burdens on specific classes of development.

ENHANCED INFRASTRUCTURE FINANCING DISTRICT

The Enhanced Infrastructure Financing District (EIFD) is a new financing tool (see Government Code Section 53398.50 et, seq.) providing broad authority to local agencies to use tax increment to finance a wide variety of projects ranging from infrastructure such as roads, bridges and wastewater facilities to parks and open space, affordable housing, and other community facilities. An EIFD can be used for a single street, in a neighborhood or throughout the entire City. With the WRSP Area being completely undeveloped, there is a unique opportunity to capitalize on the future tax increment value associated with the project build out to help fund significant infrastructure improvements associated with the WRSP Area. The City, working with the developers, will evaluate and initiate the use of an EIFD, as deemed appropriate.

LANDSCAPE AND LIGHTING MAINTENANCE DISTRICTS

Landscaping and lighting maintenance districts (LLMDs) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLMDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities. They may also be used to fund and maintain parks above normal park standards maintained from General Fund revenues. The City has an existing LLAD and may determine that the WRSP Area join the existing LLAD as a new zone.

CALIFORNIA STATEWIDE COMMUNITIES DEVELOPMENT AUTHORITY (CSCDA)

California Statewide Communities Development Authority (“CSCDA”) allows cities to provide CFD financing for infrastructure without acting as the issuer of the bonds. The City of American Canyon is a member of the Statewide Community Infrastructure Program (“SCIP”), a program run by CSCDA. In the event Developer elects to proceed with CSCDA financing, Developer shall notify City of its intent to proceed with CSCDA and CSCDA shall issue the bonds while adhering to the City’s policy’s pertaining to tax rates and value to lien ratios.

BENEFIT COVENANT TRANSFER FEE

The project applicant is assessing the appropriateness of a Benefit Covenant Transfer Fee on all properties in the WRSP Area. The fee, set at a percentage of sale price, would be levied on property transfer transactions. Such a fee may be used to create a long-term revenue stream that could be applied to infrastructure development or facilities maintenance in the WRSP Area.

CITYWIDE DEVELOPMENT IMPACT FEES

The City has a number of existing impact fees. These fees may provide funding as credits to offset the costs of developing backbone infrastructure in the WRSP Area for improvements of citywide benefit. Impact fee credits may only be used on City-wide facilities included in the City’s impact fee program for like improvements (e.g., traffic impact fee for traffic improvements credit) or as otherwise approved in the Development Agreement(s).

FEDERAL AND STATE GRANTS

Investigation of potential funding for public facilities from other levels of government is appropriate. Since the availability of funding from these sources is unknown, it has not been assumed that these sources would be available for development financing.

9.4.5 IMPLEMENTATION

The following Implementation Plan describes the process that shall be undertaken to establish the required financing mechanisms and build required infrastructure associated with development of the WRSP Area. The Action Steps detail the method to be followed by the City, in cooperation with development interests in the WRSP Area, to establish the preferred financing mechanisms. The implementing actions have been specified to respond to varying circumstances, including variations in the infrastructure financed under the WRSP and the intensity of WRSP Area development. The recommended steps are presented according to their sequence and relative importance.

ACTION STEPS

6. Establish infrastructure financing requirements and obligations.

Action 1.1: Assure consistency with WRSP EIR.

The EIR prepared for the WRSP is required to evaluate impacts of the project upon public facilities and services. This evaluation should be coordinated with parallel efforts by the City or the WRSP developers. Mitigation measures, to the extent they are required, should be consistent with the financing mechanisms implemented pursuant to the WRSP.

Action 1.2: Draft and adopt a Development Agreement(s) with WRSP developers.

The standard Development Agreement shall be offered to all developers in the WRSP Area and modified to meet each developer’s particular circumstances. The Development Agreement is viewed as necessary to convey development program entitlements commensurate with the WRSP, in exchange for the financial commitments that will be asked of the developers (including participation in land secured financing mechanisms and acceptance of the area development impact fees).

Action 1.3: Reflect infrastructure and financing obligations in tentative subdivision maps.

The City shall establish a standard set of subdivision map conditions for certain subdivision map applications within the WRSP Area. These conditions should incorporate obligations for funding specific improvements, land dedications needed for public improvements; and participation in any financing districts established.

7. Finalize cost and phasing assumptions.

Action 2.1: Identify final set of facilities.

Before financing mechanisms can be established, the City must provide direction and clarity concerning the backbone infrastructure and facilities to be financed as a part of the WRSP. This facility identification should be done in cooperation with the involved property owners or developers within the WRSP Area.

Action 2.2: Formulate final cost estimates.

The City must also establish a set of infrastructure costs that will be financed within the WRSP. The infrastructure cost estimates included in this report can be “preliminary and for planning purposes.” More specific engineering estimates will need to be prepared as actual financing mechanisms are adopted.

Action 2.3: Establish infrastructure phasing based upon development priorities.

This task serves to ensure that a financially feasible and acceptable Financing Plan can be created to support development in the WRSP Area.

8. Establish preferred financing mechanisms.

Implementing the Financing Plan within the context of overall WRSP implementation may require that the City, or State Agency, establish a CFD or assessment district to help improve the developer’s ability to meet the City’s infrastructure requirements, both with regard to amount and timing of the improvements.

9.5 Maintenance of Common Facilities

Common facilities within the WRSP Area will include both public and private facilities. Determination of public versus private shall be established through the tentative map process. The City shall be responsible for maintenance of public facilities while private facilities shall be maintained under the framework of a Master Homeowner’s Association, with sub-associations for individual neighborhoods as deemed appropriate.

9.6 Implementation Policies

9.6.1 ADMINISTRATION

9.6.1.1 DEVELOPMENT AGREEMENTS

To strengthen the public planning process, encourage private participation in comprehensive planning, and reduce the economic risk of development, the Legislature of the State of California adopted Government Code sections 65864 et seq. (“Development Agreement Statute”), which authorizes the City to enter into a development agreement with any person having a legal or equitable interest in real property providing for the development of that property and establishing certain development rights in the property. The City of American Canyon Municipal Code Chapter 19.47 further establishes the authority for the City to enter into development agreements.

The Development Agreement by and between American Canyon 1, LLC (AC-1) and the City of American Canyon (Ordinance 2019-06) establishes the administration of the terms and conditions of the WRSP. The Development Agreement ensures predictable and equitable allocation of

costs of basic and shared infrastructure, reimbursements of improvement costs benefiting other properties, and provisions for reciprocal and mutually benefiting right-of-way dedications and grants of easements for public and private infrastructure. The administration of the terms and conditions of the WRSP for the development of property owned by the Newell Family may be established in separate development agreement(s) between the Newell Family and the City of American Canyon.

The Development Agreement is subject to future modification by a mutually executed document signed by the parties thereto. In the case of a conflict between the WRSP’s description of the Development Agreement and the terms and conditions of the Development Agreement, the terms and conditions of the Development Agreement shall control.

9.6.2 ADOPTION

The WRSP was adopted by ordinance on October 16, 2018 (Ordinance 2018-08), in accordance with Section 19.17.070 of the American Canyon Municipal Code.

9.6.3 AMENDMENT

Amendment of the WRSP shall be done in the same manner as adoption of the WRSP. Pursuant to City of American Canyon Municipal Code Section 19.17.070 (D), the WRSP provides provisions for minor and major amendments.

9.6.3.1 WRSP AMENDMENTS - MAJOR

Major WRSP Amendments are those determined by the Community Development Director to be changes that could significantly alter the design intent and vision of the WRSP or affect environmental impacts and approved mitigation measures. Major Amendments to the WRSP must be reviewed and approved by Planning Commission and City Council. The following will likely be considered to be a major WRSP Amendment:

- Introduction of new land use categories.
- Changes in the boundaries of the Land Use-Zoning categories in conjunction with the approval of a tentative subdivision map or parcel map, resulting in an increase or decrease in area of more than 20% or 5 acres, whichever is greater.
- Changes to the circulation system or community facility design which would materially affect a planning concept detailed in this WRSP, or this WRSP as subsequently amended.
- Density increases in excess of the maximum allowable stated density or Density Transfers in excess of limits set in Section 9.8 below.

- Any change that would result in a significant adverse environmental impact.
- Goals, Objectives and/or policies changes.
- Changes that would substantially alter the design character or vision of the WRSP.

9.6.3.2 WRSP AMENDMENTS - MINOR

Minor WRSP Amendments are those amendments determined by the Community Development Director to be consistent with the intent of the Plan's vision, goals, objectives and policies. Minor WRSP Amendments may be denied or approved at the discretion of the Community Development Director with conditions imposed to safeguard public safety and health. No Planning Commission or City Council review is required for Minor Amendments unless an Appeal is filed. Examples of Minor Amendments to the WRSP subject to the approval of the Community Development Director include, but are not limited to, the following:

- An amendment that will not significantly affect the physical character, goals or intent of the WRSP.
- An amendment that will not be materially detrimental to the public welfare or injurious to property or improvements in the vicinity.
- An amendment that will not significantly increase environmental impacts beyond levels identified in the WRSP FEIR.
- An amendment to the sequencing or size of phases described in the Phasing Plan.
- An amendment to a Land Use-Zoning boundary in conjunction with the approval of a tentative subdivision map or parcel map, that does not result in an increase or decrease in area of more than 20% or 5 acres, whichever is less.

9.6.3.3 REQUIRED FINDINGS

All amendments to the WRSP must be found to be consistent with the City of American Canyon General Plan and this WRSP. Individually, major and minor amendments must have supporting findings which document their consistency with the sections above. For Minor Amendments, the Community Development Director shall include the findings

in a letter of approval, a copy of which is to be kept on file with the City. Findings required are:

- Changes have occurred in the community since the adoption of the WRSP which warrant approving the proposed amendment.
- The proposed amendment is consistent with the City of American Canyon General Plan.
- The proposed amendment will result in a public benefit to neighborhoods within the WRSP Area.
- The proposed amendment will not result in any unmitigated impact to adjacent properties.
- The proposed amendment will enable the delivery of services and public facilities to the population within the WRSP Area.

9.7 Subsequent Entitlements

The WRSP sets in place the vision and approach to development of the property within the WRSP Area. As a part of implementing the WRSP vision, additional entitlements are required. Some of these may be processed in conjunction with the WRSP, but most will likely follow the adoption of the WRSP. The entitlement approvals anticipated to be granted by the City of American Canyon subsequent to adoption of the WRSP are described below. The City's process for these entitlements will apply.

9.7.1 MAPPING AND DESIGN PERMITS

9.7.1.1 SUBDIVISION MAPS

Development proposals requiring division of property will require the filing of a subdivision map and review and approval by the City of American Canyon. Tentative maps must be consistent with the goals, objectives, policies and standards set forth in the WRSP. All subdivisions within the WRSP Area will be governed by the State Subdivision Map Act (Government Code Sections 66410-66499.58) as well as City of American Canyon regulations and policies not included in the WRSP (see Municipal Code Chapter 18.22).

All projects with approved maps must submit final subdivision maps to the City Engineer for review and approval. Final maps must be consistent with the tentative map, must include all information required by the City of American Canyon Municipal Code, and will be governed by the Subdivision Map Act (Government Code Sections 66410-66499.58). Subdivision maps must be recorded

prior to the expiration of the tentative maps. Multiple final maps may be filed with each phase of development when approved as part of the tentative map entitlement stage.

9.7.1.2 IMPROVEMENT PLANS

Implementation of the WRSP will require the submittal and approval of improvement plans by the City. Improvement plans must include detailed engineered grading, street improvement and utility plans for both on and off site improvements. All improvement plans shall comply with the requirements of the WRSP and other City standards not otherwise specified in the WRSP.

9.7.1.3 DESIGN PERMITS - ARCHITECTURE AND SITE APPROVAL

All Architecture and Site approvals require a Design Permit. Development proposals shall be consistent with the goals, policies and regulations of the WRSP and shall implement the Community Design Guidelines set forth in the WRSP, Appendix A. Architectural and Site Approval shall be required for all development within the Watson Ranch Area and shall be subject to review and approval of the City staff and Planning Commission pursuant to the City of American Canyon Municipal Code Sections 19.41.010 through 19.41.060. Applications shall provide a site development plan, architectural design drawings, landscaping plans and any other information deemed necessary by the American Canyon Community Development Director. All Design Permits may be processed concurrently with Tentative Tract Map application(s).

A single Design Permit may be processed for the NVRG Area. The Design Permit will include all that area defined as NVRG on Figure 4.2 and may include the adjoining open space properties and will include the Parking Management Plan (PMP). This Design Permit may serve as a master plan for the ongoing implementation of the NVRG.

9.7.1.4 CONDITIONAL USE PERMITS

Uses defined as conditional uses under each of the land use categories within the WRSP Area shall be subject to the City of American Canyon Municipal Code Section 19.42.020. An application for conditional use permit shall be submitted to the City of American Canyon for review by City staff and the Planning Commission. The Planning Commission shall review the application at a properly noticed public hearing and consider approval or denial of the Conditional Use Permit pursuant to the required findings set forth in 19.42.020 (D).

9.7.1.5 OTHER APPROVALS

Other approvals as may be required by the City of American Canyon and any other public agency having legal jurisdiction regarding development and use of property within the WRSP (e.g., final map approvals, execution of subdivision improvement agreements, building permits, certificates of occupancy, etc.).

9.7.2 ENVIRONMENTAL REVIEW/CEQA EXEMPTION

In advance of adopting this WRSP, the City of American Canyon Council Certified an Environmental Impact Report (EIR) evaluating the significant environmental impacts associated with buildout of the WRSP and adopted mitigation and monitoring program which has been incorporated by reference into the WRSP.

Under the California Environmental Quality Act (CEQA), the WRSP is a “project” subject to evaluation of potential adverse impacts to the environment. The information obtained in a project’s environmental review provides decision makers with the insight necessary to guide policy development, thereby ensuring that the WRSP’s policies will address and provide the means to avoid potential environmental impacts.

Although the EIR and the WRSP are separate documents, it is important to note that the environmental review process has been an integral component of the planning process to ensure the WRSP’s sensitivity to critical environmental concerns. The WRSP was reviewed as a project anticipated to be developed over an extended period of time. This approach enabled the City of American Canyon to comprehensively evaluate the cumulative impacts of the WRSP and consider alternatives and area-wide mitigation measures in conjunction with the adoption of the WRSP.

Subsection (a) of Government Code Section 65457 states: “Any residential development project, including any subdivision, or any zoning change that is undertaken to implement and is consistent with a WRSP for which an environmental impact report has been certified after January 1, 1980, is exempt from the requirements of Division 13 (commencing with Section 21000) of the Public Resources Code. However, if after adoption of the WRSP, an event as specified in Section 21166 of the Public Resources Code occurs, the exemption provided by this subdivision does not apply unless and until a supplemental environmental impact report for the WRSP is prepared and certified in accordance with the provisions of Division 13 (commencing with Section 21000) of the Public Resources Code. After a supplemental environmental impact report is certified, the exemption specified in this subdivision applies to projects undertaken pursuant to the specific plan”.

A large portion of the WRSP involves residential neighborhoods that will be eligible for the exemption from CEQA under this provision.

As specific development projects are proposed within the boundary of the WRSP, the City of American Canyon will evaluate if any additional environmental review is required under CEQA. The environmental document and review process required will depend on the nature of the project and circumstances at the time.

9.8 Unit Transfers

It is the intent of the WRSP to permit flexibility in adjusting unit and product mix in response to market demand. Transfers of residential units between neighborhoods and residential land use categories are permitted as long as the overall number of units does not exceed the total project entitlement of 1253 units. In addition to the 1253 units, 186 affordable units are provided at Lemos Point, and additional allowed residential units may be secured through density and affordability bonus law. Additionally, the area density resulting from unit transfers shall not exceed the maximum allowable density for each residential Land Use-Zoning category specified in the WRSP. The number of units in each land use category (MDR-12, MDR-16, HDR) as defined by WRSP (see Table 4.1) may be increased administratively a maximum of 10%. Unit transfers must be identified as part of a final subdivision map and are subject to the approval of the Community Development Director. There is no limit on the total unit decrease in any given neighborhood.

Up to 10% of the total units may be transferred from the residential area to the Mixed Use area (NVRG) as integrated mixed use residential units. This allocation of density may also be applied to the Hotel on a unit to room equivalent (e.g. 10 units = 10 rooms).

The request for unit transfer may be administratively approved by the Community Development Director. Each request must specify the total number of units being adjusted, including a summary showing what area the units are transferring from, with a running total for the entire WRSP Area of individual ownership. Transfers of units between the separate ownerships (e.g. AC-1 and Newell Family) shall not be permitted without written consent of all parties involved.

Unit transfers must not result in impacts beyond those identified in the WRSP Environmental Impact Report.

APPENDIX A: DESIGN GUIDELINES

A.1 Purpose and Intent of the Design Guidelines

The Watson Ranch Specific Plan (WRSP) design guidelines seek to create a unique, memorable identity to reinforce the City of American Canyon as a destination within Napa County.

This design direction utilizes simple forms with an architectural palette appropriate to wine country living. The design guidelines are statements that describe the desired visual character within the WRSP. They are working tools that implement the Vision and Design Strategy provided in Chapter 4. The design guidelines address issues that are primarily aesthetic in nature.

The design guidelines establish a range of encouraged design approaches while allowing for flexibility and innovation. The design guidelines are designed to provide the City of American Canyon with the necessary assurance that the WRSP Area will develop in accordance with the quality and character proposed herein; and to provide guidance to City staff, the Planning Commission, and the City Council in the review of future development projects in the WRSP Area.

The design guidelines address each land use: Napa Valley Ruins & Gardens Mixed Use (MU), High Density Residential (HDR), Medium Density Residential (MDR), Parks, and Open Space; and general categories of community monumentation, as well as walls and fencing. The intent of the design guidelines is to encourage innovative, quality design for all areas of the WRSP Area.

Per City of American Canyon Code, all development proposals must be reviewed by the Planning Commission. Any changes to a building and/or facade for any land use submitted after Planning Commission approval should be reviewed and approved administratively by the planning director or the designee. Should they deem that there has been too great a modification to the plans previously recommended for approval by the Planning Commission and approved by City Council, then the particular building elevations shall be re-submitted for review and approval.

A.2 Sustainability Design Guidelines

The sustainability of a community depends on creating and maintaining its economic and environmental health. It is important that for communities to be successful, they develop principles and a collective vision for the future that applies an integrative approach to environmental, economic, and social goals. The WRSP aims to achieve a high level of sustainability incorporating a three tier approach to sustainability through an effective management of resources at all levels of planning, design, and construction.

TIER 1 DESIGN GUIDELINES

The first tier involves using sustainable principles in community design and overall master planning. An integral relationship exists between how a community is planned and developed through its form, configuration, and use along with its capacity to meet its social, environmental, and economic needs. Community form, which represents the needs and priorities of the community, directly influences community capacity to sustain itself into the future. The sustainability design guidelines used in the overall planning of the WRSP Area include:

- Vehicle miles travelled should be reduced through the provision of a mixed-use community that accommodates a wide range of uses.
- A wide range of housing types should be provided that allow for varied housing opportunities.
- Non-vehicular circulation should be emphasized through the creation of compact, interconnected walkable blocks.
- A healthy lifestyle should be promoted through an integrated system of trails and pedestrian connections.
- Narrow street sections should be provided to reduce surface runoff and reduce urban heat island effect.

- Environmentally sensitive areas should be preserved where possible.
- Existing structures and materials at the NVRG should be reused where possible.
- Features such as electric charging stations and solar panels should be provided at key community destinations.
- Wherever applicable, the project should promote community-wide awareness on sustainability through outreach and education. Awareness techniques may include, but not be limited to LEED certified buildings incorporating signage on the sustainable design features on the built environment, landscape signage educating the community on sustainability techniques in the natural environment, and community-wide classes and seminars to make the residents aware of what they can do to help.
- Incorporate horizontal mixed-use into the community to create a walkable, pedestrian friendly community with less need for automobiles.
- If possible, accommodate a public transit stop centrally located near the core activity hub.
- High Performance Attics;
 - o High Performance Walls;
 - o High Performance HVAC Systems
 - o High Performance Windows;
 - o Tankless Water heaters; and
 - o Cooling Ventilation and house fans;
- Consider incorporating a “cool roof” which reduces the amount of heat conducted to the building. This can be achieved by using highly reflective or “cool color” roofing products.
- Outdoor lighting should incorporate fixtures that produce light using light-emitting diodes (LEDs) which use at least 75 percent less energy than incandescent lighting.
- Residences and public/commercial buildings should be designed to accommodate electric vehicle charging stations.
- Incorporating signage on the sustainable design features on the build environment, specifically LEED certified buildings.

TIER 2 DESIGN GUIDELINES

The second tier involves a commitment to using sustainable green building techniques. The technology exists to create buildings that are smarter, more energy efficient, and healthier than those of the past. Homes and commercial buildings should be built incorporating the finest building and energy saving techniques available. The sustainable green building techniques that might be used in the built form include:

- Passive energy should be used to reduce dependency on mechanized energy systems.
- Recycled, refurbished or sustainability farmed building products should be incorporated.
- A lifecycle analysis should be used when choosing building materials.
- Homes and buildings should meet strict energy performance standards by incorporating ENERGY STAR compliant products such as appliances, building, heating and cooling, and lighting products in an effort to reduce greenhouse gas emissions.
- Reduce the energy consumption in homes by the use of energy efficient design and construction methods. These could include, but not limited to:

- Homebuilders providing buyers with a system option for the homes to include photovoltaic (solar) and solar water-heating systems. Being solar-ready may include:
 - o Having a designated solar zone
 - o Adequate electric capacity
 - o Designated conduit and plumbing paths
- Commercial parking incorporating photovoltaic (solar) panels over the parking, or solar turbines located on the light poles to harvest energy.

TIER 3 DESIGN GUIDELINES

The third tier involves a commitment to sustainable landscape techniques. The desire to create community in accordance with green building principles influences the decisions regarding the landscape design, recreational programming, and physical layout of each development area. The sustainable landscape techniques that may be used include:

- Drought tolerant plants should be selected to reduce water consumption based upon:
 - o Their adaptability to the natural conditions of the site.

- o Protection and preservation of native species and natural vegetation
- o Disease and pest resistance
- o Water-conserving plant and turf species
- o Selection of plants from the East Bay Municipal Water District's "Plants and Landscape for Summer-Dry Climates" as the primary reference.
- Plant materials should be selected that promote a diversity of flora and fauna.
- Recycled water for landscape should be considered to reduce the consumption and demand of potable water.
- Pursuant to Zoning Ordinance Chapter 19.22.040 no turf is permitted in front yards and in other areas turf areas should be minimized.
- Low impact development principles should be implemented to reduce the impact of the development on the natural environment.
- Incorporate infiltration beds, swales, and basins into the design of a project to allow water to collect and soak into the ground, as required by local agency.
- Utilize pervious or porous surfaces (permeable pavers or blocks) to minimize runoff.
- Encourage the use of rain harvesting or catchment technologies (rain gardens, canisters, etc.)
- Projects shall incorporate The City of American Canyon Model Water Efficient Landscape Ordinance (MWELO) into the design.

A.2.1 SUSTAINABLE LANDSCAPE

Sustainable landscape practices proposed for the WRSP Area may include Low Impact Development techniques, innovative irrigation technologies and water conservation, drought-tolerant landscape, and energy conservation.

A.2.1.1 LOW IMPACT DEVELOPMENT (LID) LANDSCAPE

The term LID refers to storm water management techniques that use vegetation and open space to optimize natural hydrologic processes and reduce stormwater runoff.

Areas incorporated within the WRSP Area that provide treatment include NVRG, parks, community gardens, detention basins and open space. All project areas should incorporate the following sustainable landscape practices:

- Large canopy street trees should be used wherever appropriate to intercept rainwater, encourage root uptake, and facilitate evapotranspiration.
- Bio-retention systems in conjunction with vegetated swales should be incorporated where appropriate in open space and other landscape areas.
- Where space allows, residential areas should include landscape treatments that provide on-lot detention, filtering of rainwater, and groundwater recharge.
- Where suitable, permeable surfaces such as unit pavers, turf block, gravel and permeable concrete, should be used in place of paving;
- Tree and plant species for bio-retention and bio-swale areas should be selected from the approved plant palette shown in Appendix A.

A.2.1.2 IRRIGATION AND WATER CONSERVATION

Irrigation systems should be designed to conserve water resources by efficiently and uniformly distributing water. Less watering, fertilizing, and chemical control required for landscape design reduces the need for irrigation and associated water use.

- Irrigation design should follow California Department of Water Resources ordinances, Napa County Water Ordinances and tailored to the climate of American Canyon.
- Irrigation should be provided for plant establishment.
- All public areas, rights-of-way, and community landscaping should have an automatic irrigation system.
- Use of low volume spray heads and drip irrigation systems should be maximized.
- New irrigation techniques and drip irrigation systems should be used for efficient delivery of water.
- Irrigation design should accommodate hydrozones accordingly, separating high, medium and low water-use plants.

- Automatic irrigation systems should include a rain shutoff valve or weather based control system.
- Shrubs and trees should be irrigated with a drip system or MPR heads to provide deeper, more even watering and promote water conservation.
- Irrigation controls should be screened from view from the street by landscaping or attractive site materials.
- Soils should be mulched with 3 inches of organic material to reduce evaporation, keep the soil temperature even, and control weeds.

A.2.1.3 DROUGHT TOLERANT LANDSCAPE

- Plant selection should emphasize the use of native, drought-tolerant, long-lived, pest-resistant plant species that are well adapted to the climatic and soils conditions of the site.
- Turf should be limited to parks or other active uses and high visibility areas as allowable.
- Low groundcover and drought tolerant grasses should be used as an alternative to turf wherever possible.
- Sustainable maintenance techniques should be used through the use of organic fertilizers and weed/pest control products through literature provided to future residents, including local resources for procurement of drought-tolerant plant materials.



Native, drought tolerant landscaping

A.2.1.4 ENERGY CONSERVATION

- Planting design should consider location and orientation when adjacent to buildings to maximize solar orientation and reduce building heating and cooling.
- Landscape around buildings should be designed to provide shading in the summer months and solar access during the winter.
- Energy-efficient landscaping techniques are encouraged such as use of local materials, and on-site composting, to reduce green waste hauling.

A.3 Landscape Strategy

The landscape design for the WRSP Area emphasizes drought-tolerant, native and edible landscaping and provides abundant trees for beauty, definition of spaces, habitat enhancement, and comfortable & healthy community in American Canyon. Attractive and shaded streets, parks, community gardens, buffers, and trails will establish strong community character and quality living environments, both in the near future and in the long term. In addition, the WRSP incorporates green infrastructure or low impact development landscape practices.



Drought tolerant landscaping

A.3.1 OVERALL LANDSCAPE DESIGN GUIDELINES

The following design guidelines pertain to all initial planting within the WRSP Area. As applicable, landscaping within the WRSP Area will be subject to any special requirements identified by soils or inherent drainage conditions.

- Plant materials should be selected from the plant palettes in Appendix A, with an emphasis on native and adaptive plants, where appropriate.
- Substitutions or additions may be considered based on the suitability of the species in terms of similarity of form, adaptability, tolerance to site soils, climatic conditions or water quality, or other pertinent characteristics.
- The plant list is not intended to be exhaustive but to provide a clear guide for selection.
- Additional plants may be used that are compatible with this list and are consistent with the intent of these Guidelines.
- Plant selection should emphasize the use of drought-tolerant, long-lived plant species that are well adapted to the climatic and soils conditions of the site.
- Planting design should consider year-round interest and seasonal character through the careful use of flower, leaf color and texture.
- Landscape design should provide effective screening of parking areas, retaining walls, utility enclosures, utility cabinets, service areas, or transportation corridors to reduce negative visual impacts.
- Screening landscape should incorporate evergreen plant species in order to maintain year-round leaf cover.
- Plant selection should avoid the use of tree species with invasive root systems near utility lines and paving and avoid the use of non-native, invasive species that may spread into open space areas.
- Street trees should be placed in parkway strips between the curb and sidewalk, with drought tolerant shrub and ground cover plantings to encourage sidewalk use.
- Pedestrian trails or walkways with ornamental plantings may provide enhanced landscape within the various neighborhood communities.
- Landscaping is required where development is visible from major public roadways and public facilities including trails.
- Tree planting location and species should consider the need to preserve solar access, views, and fire safety requirements.
- All plants should be carefully selected to avoid toxic species that could be harmful to children or cause allergic reactions.
- Landscape plans for all areas where the developer is required to install landscaping should be prepared by a landscape architect registered to practice in the State of California.



Edible Landscape



Community Garden

A.3.2 COMMUNITY GARDENS AND EDIBLE LANDSCAPE

The use of edible landscaping will be important in expressing the healthy living theme of the WRSP Area. In addition to NVRG and neighborhood gardens, edible landscaping may be used almost anywhere in commercial and residential sites, trails, parks, and other open spaces so that residents can enjoy the benefits of edible plants integrated into their daily lives.

- Planting should combine non-edible and edible species such as combinations of fruit and nut trees, berry bushes, vegetables, herbs, edible flowers, and ornamental plants.
- Edible landscape designs should resemble an ornamental garden and create balance, unity, rhythm, interconnection, and pattern in the landscape while integrating a host of food-producing plants into the design.
- Fruit-bearing trees, if used, should be planted to not overhang paved areas or trails.
- Landscape design should emphasize the use of nectar-producing and flowering plants that supply food, shelter, and breeding habitat for beneficial insects that pollinate edible crops and control pests.
- Gardens for butterflies, hummingbirds, and native bees should be used.
- Climbing vines such as grape and kiwi should be used for arbors, pergolas, fence lines, or trellises.

A.4 Residential Design Guidelines

While a well-organized land use and circulation plan is important, it does not create a pleasant and memorable community. Identity deals with the perception of the community as we experience it, not in plan form or from an airplane. The intent of the residential Design Guidelines is to craft architectural forms that create visually appealing neighborhoods.

A.4.1 NEIGHBORHOOD CHARACTER DESIGN GUIDELINES

Neighborhood character is based upon a combination of circulation patterns, architecture, and landscape forms. The following guidelines should be considered during the neighborhood design process.

- Open ended cul-de-sacs should be utilized where practical.
- Long stretches of homes should not back onto streets or open space amenities such as parks and paseos. Where homes back onto open space amenities, open view fencing should be used.
- Inappropriate street termination points such as a bedroom window or an alley should be avoided.

A.4.2 ARCHITECTURAL CHARACTER

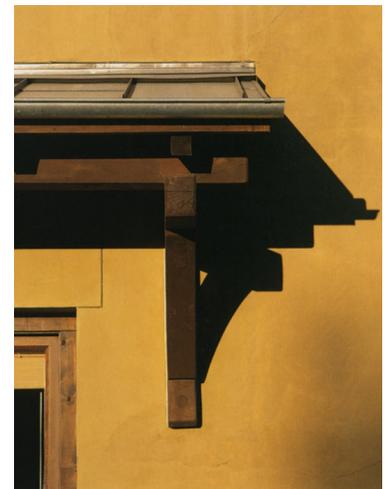
Building on the character of the NVRG, residential architecture should be form based and equally reserved



Sufficiently sized front porches to encourage use



Distinct, visible balconies



Signature detailing

in the use of ornamentation. Materials such as smooth stucco, wood, and metal roofing should be used. Detailing should be of wood and metal. A mix of rich but muted colors with bold accents can provide vibrancy to a streetscape. Form-based architecture should result in simple structures where gable and shed roofs with tower elements should be used as accents and focal points.

A.4.2.1 CREATE VARIETY ALONG THE STREETScape

Creating building relationships and massing that demonstrate variety and individuality will add to the sense of place.

- There may be a mix of two and three story homes within the community.
- Unvarying repetitive facades that present a monolithic development should be avoided.

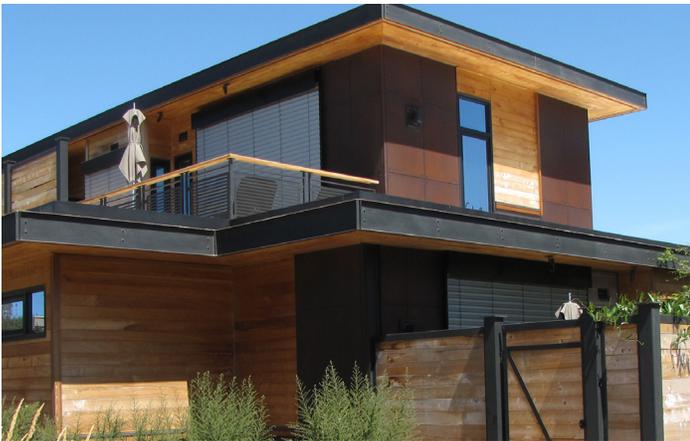
- Where practical, windows should be placed to minimize privacy conflicts.
- Visual interest in the streetscape should be created through building articulation and a variety of forms between buildings.
- There should be varied wall plane lines.
- There should be publicly visible balconies, veranda, porches, and arcades.
- Front porches should be sufficiently sized to be usable.
- There should not be repetitive, unarticulated building forms.
- Unarticulated roof forms should not be set on a constant wall plate height.



Appropriate use of accent material to create a signature architecture style



Stucco finish with metal roof



Appropriate change of materials to accent design intent



Smooth stucco finish with accent material

A.4.2.2 PUBLICLY VISIBLE FACADES

Equal concern should be given to side elevations on corner lots and rear elevations where visible to the public. There should be articulation and relief on these “public” faces.

- The “wrapping” of articulation, materials and architectural details on the sides and rears of homes should present these sides to the public.
- Unadorned flat side and rear elevations should not face onto open space areas and public streets.
- Porch elements are encouraged to wrap the corner at corner lots.
- Side elevations that face a public street should be enhanced with additional windows, pop-outs or bays, chimneys, stepped rooflines or balconies as appropriate.

A.4.2.3 MATERIALS AND DETAILING

Residential building materials should be appropriate to the community character:

- Signature detailing should establish the community’s architectural character in form, color, and materials.
- Abruptly disharmonious and monolithic architectural style, color, and material should not be used.
- Where buildings are predominantly stucco, they should have as smooth a finish as practicable. Heavy stucco textures are strongly discouraged.
- Siding materials including stucco, lap siding, board and batton, corrugated steel.
- Accent materials include brick, stone, tile, wood trim, sills or headers, metal roofing, and wrought iron.
- Roofing materials should include dimensional composite shingles and metal roofing.
- Half round gutters and round downspouts should be used.
- Changes in materials should occur at inside corners where the building plan changes direction, or where they accent the design.
- Style “appliqué” details should be avoided.

A.4.2.4 ENTRANCES

Entries should be given special attention as a whole system including the door, side windows, porch and entry wall.

- Entries should be inviting from the street.
- Adequate weather protection should be provided.
- Where homes occur on corners opportunities to wrap the porch to the side should be explored.

A.4.2.5 REDUCING THE VISUAL IMPACT OF STREET-FACING GARAGES

Where street facing garages occur on single family detached homes, lessening the impact of garage doors on the streetscape is important in the creation of pedestrian oriented neighborhoods

- Driveway pavement should be reduced to the minimum functional width.
- Living space or architectural elements should be forward of the garage doors.
- Sectional garage doors should have appropriate panel detailing and window panels, and be recessed into the building plane.

A.4.3 RESIDENTIAL LANDSCAPE GUIDELINES

Each Neighborhood should further support the community theme by providing private landscapes that connect and blend into the larger setting and character of the WRSP Area. Private landscapes include all private front yard landscapes and any residential common area landscapes such as those landscapes included as part of a Homeowner Association. Private landscapes should contain a pleasing mix of Neighborhood Identity plants as identified at the end of this section.

The predominant ground plane in each lot should be planted with groundcovers, turf or shrubs to maintain an open character and view corridors throughout the community while providing neighborhood identity. The use of turf in the front yard is strongly discouraged.

Maintenance of residential landscape beyond the establishment period shall be the responsibility of the Homeowner, to the extent that it occurs within the legal property of each lot; except for certain easement areas on lots where the landscape will be established and maintained by an HOA.

A.4.3.1 WALLS AND FENCING

Fences and walls should be built according to the design criteria and be of the materials indicated in these Design Guidelines. Retaining walls over eighteen (18) inches in height should be of masonry or poured in place concrete construction, and any retaining wall over three (3) feet shall not be installed without structural calculations developed by a structural engineer. All wall locations, heights, and finishes shall require approval of the City of American Canyon.

Fencing and walls at property lines, and between neighborhoods and adjacent to land uses are highly visible and are a major component of the visual landscape. The goal of fencing and walls in the WRSP Area is to maintain an open park setting that unites and blends private landscape with the lake, parks, open spaces and urban areas while providing a sense of privacy and security to homeowners with sound attenuation.

A.4.3.2 RESIDENTIAL LANDSCAPE GRADING AND DRAINAGE

All site surface drainage from private landscapes should be conducted to a storm water quality treatment system by the Builder.

Mounding and earth berms are discouraged as part of Private Landscape design.

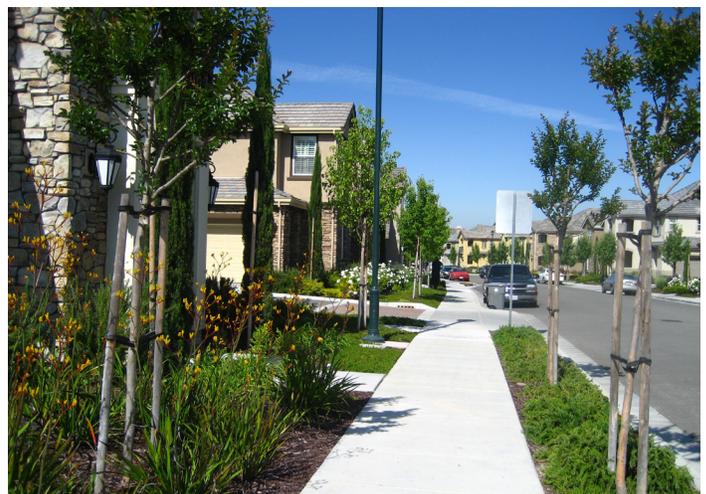


Variety of shrubbery

A.4.3.3 RESIDENTIAL LANDSCAPE IRRIGATION

The WRSP is committed to water conservation and the efficient use of water through innovative and accepted irrigation practices. Irrigation designers shall use current water use guidelines and tables, state-of-the-art irrigation equipment, and automatic controllers capable of multiple programming.

- Drip irrigation is encouraged particularly in small planting areas.
- Overhead irrigation systems may be used in larger planting areas.
- Overspray onto paving, fences or walls should be avoided and soil erosion should be minimized by the use of in-line or in-head check valves.
- Turf areas should be irrigated by an overhead spray or rotor system or by subsurface irrigation drip tubing. Overspray onto paving, fences or walls should be avoided and soil erosion should be minimized by the use of in-line or in-head check valves.
- All valves and equipment should be located adjacent to buildings where feasible and visually screened from public view.
- No irrigation equipment should be located in such a way so as to create a safety hazard to persons or property.
- Operating manual and scheduling charts should be provided to all Homeowners, Maintenance Companies, Associations, or Agencies.



Appropriate landscaping adjacent to sidewalks

- As-built plans should be prepared and provided for any Common Area Systems, such as may be used in HOA or LLD jurisdictions.

A.4.3.4 RESIDENTIAL LANDSCAPE PLANTING DESIGN

Planting layout and design should be conducted with knowledge of plant material habits and varieties as a guide for proper spacing and combinations. The ultimate size and growth speed of plant materials should be considered to achieve an interesting, diverse landscape effect.

- Residential Local Street trees should be chosen from the appropriate Neighborhood Local Residential Street Tree Lists.

A.4.3.5 PRIVATE COMMON AREA LANDSCAPES RESIDENTIAL

Private Common Area Landscapes surrounding high-density housing or alley-loaded units will be determined by the Developer/Builder and should exhibit the landscape characteristics of the Neighborhood in which it is located. Planting materials should follow the same criteria and Neighborhood Identity Plant lists.

A.4.4 RESIDENTIAL STREET LANDSCAPING

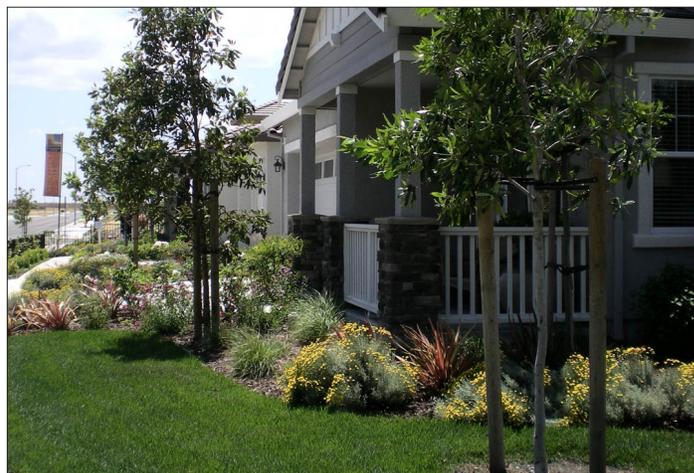
The overall landscape design concept for public streets is to create a distinct village by blending public and private landscapes and reflecting historic materials and styles found in the region. The landscape should also mirror the organizing elements of surrounding rural and agricultural landscapes and emphasize connections between

residential neighborhoods, using California adapted plant species that provide scale, color and seasonal interest.

- All 'ground plane' landscape materials including shrubs, groundcovers and turf should conform to the City of American Canyon.
- Groundcover should be used in parkways on loop parkways, collectors, enhanced locals, or residential locals per the City of American Canyon.
- Any trees planted in parkways should be irrigated separately from groundcovers and shrubs with approved tree bubblers.
- Turf should not be allowed in medians or parkways.
- Project walls and private yard fences should be landscaped with varying heights of plant materials and a variety of vines within the right-of-way to provide a pleasing appearance along the streetscape.
- In areas where adjacent parking lots are planned, a minimum planting screen with a minimum height of 30 inches and a maximum height of 36 inches should be designed through the use of shrubbery, landscape berming, low walls, or a combination of these elements.
- All streets should have continuous street tree planting located within parkways and medians.
- The street tree canopy should provide a sense of enclosure, shade and separation of pedestrian and vehicular uses.
- Street tree planting of Arterial Roads and Collector Roads should provide continuity and orientation throughout the community.



Drought tolerant landscaping



Residential landscaping

PLANT COMMUNITY

BOTANICAL NAME	COMMON NAME	PLANT COMMUNITY						EDIBLE	LOW WATER USE
		RESIDENTIAL NEIGHBORHOOD	NEIGHBORHOOD PARKS	PARKS & OPEN SPACE	OPEN SPACE BUFFERS	QUARRY LAKE PARK	NAPA VALEY RUINS & GARDENS		
TREES									
Arbutus 'Marina'	NCN								
Arbutus unedo	Strawberry Tree								
Betula pendula	European White Birch								
Camellia	Camellia								
Carpinus fastigiata	European Hornbeam								
Cedrus deodara	Deodar Cedar								
Cercis canadensis	Eastern Redbud								
Citrus sp.	Orange/Kumquat								
Cupressus sempervirens	Italian Cypress								
Eriobotrya deflexa	Bronze Loquat								
Ginkgo biloba	Maiden Hair Tree								
Lagerstroemia faurei	Crape Myrtle								
Laurus nobilis 'saratoga'	Grecian Laurel								
Liquidamber styraciflua 'festival'	American Sweetgum								
Magnolia 'St. Mary's'	Magnolia								
Malus sp.	Apple								
Maytenus boaria	Mayten Tree								
Melaleuca sp.	Paper Leaf Tree								
Melaleuca quinquenervia	NCN								
Olea europaea	Olive								
Palm trees	Palms								
Pistacia chinensis	Chinese Pistache								
Pinus eldarica	Japanese Black Pine								
Pinus pinea	Italian Stone Pine								
Platanus acerifolia 'Columbia'	London Plane Tree								
Populus nigra 'Italica'	Lombardy Poplar								
Prunus sp.	Cherry/Plum								
Prunus sargentii 'columnaris'	Sargent's Cherry								
Punica granatum	Pomegranate								
Pyrus calleryana 'Chanticleer'	Flowering Pear								
Pyrus calleryana 'Bradford'	Flowering Pear								
Quercus agrifolia	Coast Live Oak								
Quercus lobata	Valley Oak								
Quercus virginiana	Southern Live Oak								
Tilia cordata	Little Leaf Tilden								
Ulmus parvifolia 'True green'	Chinese Elm								

SHRUBS											
Abelia x grandiflora	Dwarf Abelia	Yellow									
Arbutus unedo	Strawberry Tree		Light Green						Red	Orange	Brown
Aloe spp.	Aloe								Red		Brown
Arctostaphylos spp.	Manzanita	Yellow	Light Green	Green			Purple		Red		Brown
Artemisia 'Powis Castle'	NCN								Red		Brown
Camellia spp.	Camellia	Yellow									Brown
Ceanothus spp.	Wild Lilac					Light Blue					Brown
Chondropetalum spp.	Cape Rush						Purple				Brown
Cuphea hyssopifolia juncus spp.	Rush	Yellow									
Cyanara cardunculus var. scolymus	Globe Artichoke		Light Green	Green		Light Blue	Purple		Red		Brown
Dietes spp.	Fortnight Lily	Yellow	Light Green	Green			Purple		Red		Brown
Feijoa sellowiana	Pineapple Guava								Red	Orange	Brown
Grevillea canberra 'Noellii'	NCN	Yellow	Light Green	Green			Purple		Red		Brown
Hemerocallis spp.	Evergreen Daylily	Yellow	Light Green	Green			Purple		Red	Orange	Brown
Lavandula spp.	Lavender	Yellow							Red		Brown
Leucophyllum spp.	Texas Ranger	Yellow	Light Green	Green					Red		Brown
Myrtus communis	Myrtle								Red		Brown
Nandina domestica 'Compacta'	Dwarf Heavenly Bamboo								Red		Brown
Osmanthus fragrans	Sweet Olive	Yellow	Light Green	Green					Red		Brown
Penstemon spectabilis	Showy Penstemon	Yellow							Red		Brown
Perovskia atriplicifolia	Russian Sage								Red		Brown
Phlomis fruticosa	Jerusalem Sage								Red		Brown
Phormium spp.	New Zealand Flax	Yellow	Light Green	Green			Purple		Red		Brown
Potentilla spp.	Cinquefoil	Yellow									Brown
Prunus virginiana	Chokecherry		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Punica granatum	Non-Fruiting Pomegranate								Red		Brown
Rhamnus spp.	Coffee Bush					Light Blue					Brown
Rhaphiolepis indica	Indian Hawthorn	Yellow	Light Green	Green			Purple		Red		Brown
Ribes spp.	Currant		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Rosa spp.	Rose	Yellow	Light Green	Green		Light Blue	Purple		Red		Brown
Rosmarinus spp.	Rosemary		Light Green			Light Blue	Purple		Red	Orange	Brown
Rubus fruticosus 'Triple Crown'	Thornless Blackberry		Light Green	Green		Light Blue	Purple		Red		Brown
Salvia spp.	Sage	Yellow	Light Green			Light Blue	Purple		Red	Orange	Brown
Salvia leucantha	Mexican Bush Sage						Purple		Red		Brown
Santolina spp.	Lavender Cotton	Yellow							Red		Brown
Vaccinum ovatum	Evergreen Huckleberry		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Viburnum spp.	Viburnum	Yellow							Red		Brown
Westringia fruticosa	Coast Rosemary	Yellow	Light Green	Green			Purple		Red		Brown
Woodwardia spp.	Giant Chain Fern	Yellow					Purple		Red		Brown
VINES											
Actinidia deliciosa	Kiwifruit		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Campis radicans	Trumpet Vine	Yellow							Red		Brown
Distictis buccinatoria	Blood Red Trumpet Vine	Yellow	Light Green	Green					Red		Brown
Gelsemium sempervirens	Carolina Jasmine	Yellow	Light Green	Green					Red		Brown
Jasminum polyanthum	Pink Jasmine	Yellow	Light Green	Green					Red		Brown
Parthenocissus tricuspidata	Boston Ivy	Yellow	Light Green	Green					Red		Brown
Passiflora edulis 'Maypop'	Passion fruit		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Vitis spp.	Grape		Light Green	Green		Light Blue	Purple		Red	Orange	Brown
Wisteria spp.	Wisteria	Yellow							Red		Brown

GROUNDCOVER													
Abelia Prostrate Form	NCN												
Arctostaphylos spp.	Prostrate Manzanita	Yellow	Light Green	Dark Green	Light Blue	Purple	Red						
Ceanothus spp.	Prostrate Wild Lilac		Light Green		Light Blue								
Coprosma spp.	Coprosma	Yellow											
Cotoneaster spp.	Cotoneaster		Light Green	Dark Green		Purple	Red						
Fragaria chiloensis	Wild Strawberry	Yellow	Light Green										
Fragaria x ananassa	Garden Strawberry		Light Green	Dark Green	Light Blue	Purple	Red			Orange			
Juniperus spp..	Juniper			Dark Green									
Lantana montevidensis	Lantana	Yellow	Light Green			Purple	Red						
Myoporum parvifolium	NCN		Light Green	Dark Green		Purple							
Rosa spp.	Groundcover Rose												
Rosmarinus spp.	Rosemary				Light Blue					Orange			
Salvia spp.	Sage				Light Blue					Orange			
Tuecium spp.	Germander						Red						
Thymus spp.	Thyme		Light Green	Dark Green			Red			Orange			
Trachelospermum asiaticum	Jasmine	Yellow											
Verbena spp.	NCN	Yellow	Light Green				Red						
GRASSES													
Acorus spp.	Forest Grass	Yellow											
Carex spp.	Sedge	Yellow	Light Green	Dark Green			Red						
Eleocharis spp.	Spike Rush	Yellow	Light Green	Dark Green									
Festuca spp.	Fescue	Yellow	Light Green	Dark Green	Light Blue								
Juncus spp.	Rush	Yellow	Light Green	Dark Green									
Leymus spp.	Wild Rye	Yellow	Light Green	Dark Green		Purple	Red						
Miscanthus spp.	Eulalia Grass	Yellow	Light Green	Dark Green	Light Blue	Purple	Red						
Muhlenbergia capillaris	Hairy Awn			Dark Green	Light Blue	Purple	Red						
Pennisetum spp.	Fountain Grass	Yellow	Light Green	Dark Green	Light Blue	Purple	Red						
TURF													
Fescue Turf	Fescue	Yellow	Light Green	Dark Green	Light Blue	Purple	Red						
Festuca rubra	No-Mow Fescue	Yellow	Light Green	Dark Green	Light Blue	Purple	Red						

- Specific tree species and tree spacing should conform to the specific guidelines described in this section.
- Street tree planting on enhanced local roads and local residential roads should be designed to encourage pedestrian use, shorten the perception of walking distances and provide shade and seasonal interest.
- All street trees located within five feet (5') of any paving or utilities should be provided with root barriers as approved by the City of American Canyon.
- All street trees should be planted from minimum 15 gallon box containers.
- The ground plane in the parkways on Collector Streets should have irrigation systems separating planter zones from the tree zones
- The planting area between the back of sidewalk and the right-of-way on Collector Streets should be planted with a combination of shrubs and groundcovers according to the adjacent land use and the presence of walls.
- All street trees planted in the right-of-way or adjacent easement on Local Streets should be from 15 gallon box containers and planted a minimum of 30' on center in single straight rows in the parkway.
- Community entry nodes should be enhanced with accent trees and shrubs to further define and enhance vehicular and pedestrian circulation.
- Parkway along Residential Local Streets should be groundcover as approved by the City of American Canyon with street trees from the approved list at the end of this section.
- Each street tree in the parkways should have irrigation systems separating planting area zones from the tree zones.
- Street trees on Residential Local Streets should be a minimum 15 gallon box container size, have a caliper of not less than 1-1/2 inches and have a minimum crown size of three feet in diameter per the City of American Canyon.
- All proposed street trees and designs should be based on most currently available guidelines and approved lists from the City of American Canyon.
- Lists of Residential Local Street trees for use in each of the neighborhoods are provided at the end of this section.

A.4.5 RESIDENTIAL LIGHTING GUIDELINES

The goal for the site lighting design is to provide a comfortable level of illumination that meets the community's needs for orientation and safety in a way that complements the aesthetic qualities of the architecture and surrounding environment. A family of light fixtures is selected for the development that recognize the different qualities of the land uses while creating a consistent design theme throughout the community. The quality of the light fixtures, the lighting intensity, and source controls provide criteria that will establish a high quality experience to this important aspect of design.

- High efficiency fixtures and sophisticated optics should be used to direct light where it is needed without creating excessive glare.
- Long lasting LED lamps should be used to minimize energy use and lamp replacement.
- Lights should be placed where they are needed for specific uses, rather than a continuous foot-candle requirement across the site, allowing for the appreciation of the dark sky in the residential neighborhoods.
- To preserve the quality of a dark sky at night, high intensity light fixtures should include a shielded light source that reduces the view to the light source.
- Pole lights are proposed for the lighting of the streets in the neighborhoods. The fixtures should have a shielded light source, with optics that direct the light down to the ground so that the light source is not visible outside of the light distribution area.
- Street identification signs and traffic control signs should be mounted on light poles to integrate these elements into the design and minimize the number of poles at intersections.
- Pole lights used for residential streets should be placed in parks to preserve the quality of the overall community.
- Low intensity lighting in some of the park spaces should be used to provide orientation and a sense of security.
- Pole lighting should be used along the multi-purpose path throughout the community to encourage use of this amenity into the evening.
- Lighting should provide an even distribution of light that minimizes glare.

- No direct view to light sources should be visible from off site.
- Where lighting is provided, fixtures should utilize house side shields and cut-off optics to reduce light spill over the property line.
- Pedestrian pole light fixture locations should not conflict with the pattern of tree planting along the roads and parking lots.
- Pole-mounted lights should be used sparingly in the Civic Park Plaza and the use of illumination encouraged for pedestrian circulation and less ambient light at night.
- Illuminated bollards should be used primarily along pedestrian pathways to encourage evening strolls.
- Accent lighting should also be used sparingly and primarily for the use of highlighting nodes or destination spaces.

A.4.6 RESIDENTIAL MAILBOX DESIGN GUIDELINES

- Mailbox materials should be consistent with architectural theme.
- Multibox stations should be designed to replicate architectural details and colors.

A.5 Napa Valley Ruins & Gardens Design Guidelines

The mixed-use NVRG is a unique area made up of several different uses including retail, residential, office, commercial, visitor service including hotel and events center, entertainment, recreation, civic, and community facilities. This blending of uses creates an urban fabric with distinct, yet connected areas.

As the “heart” of the WRSP Area, the NVRG creates the hub of activity for the overall community. The quality of design is important to the perception of the Watson Ranch community. Architecture and landscaping should create a village atmosphere where people are comfortable to mix and mingle and desire to return to over and over again.

A.5.1 COMMUNITY CHARACTER DESIGN GUIDELINES

The NVRG will create a strong focal point for the new community. While having the flexibility to be the setting for community gatherings and special events, the area must also provide for comfortable, more intimate resting spaces. This is a place where all residents and visitors should feel comfortable whether they are a participant in an activity such as an art fair or street performance, or quietly sipping coffee, spending time with family, shopping, people watching or playing checkers.

A.5.1.1 THE PEDESTRIAN ENVIRONMENT

NVRG should create an environment where people are comfortable walking and spending time, and should incorporate the following Design Guidelines:



Linear outdoor seating, along sidewalks



Enclosed outdoor seating, in a courtyard

- Provide for a mix of sizes of businesses that creates a pedestrian friendly atmosphere.
- Create strong pedestrian links to the rest of the WRSP Area.
- Provide unique and pedestrian scaled lighting fixtures.
- Trellis, awnings and other weather protection devices are encouraged.
- Significant intersections should be highlighted with bollards, street furniture, special paving, accent trees and opportunities for monumentation or public art.

Outdoor seating areas should be provided to create gathering places for residents and visitors in order to enhance a village atmosphere.

- Focal points should be created with features such as signature landscaping, fountains, outdoor performance areas, or opportunities for public art.
- A generous amount of outdoor seating should be provided and should include both sunny and sheltered areas.
- Both hard surface and planted areas should be incorporated into the design.
- Existing concrete plinths should be investigated for inclusion in the design as seating elements.

The pedestrian linkages are an important feature of the mixed-use environment. These areas should be furnished with enriched materials and furnishings that create a comfortable, convenient, and entertaining experience. With the exception of features created as public art, the



Buildings relate to the street

furnishings should be in a similar family of style, color, and finish to create a refined and uncluttered appearance.

- Seating areas should be created using benches, chairs, seat walls, and steps and may include informal seating on raised planters, steps, and benches cast into the plaza areas.
- Newspaper racks should be of one cohesive design and integrated into the landscape design.
- Bicycle parking should be integrated into the landscape design to place these elements in a discrete manner, or be of unique design as public art.
- Bollards may be placed to provide separation from autos at pedestrian crossings.
- Trash and ash receptacles should be placed at convenient locations.

Various decorative paving material should be incorporated into the design.

- Locations include pedestrian crosswalks, prominent pedestrian intersections, building entries, gateways, social activity areas and other focal points.
- Paving materials should complement adjacent architecture.

A.5.2 NVRG ARCHITECTURAL DESIGN GUIDELINES

The architectural character of the NVRG is intended to create a unique and memorable place within the City of American Canyon and the region. The aesthetic draws from the visual strength and simplicity of the concrete



Distinct but related forms and material use to create a sense of place

ruins as well as from the vineyard heritage of Napa County. Even though the ruins are over 100 years old, their simple shapes form a very contemporary backdrop for the community.

A.5.2.1 BUILDING ENTRANCES

- Buildings should be oriented to face streets or other public spaces.
- Entrances should be well marked, articulated, and oriented to streets and open space.
- Entrances should provide opportunities to create unique addresses along the street through use of elements such as distinctive form, detail, materials, color, ornament, lighting and signage.
- Street addresses should be clearly displayed and must comply with applicable sign regulations (see Section 6.5 for signage regulations).
- Entrances to uses above the ground floor should be identifiable as building entrances.
- The design of a secondary side or rear entry should be architecturally related to the front entry and enhanced with detailing, trim, and finish consistent with the character of the building.

A.5.2.2 MASSING AND FORM

- Roofs should match the building in terms of style, detailing and materials and should contribute expressive and interesting forms that add to the overall character of its environment.



Use of trellis / awning to weather protection and to lend a pedestrian scale to street level

- The location, spacing, materials, and colors of downspouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the façade and roof. Downspouts should be concealed within walls or located to harmonize with window spacing and façade composition.
- Any mechanical penthouses and stair towers should appear as integrated building forms and shall be structures that complement the design of the building through the use of similar materials, colors, finishes and architectural details.
- Rooftop equipment should be located away from the street edge and/or screened so that it is not visible from streets or other public spaces. Screening should complement the design of the building through the use of similar materials, colors, finishes and architectural details and appears to be an integral part of the building's form.
- Attached equipment such as solar collector panels, antennas, satellite dishes, etc. should be integrated into the project architecture or screened from view.

The main area of the ruins creates a series of cloisters. New buildings within the cloisters of the ruins should have a contemporary aesthetic.

- Any building within the southern cloister should have a prominent roof shape that pays homage to the roof structure on the round building. This bold form would create a focal point within the ruins complex.



Long walls punctuated with 3 dimensional details

- Architecture within the NVRG should be form based, and not rely on ornamentation.
- Building should have lines clean and shapes that define interior space.
- Commercial structures should have strong simple forms that are made up of fairly monolithic materials such as concrete and stucco.
- Wood and metal detailing should be used to soften these simple forms.

The residential uses within the NVRG should respect the same aesthetic. Town homes or condominiums should create an urban streetscape. They should address the street as row homes with front porches or stoops.

A.5.2.3 BUILDING FACADES FACING PUBLIC SPACES

Public visible facades of all buildings should have the same level of articulation and quality of details and materials as the front of the building.

- Where long expanses of blank wall are unavoidable, they should be articulated and softened with 3-dimensional details, planters, vines and other landscaping.
- A composition of distinct but related forms should be used.
- Loading and service areas for commercial uses should be integrated into the overall building composition.



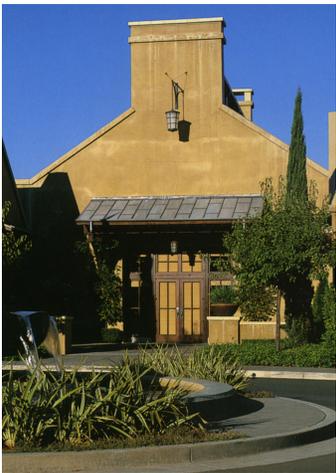
Use of high quality material in public spaces



Simple, yet prominent roof forms as an architectural element.



Architecture and material of existing ruins set a precedent



Strong simple concrete / stucco forms with wood and metal details

- Architectural enclosures should be designed as integral elements of the building architecture
- Long unbroken forms and flat planes are discouraged.

Buildings facades facing public spaces should incorporate 3-dimensional facade elements that lend a pedestrian scale to the street level.

- Trellises or permanent awnings should be incorporated where appropriate for shade and weather protection.
- Arcades, wide overhangs, deep reveals, permanent awnings, etc. should be used.

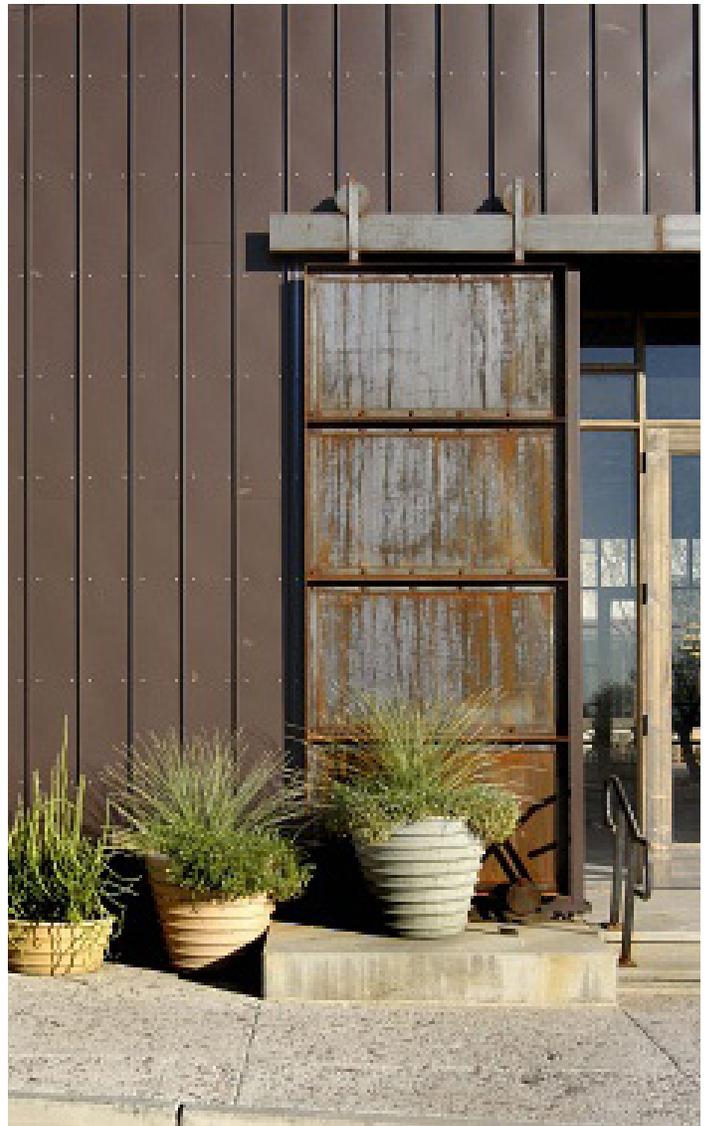
Roof design should be integral to the overall building design.

- Parapets should screen rooftop mechanical equipment from ground level view.
- Tower elements should be included at key locations to provide points of interest along the streetscape.
- Roofline variation should be created by differing plate heights.

A.5.2.4 BUILDING MATERIALS

Building materials should be appropriate to the community character and suited to commercial construction.

- Materials such as stucco, masonry, storefront glazing and well-detailed precast concrete should be used.
- Accent materials such as brick, stone, tile, and anodized or painted metals should be used.



Materials reflect the industrial history of the site



Facades facing public spaces with articulation



Color and material relate to the context

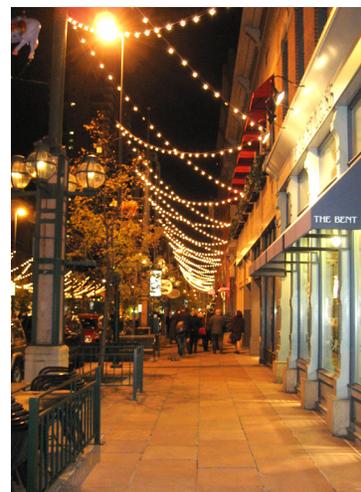
- Any changes in materials should occur at inside corners where the building plane changes direction.
- Mirror glazing should not be used.
- Where used, stucco should have with as smooth a finish as practicable. Heavy stucco textures are discouraged.
- Accent materials include stone; tile; wood trim, sills or headers; metal roofing and wrought iron.
- Roofing materials include dimensional composite shingles and metal roofing.
- Half round gutters and round downspouts should be used.
- Changes in materials should occur at inside corners where the building plan changes direction.
- Color and material changes should occur at inside corners.
- Mirror and reflective glass is discouraged.
- Vinyl and aluminum siding is discouraged.
- Brick, stone or other masonry should not “float” above the ground plane, but must be detailed such that it can continue either to the ground or to a concrete shelf or edge in the foundation so that its “weight” is visually supported. Simulated finishes must be a level of quality to appear authentic.

A.5.3 NVRG LANDSCAPE DESIGN GUIDELINES

Landscaping of the NVRG will draw heavily from classic images utilizing olive trees, Italian Cypress, lavenders, roses and a variety of ornamental grasses. The center cloister should include unique paving materials specially selected to enhance the outdoor experience, along with providing for formal functions. Shade tolerant plant materials should be encouraged for landscape planters and interior low light situations. The central portion of the NVRG should incorporate both soft surface materials and unique organic paving, a formal fountain, and formal planting areas to allow for large gatherings and functions. Around the perimeter of the ruins the planting concept should be reminiscent of an Italian hill town, with a variety of soft textures and flowing landscape. Olive trees, ornamental grapes coexist with the stark lines and dramatic shapes of the ruins, softening edges and defining spaces.

A.5.3.1 NVRG PLANTING DESIGN GUIDELINES

- Olive trees, turf areas, and ornamental grasses should dominate the landscape adjacent to the ruins structures.
- Turf areas should foster outdoor spaces and gathering areas.
- Ornamental grasses should be used primarily to convey informality and rustic charm.



Ornamental street perimeter that compliment the architectural style of the buildings

- Olive trees should be planted in rows symbolic of orchards in Italy and the Napa Valley.
- Lavenders, roses, and evergreen groundcovers should be primarily used to enhance entries, highlight sitting areas, and line walkways.

A.5.3.2 NVRG STREET FURNITURE/ SEATING DESIGN GUIDELINES

Street furniture will add to the unique architectural environment of the NVRG.

- Street furniture should be designed to foster comfortable, accessible, interactive public areas.
- Street furniture should be constructed of durable non-weathering materials utilizing recycled or eco-friendly materials where practical.
- Seating should be provided with a variety of furnishings such as benches, chairs, and low walls with landscape features such as salvaged plinths.
- Other street furniture should include waste and recycling receptacles, decorative planters and pottery, bicycle racks, flag poles, and drinking fountains.

A.5.3.3 NVRG PARKING LOT LANDSCAPING DESIGN GUIDELINES

- Landscaping for parking areas should include large dome-shaped canopy trees to provide shade in hot summer months while allowing winter sun exposure.
- Parking lot trees should have root barriers.



Lighting integrated to the landscape to highlight gathering spaces

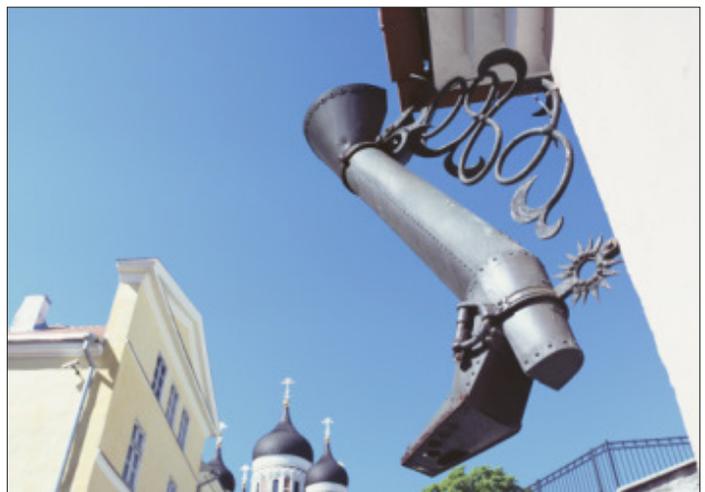
- Understory planting should be primarily with low-growing (+24”), drought tolerant, evergreen groundcover or grasses.

A.5.4 NVRG LIGHTING DESIGN GUIDELINES

These lighting design guidelines provide criteria that will provide a comfortable level of illumination that meets the community’s needs for orientation and safety in a way that complements the aesthetic qualities of the architecture and surrounding environment. A family of light fixtures is selected for the development that recognize the different qualities of the land uses while creating a consistent design theme throughout the community. The quality of the light fixtures, the lighting intensity and source controls provide criteria that will establish a high quality experience to this important aspect of design.

A.5.4.1 LIGHT FIXTURE DESIGN GUIDELINES

- High efficiency fixtures and sophisticated optics are encouraged to direct light where it is needed without creating excessive glare.
- Long lasting high pressure sodium lamps are suggested to minimize energy use and lamp replacement.
- Lights should be placed where they are needed for specific uses, rather than a continuous foot-candle requirement across the site, allowing for the appreciation of the dark sky in the residential neighborhoods.
- To preserve the quality of a dark sky at night, high intensity light fixtures should include a shielded light source that reduces the view to the light source.



Unique signage

- Plazas and walkways should utilize a combination of decorative pedestrian scale pole and illuminated pedestal or bollard lights selected to complement the architectural style of the buildings.
- Wall mounted fixtures should be used where appropriate on the building elevations to supplement the pole lights and to complement the building architecture and should be scaled appropriately.
- Arms for banners and flower baskets should be provided in the main street and plaza areas to provide seasonal decorative opportunities and add a festive quality to the environment.
- Special lighting features to further animate the spaces encourage nighttime use. Ambient lighting such as pole mounted low intensity “string of pearl” lights as well as uplights and lanterns in trees are concepts that would be appropriate to create festive lighting.
- Wall mounted niche lights may also be used to provide indirect light sources to further articulate walls, steps, and signs within the plaza.

5.4.2 PARKING LOT LIGHTING DESIGN GUIDELINES

- Parking lots should be lit with a high efficiency pole fixture to provide an even distribution of light while minimizing glare.
- Lamps combined with sophisticated optics and sharp cut off features should be selected to direct all of the light to the ground.

5.4.3 RUINS LIGHTING DESIGN GUIDELINES

- Lighting for the ruins surrounding gardens should include a unique monument pilaster light.

5.4.4 LANDSCAPE LIGHTING DESIGN GUIDELINES

Landscape lighting is designed to contribute to the extended daily use of mixed-use districts. Lighting helps to create welcoming visible spaces and accentuates design features such as public art, concrete columns and pedestrian travel routes.

- Pole mounted lights should be used for socializing and active use areas.
- Illuminated bollards should be used for circulation and travel routes.
- Ambient lights should be used for entries, steps, and tree up-lights.
- Landscape lighting for the NVRG should be designed primarily for accent purposes.
- Small accent lighting may be used for the up-light of trees, ambient light for specimen planting, building edges, steps, and entries.

A.5.5 SIGNAGE DESIGN GUIDELINES

These guidelines are intended to result in functional, attractive signage incorporating a high level of design, graphics, and efficient maintenance throughout the WRSP Area. These signage design guidelines apply to retail, office and, other commercial uses.



Signs that are unique and reflective of the industrial history of the site

All signage within the WRSP Area shall conform to the specific requirements, spirit, and intent of the City of American Canyon code provisions regulating signage, unless otherwise stated. If there are any inconsistencies between the City of American Canyon regulations and the WRSP, the guidelines in the WRSP will take precedence.

5.5.1 GENERAL SIGN DESIGN GUIDELINES

The building architecture should be designed to accommodate signage and other graphics as an integral part of the building design.

- Illuminated signs should be fixed and directed to control and avoid light pollution
- Individual letter characters are encouraged.
- Metal signs may be made of aluminum, brass, bronze, copper, or stainless or welded steel.
- Raised “Halo” letters on building face; pedestrian oriented blade signs; sculptured cantilevered signs; non-internally lit signs with lighting from a secondary source; and artistic neon signs backed by building face or storefront are encouraged.
- Signage is intended to be unique and original for each establishment and executed with a high degree of craftsmanship.



Community art

A.6 Community Art and Monumentation

A.6.1 COMMUNITY ARTS PROGRAM

The WRSP strongly supports the inclusion of community art as an enrichment of the public community space. Art blended with urban design in the NVRG can add value to the experience of the place. Community art should not only be an individually commissioned pieces, but an attitude towards the elements that make up the public realm as experienced within a community. Given the unique identity established by the NVRG, community art can have a complementary role in energizing and defining the spaces. The NVRG may include items such as:

- A thoughtful and carefully selected lighting program that uses fixtures to establish a character.
- Adding banners and hanging planters or other decorative items to lighting fixtures.



Art in community gathering spaces



Infrastructure as art

- Special paving to highlight areas within the community of special interest.
- Low walls and seating areas that are designed to encourage pedestrians to linger and fully experience a space.
- Landscape architecture elements such as custom benches, trash receptacles or bike racks.
- Rethinking standard infrastructure items such as man hole covers and tree grates, that typically are “off the shelf” items, but in focal point places, can be created as a custom art piece.
- Incorporating focal points for commissioned permanent or rotating art.
- Preservation of particularly creative graffiti art on the ruins themselves, or wall space dedicated to shows of street art.
- Providing small venue spaces for performance art.
- Building facade elements such as mosaics, frescos and custom iron work.
- A custom wayfinding/community signage program.
- Interactive fountain or water elements.

Key to the quality of the community environment is eliminating visual clutter by creating a cohesive program of street furnishings such as trash receptacles, and newspaper vending fixtures. These features should be provided on a community wide basis and are regulated as part of the WRSP.



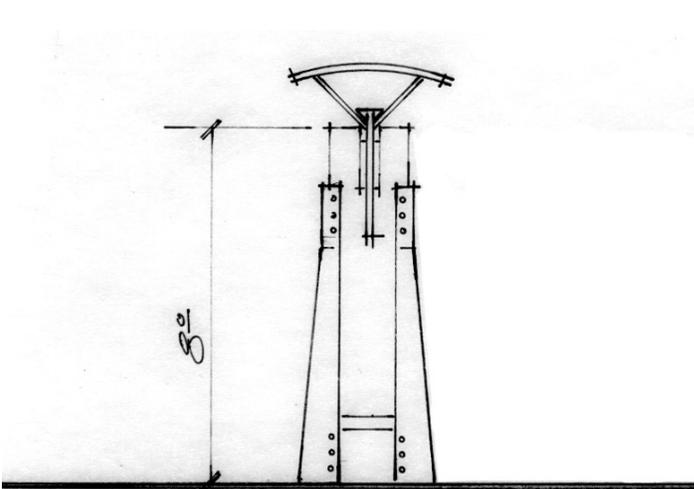
Community monumentation as art



Creating a sense of place



Community Identification



Freestanding tower

A.6.2 COMMUNITY MONUMENTATION

A cohesive program of community identification; form the community threads which tie all aspects of the community together within the context of varied architecture.

Community monumentation is a strong visual element that defines and enhances a community's special sense of place. These elements define a district within the greater context of a town. The community's personality is displayed by these features. The icons of classic communities are traditionally monuments that signalize your arrival and an entry or focal point.

- The palate of identification for the WRSP Area is based upon the free standing monuments that exist as part of the ruins.
- The monumentation may include pilasters, low walls, or strategically placed pieces of community art.
- They may also be a focal point created as an element of a building such as a tower, or a publicly visible landscape or retaining wall.

The design of the community identification is unique and takes its cues from the ruins. Materials have a permanence to them, complement the architecture of the community or create a counterpoint, and may include materials such as steel or other metals, concrete, tile, brick, stone and glass. A typical pilaster monument is shown here.

Monumentation locations are identified on the following plan:

A.7 Walls and Fencing Design Guidelines

A.7.1 GENERAL DESIGN GUIDELINES FOR WALLS

- Walls and fences are to be minimized to the greatest extent possible and their design should be complementary to the building architecture.
- Site walls should be made of materials complementary to the building architecture palette.
- Walls, which are located in visually prominent locations, should be decorative and faced with a material such as stucco or stone.
- Concrete retaining walls should only be used where necessary for structural and spacial requirements.

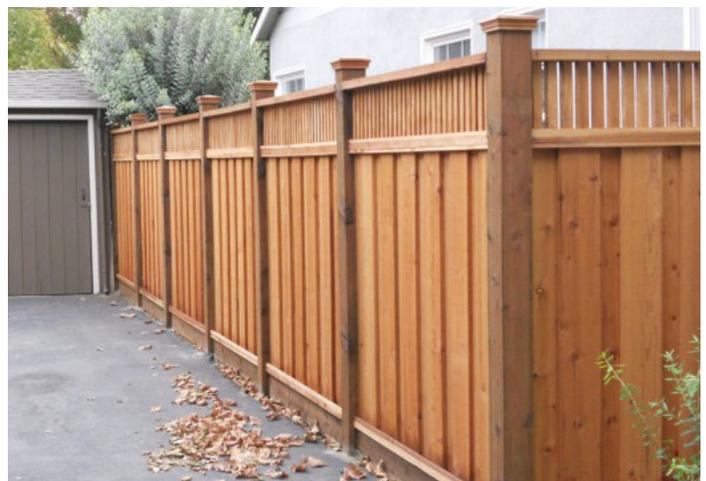
- Plantings should be utilized to minimize the visual impact of all retaining walls.
- Walls, fencing materials and colors should complement adjacent architecture. Typical fence designs are shown here.

A.7.2 GENERAL DESIGN GUIDELINES FOR FENCES, SOUNDWALLS AND VIEW FENCES

- Fences throughout residential landscapes should be constructed of wood, metal, masonry, or a combination of these products to maintain the community theme and provide continuity.
- There are four (4) distinct levels of fencing and walls, such as standard privacy fencing, enhanced privacy fencing, enhanced soundwalls, and view fences.
- Standard six-foot (6') high privacy fencing of a 'good neighbor' type should be used on all fences located between private lots which are not visible to the public view.



Combination of Fencing Materials



Traditional residential wood fence

- Fencing should be constructed of weather-resistant wood products and should have a continuous wood cap covering the ends of all posts and fence boards.
- All wood fencing should be sealed and stained with clear finish products.

A.7.3 RESIDENTIAL NEIGHBORHOOD FENCING DESIGN GUIDELINES

- Enhanced 6' high privacy fencing should be used on any fence visible to the public view and should be created using weather-resistant wood products, an enhanced horizontal 'decorative' top, a horizontal 'plain' bottom, and decorative post caps.
- All wood fencing should be sealed and stained with clear finish products.
- View fences should be located along any property line between public and private property which abut the public open spaces such as the parks and areas where landscape views are to be maintained.
- View fences should be 6' in overall height from finish grade and should be constructed primarily using wire mesh and decorative tubular steel.
- Where no low wall or fence defines the front yard, side/rear fences between homes should be set back at least eight feet from the fronts of garage.

serve intruders as well as regular users, and thus provides the opportunity to challenge inappropriate behavior or report it to the police or the property owner. When natural surveillance is used to its greatest advantage, it maximizes the potential to deter crime by making the offender's behavior more easily noticeable to a passing individual, police patrol, or private security detail.

It also involves providing a good visual connection between residential and/or commercial units and public environments such as streets, common areas, parks, sidewalks, parking areas and alleys.

2. Natural Access Control

Natural access control employs elements like doors, shrubs, fences, and gates to deny admission to a crime target and to create a perception among offenders that there is a risk in selecting the target. The primary thrust of an access control strategy is to deny access to a crime target and to create a perception of risk to offenders. Physical and mechanical means of access control—locks, bars, and alarms can supplement natural access control measures if needed. A fence around a neighborhood playground is an example of an access control measure that protects children from wandering off and inhibits entry of potential offenders.

3. Territorial Reinforcement

Territorial reinforcement employs such design elements as sidewalks, landscaping, and porches to help distinguish between public and private areas and helps users exhibit signs of "ownership" that send "hands off" messages to would-be offenders. The concept of territorial reinforcement suggests that physical design can create or extend a sphere of territorial influence and potential offenders perceive that territorial influence. For example: low walls, landscape and paving patterns to clearly define the space around a unit entry as belonging to (and the responsibility of) the residents of the unit.

4. Maintenance

Lastly, care and maintenance allows for the continued use of a space for its intended purpose. Deterioration and blight indicate less concern and control by the intended users of a site and indicate a greater tolerance of disorder. Proper maintenance protects the public health, safety and welfare in all existing structures, residential and nonresidential, and on all existing premises by establishing minimum requirements.

A.8 Crime Prevention Through Environmental Design (CPTED) - Design Guidelines

CPTED design guidelines should be used during all processes of the design, from initial concept planning to execution and also at all levels, from community wide planning to individual homes.

The four elements of CPTED include:

1. Natural Surveillance

Natural Surveillance is a design concept directed primarily at keeping intruders under observation. It utilizes design features to increase the visibility of a property or building. The proper placement and design of windows, lighting, and landscaping increases the ability of those who care to ob-

APPENDIX B GENERAL PLAN CONSISTENCY

B.0 General Plan Policy

A “town center” should be established in proximity to the closed basalt plant which functions as the symbolic center of American Canyon. A wide diversity of uses characterized by a high level of activity should be accommodated, including government, retail, office, service, entertainment, housing, and open space. The center should be developed as a pedestrian-oriented village that is physically linked by pedestrian and bicycle trails and other elements to surrounding neighborhoods and districts.

Watson Ranch Specific Plan (WRSP) calls for housing, parks and open space, and a non-residential component – the Town Center Mixed Use area (TC-1 MU) also referred to as the Napa Valley Ruins & Gardens (NVRG), which allows retail, entertainment, hotels, events center for weddings, receptions, farmers’ market, restaurants, winery, wine tasting and related uses. The WRSP also provides a site which would allow public and or private community facilities such as church, day care, non-profit community-oriented service or government offices.

The WRSP will function as the symbolic center of the city due to the following aspects:

- Retention of iconic structures such as the rotunda building.
- High level of activity programmed for the TC-1 MU area, including both public and private functions.
- WRSP Circulation Plan includes multi-use trails which link TC-1 MU subarea to the remainder of the WRSP, existing residential neighborhoods, and other off-site destinations.

B.1 Land Use Element

1.6.7 Work with local agricultural producers to consider the possible establishment of farmers markets, festivals, site tours, and other events that emphasize the agricultural and rural character of the region. (I 1.23)

1.11.8 Require that any lands proposed for dedication to the City (parks, schools, etc.) be usable and appropriate for the intended use and a source of funds to maintain the area be confirmed to ensure that inappropriate costs are not shifted to the City. (I 1.1, I 1.2, I 1.4, I 1.5, and I 1.8)

Events and festivals open to the public such as a farmers’ market and community gardens are allowable uses for the TC-1 MU area.

Chapter 9 (Implementation) proposes the establishment of a Community Facilities District (CFD) and/or a Lighting Landscaping Assessment District (LLAD) to pay for the cost of public park maintenance, street lighting and other similar improvements. There will be Homeowners Association (HOA) for the residential portion of the WRSP. The HOA will maintain any private recreational facilities and common areas. See Implementation Chapter for financing information.

1.12.1 Accommodate the development of parks, schools, libraries, community meeting facilities, religious facilities, and similar community-serving uses in all residential areas, provided that they are compatible with the intended residential function and subject to City review and approval. (I 1.1, I 1.2, I 1.4, I 1.5, and I 1.8)

1.15.4 Require that mixed use structures and sites be designed to mitigate potential conflicts between the commercial and residential uses, considering such issues as noise, lighting, security, and truck and automobile access. (I 1.1, I 1.4, I 1.5, 1.11, and 1.14)

1.15.5 Require that mixed-use developments be designed to provide adequate transitions with adjacent land uses, which may include horizontal and vertical setbacks, landscape, screening elements, and similar techniques. (I 1.1, I 1.4, I 1.5, 1.11, and 1.14)

1.18.4 Require that entertainment, drinking establishments, and other uses characterized by high levels of activity provide adequate physical, safety, and operational measures to prevent negative impacts on adjacent properties. (I 1.1, I 1.4, I 1.5, I 1.11, and I 1.12)

1.19 Provide for the development of a Town Center that physically and functionally serves as the symbolic and identifiable focus of community activities and events for the City of American Canyon and which is a regional destination within Napa Valley. The Town Center shall have two principal land use areas: (s) Town Center Core Area, and (b) Town Center Residential Neighborhoods. The Town Center Core Area shall mean the area around the basalt industrial ruins which will be developed with the land uses described in Policy 1.19.2 below to create a the residential areas portion of Town Center that surrounds the Town Center Core Area, which shall be developed with land uses described in 1.19.4 below

The WRSP designates land for public parks. Other community facilities which may be developed as day care or other community facilities are allowable uses in the NVRG.

While the TC-1 MU Land Use Category provides a list of allowable uses and development standards, the precise nature and intensity of such uses cannot be determined at the time of consideration of the WRSP.

Potential conflicts between residential and commercial uses will be addressed in the context of subsequent approval when there is a specific development proposal.

The WRSP has two sub-areas: (1) Watson Ranch Residential and (2) NVRG. The Watson Ranch Residential provides residential neighborhoods surrounding the nonresidential uses in the NVRG subarea. The allowable uses and the proposed public improvements within the NVRG:

- Provide a focal point for community activities.
- Functions as a regional destination with such allowed uses as a hotel, winery, and specialty retail and events center.

1.19.1 Ensure that the Town Center shall have two principal land use areas: (a) Town Center Core Area, and (b) Town Center Residential Neighborhood.

1.19.2 Require that the Town Center Core Area will be the “downtown” for the City of American Canyon, centered around the basalt industrial ruins and quarry lake, including:

a. A rich diversity of land uses which may include

government and community services, retail commercial, professional offices, entertainment, restaurants, cultural facilities (museums, libraries, etc.), visitor-serving facilities (hotels, information centers), event center/conference center, wineries, transit, parking, variety of housing types including single family attached and detached, townhouses, condominiums, mixed-use and apartments, and public park and other amenities;

b. A plaza to facilitate community gatherings and events.

1.19.3 Require that the Town Center Core Area is surrounded by the Town Center Residential Neighborhoods in order to satisfy the City’s need for housing and to support the economic vitality of the commercial uses within the Town Center Core Area.

1.19.4 Provide for a broad range of housing types within the Town Center Residential Neighborhoods, including single family detached, attached single family, townhouses, condominiums, and apartments

The WRSP consists of two principal land use areas. The Town Center Mixed Use area NVRG(MU) corresponds to the Town Center Core Area. The remainder of the WRSP consists of residential uses, corresponding to the Town Center Residential Neighborhoods.

The TC-1 MU area provides for a diverse range of land uses. The land uses allowed include: specialty retail, restaurants, hotel, event center, wine tasting, winery, brewery, distillery, parking, open space and public park. This subarea land uses also include a variety of live/work residential.

The TC-1 TC area includes the provision of a 1 to 2 acre public plaza with an adjoining site for a community center.

The WRSP residential areas surround the TC-1 MU area (corresponding to the Town Center Core Area). Higher density uses are located more proximal to the TC-1 MU area.. The densities then transition to lower densities moving away from the TC-1 MU to the north. The WRSP provides for a broad range of housing types including single family detached, attached single family, townhouses, condominiums, and apartments.

1.19.5 Require that the Town Center Residential Neighborhoods provide sites for public parks, with the size of facilities corresponding to the future population of the Town Center and which may allow community facilities that support residences. (See WRSP Executive Summary, General Plan Amendment),

1.19.6 Provide for the extension of Newell Drive, which will define the eastern boundary of the Town Center, connecting with Highway 29 in the vicinity of Green Island Road and for the extension of South Napa Junction Road from Highway 29 to Newell Drive.

1.19.7 Determine the location of land uses within the Town Center through the subsequent approval of one or more Specific Plans.

1.19.8 Determine the range of residential densities allowed through the subsequent approval of one or more Specific Plans, with the guiding principle that, in general, densities will be highest around the Town Center Core Area with decreasing density further away

1.19.9 Determine the range of intensities of non-residential uses, as measured in building height and/or floor area ratio, through the subsequent approval of one or more Specific Plans, with the guiding principle that in general, intensity will be highest around the Town Center Core Area with decreasing intensity further away.

1.19.10 Provide for unified design standards and a cohesive development through the adoption of one or more Specific Plans for the Town Center

The WRSP provides for approximately 33 acres of public parks . The acreage of the public parks exceeds the City’s park land dedication policy of 5 acres of public park land per 1000 residents.

The WRSP provides an alignment for Newell Drive along the eastern boundary of the property. The alignment is consistent with the Circulation Element depiction. This alignment is within the boundary of the WRSP and within the existing city limits.

The WRSP determines the location of land uses within the entire area designated by the City’s General Plan as “Town Center”.

The WRSP provides for three residential density categories: medium density (MDR-12 land use designation – between 2 and 12 dwelling units per gross developable area), medium density (MDR-16 – between 8 and 16 dwelling units per gross developable acre), and high density (HDR – minimum of 20 dwelling units per gross developable acre).

The Land Use Plan shows that MDR-16 and HDR areas are immediately around or within the NVRG. The medium density (MDR-12) residential is generally further from the NVRG Area.

Chapter 4 contains a description of the range of non-residential uses within the WRSP. Appendix A contains design guidelines for WRSP.

1.19.11 Ensure that the Town Center is a sustainable, “green” development through the implementation of low impact development practices, highlighted in Appendix A. Such features could include:

- a. use of recycled water for landscape irrigation
- b. use of drought tolerant vegetation
- c. energy efficient buildings
- d. pedestrian and bicycle circulation system
- e. mix of land uses which reduce travel

1.19.12 Require that the Town Center Core Area provide a pedestrian-oriented, “village environment, including a plaza as a gathering place for community activities.

1.19.13 Require the implementation of public streetscape improvements that uniquely identify the Town Center, including elements such as landscape, street furniture, signage, and lighting; public street sections may vary from citywide standards in order to create this unique identity.

1.19.14 Require that development of Town Center incorporate the natural and cultural resources on site including:

- a. preserving portions of the basalt industrial ruins and incorporating them into the commercial and public activities, to the extent it is economically viable;
- b. preserving the quarry lake as a future public park site, for the benefit of both Town Center residents and the residents of the City as a whole.

The WRSP details out low impact development practices, outlined in Appendix A. The WRSP requires use of recycled water for landscape irrigation for major park and arterial landscape parkways, and provides guidance for the use of drought tolerant landscaping, and energy efficient homes. The Plan includes an extensive system of neighborhood multi-use paths (pedestrian and bicycle) which links neighborhoods with parks and

commercial areas; in addition, the local paths will connect with two regional trails – Napa Valley Vine Trail and River to Ridge Trail. The WRSP features a mix of land uses including residential, parks, and commercial areas which will reduce automobile travel.

Residential neighborhoods within the WRSP are linked to the NVRG Area (Town Center Core) through a system of multi-use trails. The NVRG Area will include public gathering space such as a plaza which will accommodate gatherings for community events.

WRSP design guidelines, as contained in Appendix A, provide unique standards for the NVRG Area.

A substantial portion of ruins will be activated; those preserved portions will be incorporated into commercial and public activities.

The Quarry Pond will be preserved and the surrounding area will be dedicated to the city and improved as a public park.

Development Policy: Community Subarea M – Town Center

Uses/Density:

- **Location and types of land uses, residential densities and non-residential intensities shall be determined through approval of subsequent Specific Plan(s) (1.19.7)**

Design and Development:

- **WRSP required prior to development of any portion of the site (including land use plan, circulation plan, infrastructure plan, public facility plan, conceptual landscape plan, and similar elements) (1.19.10)**
- **Adherence to Town Center Design and Development Principles sections 1.19.10-1.19.14**
- **Create a “main street” design in the commercial portion of the Town Center Core Area with such items as on-street parking, plaza and buildings constructed to the right-of-way line (1.19.12)**
- **Incorporate pedestrian and bicycle circulation linkages to the Community Commercial Center along Rio Del Mar**
- **Provide view corridors to the Napa River, valley, and foothills**
- **Encourage the retention and integration of existing structures previously used for the basalt plant operations**

- **Town Center Core Area will be centered around the basalt industrial ruins and quarry lake; this mixed-use area will have higher residential densities and higher intensity of non-residential uses; Residential Neighborhoods will surround the Core Area and will generally have lower residential densities and will include community facilities such as a park**

The WRSP identifies Land Uses, a range of residential densities and describes non-residential uses and densities.

WRSP is being proposed in advance of development of the area.

The circulation plan calls for a pedestrian and bike trail along Rio Del Mar to Highway 29.

A substantial portions of ruins will be preserved and re-used..

The WRSP calls for Medium and High Density residential surrounding core area.

1.32.5 Require the use of drought-tolerant species in landscape design in accordance with the provisions of the Water Conservation and Landscape Act. (I 1.1, I 1.2, I 1.4, I 1.5, I 1.7, and I 1.8)

1.32.6 Require that commercial, industrial, and multi-family residential development incorporate adequate drought-conscious irrigation systems and maintain the health of the landscape. (I 1.1, I 1.2, I 1.4, I 1.5, I 1.7, and I 1.8)

The Design Guidelines in the WRSP address landscape design and the use of drought-tolerant species.

The WRSP Master Landscape Plan requires uses of drought-tolerant species.

B.2 Housing Element

PROGRAM 2.1.2

Sustain residential land use designations that:

1. *Include a minimum density for each designation's range of permitted densities.*
2. *Allow flexibility in the types of units that may be constructed in master-planned communities and other planned developments.*
3. *Include a density category that, when combined with an affordable housing density bonus, is sufficiently high enough to facilitate the development of lower-income housing.*

The residential portion of WRSP provides three density categories: LDR, MDR, and HDR. Each density category has a minimum and maximum density as defined by the City's General Plan.

WRSP will be a master planned community. It provides flexibility in the types of housing units as follows:

4. *Medium Density Residential allows single family detached, single family attached and townhouses*
5. *High Density Residential allows both apartments and townhomes*
6. *The Implementation Chapter 9 allows a density transfer, which means the redistribution of residential units from one planning area to another, under specified procedures and criteria*

The High Density Residential has a density range of 18 to 30 dwelling units per acre

Policy 2.2.1 Allow flexibility in the type of units developed on vacant, residentially designated properties in master-planned communities and other planned developments.

Policy 2.2.2 Require larger projects to include a mix of housing types

Policy 2.2.3 Encourage the development of residential uses in association with compatible nonresidential uses.

Objective 2.3 Promote residential design that is functional, people and pedestrian-oriented, aesthetically pleasing, and contributes to a sense of community through the sensitive arrangement of buildings, open space (public and private), and circulation (vehicular and pedestrian). Encourage innovative and creative design in residential projects.

Policy 2.3.1 Ensure that new residential development fulfills the above objective through the establishment and application of comprehensive design guidelines and development standards.

Objective 2.6 Expand the availability of affordable housing in American Canyon

WRSP will be a master planned community. It provides flexibility in the types of housing units as follows: Low Density Residential allows both single family detached and secondary living units

Medium Density Residential allows single family detached, single family attached and townhouses High Density Residential allows both apartments and townhomes

The Implementation Chapter 9 allows a density transfer, which means the redistribution of residential units from one planning area to another, under specified procedures and criteria.

WRSP allows a broad range of housing types including single family attached, single family detached, townhomes, apartments, condominiums, and secondary living units.

WRSP proposed residential uses in conjunction with nonresidential uses.

The WRSP Circulation Plan provides an extensive system of multi-use trails linking the plan area and providing connections to regional trails. WRSP provides approximately 36 acres of public and private open space.

WRSP contains comprehensive design guidelines and development standards.

WRSP does not call for the construction affordable housing within the project boundaries.

PROGRAM 2.13.1 Require all residential projects of ten or more above moderate-income units to include affordable units.

The intent of the Inclusionary Alternatives is to provide options for developers while still meeting the City's affordable housing demand. Alternatives to providing on-site inclusionary units are 1) the provision of the units at an acceptable off-site location or 2) payment of in-lieu fees to the City's Housing Fund, to be used in the provision of housing affordable to lower-income households, or 3) the dedication of suitable land to the City for future inclusionary units, or 4) if the project exceeds its required inclusionary amount, the project owner may request inclusionary credits be counted toward another project subject to the approval of the City Manager

Included within Zoning Code Chapter 19.28 Inclusionary Housing Requirements is Section 19.28.050, Inclusionary Alternatives. Subsection B.2. states:

Provided, however, that the following alternatives to providing inclusionary units on-site may be approved by the decision-making body if it finds that evidence presented by the applicant shows that on-site inclusionary units are infeasible due to project size, location or site characteristics, or that the alternative would further housing opportunities for lower-income households to an equal or greater extent: The provision of some or all of the required inclusionary units at an offsite location.

The payment of an in-lieu contribution to the City equal to \$10,250.00 per unit in an ownership project. (The in-lieu contribution alternative is not available to rental projects.)

All fees shall be updated annually to adjust for market conditions.

Dedication of suitable land can be accepted as a substitute for payment of fees

The WRSP proposes that project will comply with the Inclusionary Housing Ordinance through payment of an in-lieu fee or its financial equivalent.

B.3 Economic Development

Town Center Development

The City currently does not have a Town Center. Designation of the basalt plant area to be a government center and a commercial core will provide at least one stimulus for further development in the long term.

3.6.2 Provide and promote opportunities for uses which capitalize on the City’s proximity to the wine country and the airport, as well as take advantage of the City’s natural resources. Consider tourist- supported commercial activities, such as a wine train depot, winery outlets, hotel, and golf course.

3.7.1 Adopt a Specific Plan for the Highway 29 commercial corridor and Town Center to guide future developments. Provide for a cohesive urban design which creates and maintains an attractive image to ensure that the City captures its full potential share of market demand. (I 3.1)

Objective 3.8 In the long term, improve and strengthen the City’s identity through the creation of a Town Center as a principal government and commercial center.

3.8.5 Encourage the development of uses in the designated Town Center area (e.g., theater complex, auditorium, cultural entertainment, dining, retail, or other) that would draw residents, promote foot traffic and provide evening activity. (I 3, 6)

The NVRG provides for a variety of commercial uses within the NVRG Area.

The NVRG provides for commercial uses in and around the ruins of the Portland Cement Company structures and basalt plant structures. These structures provide a unique environment for specialty retail, wineries, pubs, distillery, special events and entertainment. Development of this unique property assists in establishing the City’s identity and image.

The allowable uses within the NVRG includes several regional (tourist-oriented) uses such as hotel, events center for weddings and receptions, winery, pubs, distillery, and specialty retail.

The WRSP is the specific plan for the Town Center area. It will include design guidelines to ensure a cohesive and attractive design.

The NVRG Area corresponds to the Town Center Core Area in the General Plan. It is planned as a mixed-use area where government uses are an allowable

amphitheater(s)

The uses allowed in the NVRG include uses that generate activity such as: specialty retail, events center for weddings and receptions, winery, brewery, distillery, hotel, restaurants, farmers market and outdoor amphitheater for entertainment.

B.4 Circulation Element

Achieve and maintain a Multimodal LOS D or better for roadways and intersections during peak hours where possible and as long as possible. However, recognizing that LOS D may not be achievable or cannot be maintained upon full build-out of the General Plan, due to traffic generated from sources beyond the control of the City, the City Council shall have the discretion to only require feasible mitigation measures that may not achieve LOS D, but will reduce the impact of any development use or density planned for in the Land Use Element of the General Plan.

1.24 Impacts of new development. Based upon the findings of a transportation impact analysis, consistent with Guiding Policy 1.26, new development will be responsible for mitigation of transportation related impacts.

1.34 Rights-of-Way fully within master plan boundaries. Planning areas shall not use roadway centerlines as boundaries. Roadways shall be built to their full width within the annexed City limits. Part-width roads shall not be permitted where master plan areas abut unincorporated properties that are not expected to be annexed to the City within the time frame of this General Plan. Road rights-of-way that demarcate the edge of a planning area shall be fully contained within the development area boundary, and expanded only within that boundary.

4.10 New railroad crossings. Provide new crossings across the railroad (UPRR) in conjunction with the planned roadway improvements shown on Figure 3. The provision of new crossings will ensure at least one cross-town route is maintained in case rail activity interrupts local and regional traffic flow and/or emergency access. New crossings will be at: Newell Drive (grade separated); and Rio Del Mar or South Napa Junction Road (initially at-grade, may be grade separated in the future).

An Environmental Impact Report was prepared by the City prior to its consideration of WRSP. A determination of the LOS was made through the EIR process.

New development shall mitigate its share of transportation-related impacts.

The alignment for Newell Drive extension is proposed along the eastern boundary of the WRSP. The proposed roadway is entirely within the city limits and entirely within the boundary of the WRSP.

The WRSP incorporates a below grade or at grade crossing at Rio Del Mar connecting Highway 29 with Newell Drive. This will ensure a cross town connection without interruption by the rail activity.

Should an at-grade or grade-separated railroad crossing at Rio Del Mar prove unfeasible, an alternate route for providing the connection with Highway 29 is South Napa Junction Road. There exists a private at-grade railroad crossing at South Napa Junction that is the subject of a previous settlement agreement.

B.5 Utilities Element

5.8.1 Require improvements to the existing water supply, distribution, storage, and treatment facilities necessitated by a new development proposal be borne by the project proponent (in proportion to benefit); either through the payment of fees, or by the actual construction of the improvements. (I 5.2, I 5.8, and I 5.9)

5.10.3 Require that adequate storm drain and flood control facilities be constructed coincident with new development. (I 5, 24, 15,25, and I 5.33)

5.11. Require improvements to existing storm drain and flood control facilities necessitated by a new development proposal be borne by the project proponent; either through the payment of fees, or by the actual construction of the improvements in accordance with State Nexus Legislation. (I 5.30, I 5.31, and I 5.33)

5.16.1 Require that the cost for improvements to the existing wastewater collection and treatment facilities necessitated by a new development proposal be borne by the project proponent in proportion to benefit; either through the payment of fees, or by the actual construction of the improvements. (I 5.51 and I 5.52)

The Infrastructure Chapter of WRSP identifies the improvements for water distribution, storage and treatment necessary to serve the proposed development. A Water Supply Assessment was prepared in conjunction with the EIR. New development with WRSP will make a proportionate contribution to the City's water distribution, storage and treatment facilities.

The Infrastructure Chapter of WRSP identifies the storm water detention and conveyance facilities needed to serve the development. The WRSP states that appropriate storm water detention and conveyance facilities shall be constructed with each phase of development. These facilities will be designed and constructed to accommodate a 100-year storm event.

New development with WRSP will make a proportionate contribution to the City's wastewater collection and treatment facilities.

B.6 Public Services and Facilities

6.1.1 Work with the Napa Valley Unified School District to ensure that school facilities and programs are expanded commensurate with the City's population growth and development. (I 6.1 and I 6.2)

*Development in the WRSP Area shall mitigate its school impacts through the payment of school impact fees, pursuant to controlling law.***6.1.13** Locate preschool and day care facilities in appropriate areas throughout the City to meet the needs of a growing population. Require master planned areas to designate sites for daycare facilities. (16.1 and I 6.4)

The WRSP Land Use Plan includes the Town Center Mixed Use (TC-1 MU) area (NVRG) where allowable uses include community facilities such as a community center site, preschool and day care uses.

B.7 Parks and Recreation

7.1.1 Provide a sufficient number of mini, neighborhood, community and regional park facilities to achieve a minimum standard of 5 acres of parkland per 1,000 residents (see Figure 5-1 and Table 5-1 for an illustration and explanation of existing park facilities). (I 7.1)

7.1.3 Work toward the establishment of a system of public parks interconnected by off-street trails or bicycle lanes. (I 7.1 and I 7.4)

7.2.5 Require all large scale developments to incorporate an internal trail system with linkages to the surrounding sidewalk network. (I 7.7)

The WRSP Land Use Program allows for a maximum of 1439 dwelling units. Using the 3.49 persons per dwelling unit figure in the City's General Plan, the project would have 5022 residents. At 5 acres per 1000 residents, the standard for parkland for the WRSP is 25.1 acres. The WRSP Land Use Plan designates approximately 33.27 acres for public park and open space uses, exceeding the City standard.

As shown on Figure 5.1 of WRSP, the public parks are connected by a series of multi-use trails.

As shown on Figure 5.1 of WRSP, there is an internal system of multi-use trails which connect neighborhoods with public parks, commercial areas and regional trails.

7.3.1 Establish the following classifications for American Canyon's parks:

a. Mini-Park-Mini parks are less than one acre in size and are not designed for active recreational uses. They do, however, provide passive open space and buffering from adjacent urban development and typically include elements such as walking paths and benches.

b. Neighborhood Park Neighborhood parks are usually two and a half to five acres in size and are primarily planned for children five to fourteen years of age. To maximize the use of the parcel, these parks are ideally located within the center of a neighborhood and sited adjacent to a school whenever possible. Recreation facilities typically provided in neighborhood parks include children's play areas (ideally equipped with a variety of play equipment for a range of users), picnic tables, and basketball courts. The service area for a neighborhood park is generally one-quarter to a one-half mile radius.

7.4.4 Require that adequate development and maintenance funds are available before new parklands are acquired. (I 7.24)

7.4.7 Require that new residential subdivisions dedicate parklands within their project boundaries, unless it is the City's objective to develop a park that serves the subdivision at an alternative location as specified in the Parks and Recreation Master Plan, in which case in-lieu fees shall be provided by the developer. (I 7.1 and I 7.3)

7.4.8 Require that new multi-family residential developments of five or more units provide recreational or open space facilities onsite and contribute fees that aid in the public development of other facilities to offset additional demands generated by their resident population. (I 7.1 and I 7.3)

The WRSP Land Use Plan shows two large parks which serve the WRSP community. The type of recreational facilities and recreational uses within each park will be determined through City approval of a conceptual park plan concurrent with a tentative subdivision map.

The parks within WRSP will be dedicated and improved by the project for fee credits. That policy also requires that public parks be maintained through a Lighting Landscaping Assessment District, which must be established concurrently with final subdivision map approval.

The dedication of park land shall occur through the City's approval of final subdivision maps.

The Development Standards require multifamily residential development to include private open space at a ratio of 80 square feet per dwelling unit and 150 square feet of common open space per dwelling unit. The common open space would provide for recreational and open space facilities onsite.

7.7.4 Design and improve community and neighborhood parks according to the following:

- a. locate on collector or neighborhood streets, accessible to adjacent residential neighborhoods;
- b. site uses so that they do not adversely impact adjacent residences (e.g. locate high activity, noise-generating uses away from residences);
- c. provide parking so that it does not disrupt abutting residences;
- d. design for defensible space;
- e. site parks away from high noise generators (highways) and other nuisances (i.e., power lines); and
- f. incorporate park landscape that is compatible with the landscape of adjacent areas. (I 7.4, 1 7.5, and 1 7.28)

As shown on the Land Use Plan, all public parks are located on collector or neighborhood streets and accessible to surrounding residential neighborhoods.

Landscaping within the parks should support the overall character of the Watson Ranch Community as defined by the Design Guidelines.

B.8 Natural & Historic/Cultural Element

Goal 8E To promote the preservation and restoration of the sites, structures and districts that have architectural, historical, archaeological and/or cultural significance to the City of American Canyon.

Objective 8.19 Ensure that the City’s historically and archaeologically significant resources are protected in a manner that preserves and/or enhances the resources’ inherent historic value,

Policies

8.19.1 Conduct a comprehensive survey of archaeological and cultural resources and historic vegetation that is based on established criteria and encompasses the entire City and its Sphere of Influence. (I 8.40)

8.19.2 Adopt a Preservation Ordinance that will authorize the City to designate appropriate vegetation or archaeological sites deemed to be of historic, archaeological, or cultural significance an American Canyon City Historic Point, Site or District. Such an ordinance shall conform to state and federal criteria for establishing a preservation ordinance. (I 8.5, I 8.48, and I 8.49)

8.19.4 Though the design review process, encourage compatibility between new development and existing adjacent historic structures in terms of scale, massing, building materials and general architectural treatment. (J 8.29)

8.20.4 Prohibit demolitions if other alternatives exist that enable a property owner to sensitively add to the existing structure, or develop an accompanying building on the site that allows property development rights to be realized. Variances of setbacks, heights and parking requirements should be given to make the preservation of an existing historic building feasible when no other reasonable alternative exists. (J 8.49)

8.20.4 Prohibit demolitions if other alternatives exist that enable a property owner to sensitively add to the existing structure, or develop an accompanying building on the site that allows property development rights to be realized. Variances of setbacks, heights and parking requirements should be given to make the preservation of an existing historic building feasible when no other reasonable alternative exists. (J 8.49)

8.20.5 Encourage appropriate adaptive reuse of historic resources such as the Basalt Plant in order to prevent misuse, disrepair and demolition, taking care to protect surrounding neighborhoods and/or agricultural land from incompatible uses. (I 8.49)

The City has not established criteria or conducted an archaeological and cultural resources survey for the city and its Sphere of Influence. An archaeological survey was prepared by ESA (dated 4/2006). Further assessment was included in the EIR. The evaluation of the resources onsite will be evaluated in accordance with applicable State and federal criteria and guidelines.

The City has not adopted a Preservation Ordinance.

8.13.1 Encourage the preservation of agricultural uses on the City’s periphery through the creation of a permanent greenbelt. (I 8.11, I 8.17, r 8.26, I 8.28, and I 8.41)

Through the initiative process, the City has adopted an Urban Limit Line (“ULL”), as part of its General Plan. In

accordance with the provisions of that initiative, no urban development may occur outside the ULL. The terms of the initiative remain in effect until January 1, 2030.

B.9 Geology Element

9.4.1 Require the determination of the landslide, slope instability, and erosion potential of all proposed development sites with a grade of 10 percent or greater and incorporate pertinent measures in the project design to mitigate this potential. Exceptions to these mitigation requirements shall be considered for agricultural areas. (I 9.1, I 9.6, I 9.25, 19.27, and I 9.28)

9.4.3 Prohibit the mass grading of slopes with a grade of 25 percent or greater and development on slopes with a grade of 50 percent or greater. For projects built upon slopes with a grade of between 15 and 25 percent, cluster the units and reduce overall density in accordance with Natural/Cultural Resources Policy 8.18.2. (I 9.1 and 19.2)

A preliminary geotechnical study shall be prepared in conjunction with the WRSP EIR. The study evaluates the potential for landslides, slope instability and erosion and proposes mitigation measures. Development shall comply with any adopted mitigation measures in accordance with the Mitigation Monitoring and Reporting Program.

Development in WRSP Area is on land with a slope less than 15%.

B.10 Flood Hazards Element

10.1.4 Ensure that storm water drainage is designed for peak flow conditions. (I 10.1)

All storm water detention and conveyance facilities within the WRSP are designed to accommodate a 100- year storm event.

B.11 Noise Element

Airport Vicinity Land Use Compatibility Criteria Land Use Zone E (Figure 11-2): Prohibited Uses: Noise-sensitive outdoor uses

Other Development Conditions: Overflight easement or deed notice required Examples of Normally Acceptable Uses: Any permitted use

Examples of Uses not Normally Acceptable:

- Amphitheaters
- Landfills
- Ponds
- Airport Land Use Zone E

7. Maximum residential densities in accordance with local adopted General Plans and zoning designations. Consideration should be given to the proximity of flight patterns, frequency of overflight, terrain conditions, and type of aircraft in determining acceptable locations of residential uses. Referral to the ALUC for review of development plans prior to approval is recommended.

Objective 11.6 Minimize the potentially adverse noise impacts associated with the development of mixed-use structures in which residential units are located above ground floor commercial uses.

Policies 11.6.1 Require that the commercial component of a mixed-use project not generate excessive noise. (I 11.9)

11.9.1 Require that entertainment and restaurant/bar uses take appropriate steps to control the activities of their patrons onsite, as well as within a reasonable and legally justified distance from the place of business, to minimize potential noise-related impacts on adjacent residential neighborhoods. (I 11.11)

The WRSP was submitted to ALUC for review and a conformance determination with the Airport Land Use Plan was made.

A Residential Compatibility Plan shall be prepared prior to approval of nonresidential development; noise standards and noise mitigation measures are included in that Plan.

11.9.2 Discourage the development of new nightclubs, discotheques, and other high noise-generating entertainment uses directly adjacent to any residential neighborhoods, residential dwelling units, schools, health care facilities, or other “noise-sensitive” land uses, unless it is demonstrated that adequate measures can be installed and employed by these establishments to adequately mitigate the potential impacts of onsite operations and/or offsite customer activities upon these areas. (I 11.8)

Such high noise-generating uses are not permitted per WRSP Table 4.2 (Permitted and Conditionally Permitted Uses).



TITLE

Public Hearing Notification Ordinance Amendment and Discretionary Project Site Posting Policy

RECOMMENDATION

Take the following actions related to Discretionary Project Site Posting Policy:

1. Waive first reading, read by title only, and adopt an Ordinance by the City Council of the City of American Canyon amending American Canyon Municipal Code (ACMC) Chapter 19.40.050 to increase hardcopy property owner and tenant notification from 300 feet to 500 feet of a proposed project location.
2. Adopt a Resolution of the City Council of the City of American Canyon approving a Discretionary Project Site Posting Policy.

CONTACT

Brent Cooper, AICP, Community Development Director

BACKGROUND & ANALYSIS

On October 18, 2022, the City Council and Planning Commission, in a joint meeting, conducted a workshop to discuss the draft Housing Element and City's discretionary development public hearing notification requirements. The public hearing notification workshop reviewed existing state and municipal code notification requirements and outreach practices above and beyond municipal code standards.

At the conclusion of the public hearing notification workshop, the City Council directed staff to revise the Municipal Code to increase the hardcopy letter notification radius from 300 feet to 500 feet of a proposed project; and require an on-site sign that informs the public that a development application is under review at that location. A copy of the October 18, 2022 public hearing notification workshop staff report is included as Attachment 3. A note on this report informs the reader that the Housing Element component of the staff report has been removed.

On August 24, 2023, the Planning Commission reviewed and recommended approval of the draft Municipal Code Update with one modification and approval of the Discretionary Project Site Posting Policy with input on augmenting community outreach as described below:

Municipal Code Update: The Planning Commission noted that the proposed ordinance expands

notice to property owners from 300 feet to 500 feet, but does not provide notice to residential tenants. As a result, the Planning Commission recommended the Municipal Code update include one modification to provide hardcopy public hearing notice to tenants residing within 500 feet of the project site. Staff revised the proposed Municipal Code amendment accordingly. This will ensure tenants who reside within the 500-foot boundary will receive a hardcopy notice. The Planning Commission further recommended that the City Council consider a wider notification boundary than 500 feet in the future.

Discretionary Project Site Posting Policy: In addition to the Discretionary Project Site Posting Policy, the Planning Commission recommended the City Council consider augmenting outreach efforts as follows:

- explore additional social media platforms for additional outreach in the community;
- consistent with Council Resolution 2010-75, verify that all applicants conduct neighbor outreach prior to submitting their development application to the City;
- review and modify as necessary the aesthetics of the on-site discretionary application sign; and
- maintain the discretionary application sign on-site until the project construction is completed.

The Community Outreach team in the City Manager's Office and Community Development Department have significantly increased outreach opportunities lately and are motivated to continually improve outreach. The team will consider the Planning Commission's input and advise the City Council during the regular Management Updates on these or other new outreach initiatives.

Council Resolution 2010-75 suggests that applicants conduct neighbor outreach prior to submitting their development application to the City. Staff shares this resolution with prospective applicants and many have conducted outreach with neighbors with varying degrees of effort and results. As we consider outreach efforts generally, staff will evaluate whether a more prescriptive outreach effort is needed to improve neighbor awareness and input prior to application submittal. A copy of Resolution 2010-75 is included as Attachment 6.

With regard to sign aesthetics, staff will seek input from one or more sign fabrication companies to obtain their feedback on aesthetics and reader legibility and modify the sign template accordingly.

When construction begins, staff agrees that an onsite *construction sign* would be useful to inform the public during construction. The *construction sign* could explain the project and provide contact information for dust/noise complaints etc. Maintaining a *discretionary application sign* on-site through project construction is a challenge because there could be months or years between project approval and construction. During this time, the sign could fall into disrepair and misinform the public because the discretionary application is completed. With Council direction, staff will evaluate requiring a *construction sign* for inclusion in an updated Discretionary Project Site Posting Policy.

Informal Outreach Enhancement

As discussed during the October workshop, staff enhanced outreach efforts by using the GovDelivery email tool. This tool, available to anyone that signs up from the City's website, will receive email updates on many topics. Currently, there are approximately 3,800 email recipients for Community Development Department notifications.

These notifications include:

- Project Information Announcements. The email includes a link to the Project Posted on the City's website with a link to receive public comments.
- Public hearing notices.
- Published Planning Commission and City Council agendas.
- Public hearing reminders.
- "Around Town" newsletters and other periodic notifications.

The enhanced GovDelivery email outreach has yielded much more public input on development projects. Staff responds to each commenter and provides the comment and response to the Planning Commission and City Council as part of the agenda packet.

Municipal Code Update

Early into the development application process, staff mails a hardcopy letter to property owners within 300-feet of a project to inform them that a project application has been submitted and invite their participation in the review process. This letter is sent as a courtesy to enhance neighbors' knowledge of a nearby development project. GovDelivery recipients receive this project introduction letter through an informational email.

When a project is ready for a public hearing, American Canyon Municipal Code (ACMC) Chapter 19.40.050(B)(1)(c) requires staff to mail a hardcopy public hearing notice to property owners within 300-feet of a project to inform them that a public hearing has been scheduled for the project.

Consistent with the City Council direction, staff revised ACMC Chapter 19.40.050(B)(1)(c) to expand hardcopy public hearing notices to property owners and tenants from 300 feet to 500 feet of a project location. To be consistent, staff would also expand the hardcopy letter to property owners and tenants within 500-feet of a project early into the development application process.

In addition to expanding the public hearing notice requirement to 500 feet and responding to the Planning Commission recommendations for tenant notification, the municipal code revision also includes correcting a few grammatical/typo errors as shown in ~~strikeout~~/redline text below:

19.40.050 Notice of public hearing.

When a public hearing is required by this chapter, notice of the hearing shall be given in the manner specified in this section.

A. *Contents of Notice.* A required public hearing notice shall contain at least the date, time, and place of the hearing; the identity of the hearing body, and the nature and location of the application in sufficient detail for a member of the public to be able to understand the nature, intent and scale of the project or request.

B. *Method of Noticing.* At least ten calendar days before the date of any public hearing, required notice shall be given through all ~~of~~ the following actions:

JUSTIFICATION: Corrects a grammatical error.

1. *Publishing such notice once in a newspaper of general circulation within the city, or if none, in a newspaper of general circulation within the county of Napa; and*

2. *Mailing or delivering notice, postage prepaid, to all ~~of~~ the following:*

JUSTIFICATION: Corrects a grammatical error.

a. *The property owner,*

b. *The applicant,*

c. *The owners of all property within ~~three-five hundred~~ (500) feet of the exterior boundaries of the property which is the subject of the application, and*

JUSTIFICATION: Increasing the public notification radius from 300 to 500 feet is consistent with Council direction on October 18, 2022.

d. *Address tenant notification with all physical addresses within five hundred (500) feet of the exterior boundaries of the property which is the subject of the application, and*

JUSTIFICATION: The Planning Commission on August 24, 2023 requested that all tenants residing within 500 feet of the boundaries of the project receive a hardcopy public hearing notice.

~~de.~~ *Any person who has filed a written request for such notice ~~;~~*

JUSTIFICATION: Corrects a punctuation error.

3. *For ~~the~~ purposes of this mailed or delivered notice, the last known names and addresses of property owners on the last county assessment roll shall be used. If the number of owners to whom notice would be mailed or delivered is greater than one thousand, a display advertisement of at least one-eighth page in at least one newspaper of general circulation may be published at least ten days prior to the hearing in lieu of mailed or delivered notice. (Ord. 2002-07 Exh. A, 2002; Ord. 2001-02 § 1, 2001).*

JUSTIFICATION: Language simplification.

A copy of the proposed Public Hearing Notice Ordinance is included as Attachment 1.

On-Site Sign Notification

In addition to the expanded hardcopy letter notification and public hearing notice radius, the City Council directed staff to amend the development review procedures to require Development

Applicants install a durable sign on their property that announces their proposed development application.

Much like familiar commercial real estate signs we see around town that announce property for sale or lease, the development application sign would need to be large enough to be visible from passing motorists and include contact information and a brief description of the proposal.

Staff reviewed development application signs from Seattle, Toronto, and Vancouver and prepared sign criteria to ensure sufficient information for the sign to be useful. A copy of the signs from each of the three sample cities are included as Attachment 4. Among the three sample cities, staff liked the simple layout of the City of Toronto sign and used it as inspiration for an American Canyon sign template (see Attachment 5). The proposed sign template, which would measure 4 feet tall by 6 feet wide, would include this information:

- A Brief project description with detailed bullet points (i.e.: number of units or square footage, type of land use).
- Applicant name, phone number, email address.
- Required approvals.
- A picture of the proposed project.
- A map showing the location of the project in relationship to nearby streets and properties.
- Information on how to review the project online.
- The project comment period date.

Administration of the new on-site sign, including maintenance, property location, sign removal, etc. is described in a proposed “Discretionary Project Site Posting Policy.” This policy is included in Attachment 2.

When approved, the sign template would be available on the City's website so local sign fabrication companies can access it to quickly produce development application signs for their clients.

Next Steps:

Following City Council hearing, the Discretionary Project Site Posting Policy would become effective immediately. The Public Hearing Ordinance amendment would return to the City Council as a consent item at the September 19, 2023 meeting and become effective 30 days later on October 19, 2023.

COUNCIL PRIORITY PROGRAMS AND PROJECTS

Organizational Effectiveness: "Deliver exemplary government services."

FISCAL IMPACT

N/A

ENVIRONMENTAL REVIEW

Pursuant to the California Environmental Quality Act (CEQA), the proposed public hearing notification increase from 300 feet to 500 feet and the Discretionary Project Site Posting Policy qualifies for a Categorical Exemption CEQA Guidelines §15061(b)(3) as the public hearing notification procedures have no potential to cause a significant effect on the environment; CEQA Guidelines §15060(c)(2) as a project that will not result in a direct or reasonably foreseeable indirect physical change in the environment.

ATTACHMENTS:

1. [Ordinance Public Hearing Notice Update](#)
2. [CC Resolution Discretionary Project Site Posting Policy](#)
3. [CC Report Public Notice Workshop 10.18.22](#)
4. [Sample Notice Signs](#)
5. [Draft Notice Sign](#)
6. [Resolution 2010-75 Development Review Process](#)

ORDINANCE NO. 2023-_____

AN ORDINANCE OF CITY COUNCIL OF THE CITY OF AMERICAN CANYON, CALIFORNIA AMENDING AMERICAN CANYON MUNICIPAL CODE (ACMC) CHAPTER 19.40.050 TO INCREASE HARDCOPY PROPERTY OWNER AND TENANT NOTIFICATION FROM 300 FEET TO 500 FEET OF A PROPOSED PROJECT LOCATION

WHEREAS, on October 18, 2022, the City Council and Planning Commission, in a joint meeting conducted a workshop to discuss the City’s public hearing notification requirements for discretionary development; and

WHEREAS, the workshop reviewed existing municipal code notification requirements, State law requirements, and outreach practices above and beyond municipal code requirements; and

WHEREAS, on October 18, 2022, the City Council directed staff to revise the Municipal Code to increase hardcopy public hearing notification radius from 300 feet to 500 feet of a proposed project; and

WHEREAS, pursuant to the California Environmental Quality Act (CEQA), the proposed public hearing notification increase from 300 feet to 500 feet qualifies for a Categorical Exemption CEQA Guidelines §15061(b)(3) as the public hearing notification procedures have no potential to cause a significant effect on the environment; CEQA Guidelines §15060(c)(2) as a project that will not result in a direct or reasonably foreseeable indirect physical change in the environment; and

WHEREAS, on August 24, 2023, the City of American Canyon Planning Commission recommended City Council approval of the proposed Ordinance with a modification to require all residential tenants receive notification within 500 feet of the project location; and

WHEREAS, a duly-noticed public hearing was held by the City of American Canyon City Council on September 5, 2023 on the subject application, at which time all those in attendance were given the opportunity to speak on this proposal, and all comments were reviewed and considered.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON DOES HERE BY ORDAIN an amendment to American Canyon Municipal Code (ACMC) Chapter 19.40.050(B)(1)(c) to increase property owner hardcopy notification to 500 feet of a proposed project location, provide notification to all residents within 500 feet of the project location and correct grammatical/typographical errors as shown below:

19.40.050 Notice of public hearing.

When a public hearing is required by this chapter, notice of the hearing shall be given in the manner specified in this section.

A. Contents of Notice. A required public hearing notice shall contain at least the date, time, and place of the hearing; the identity of the hearing body, and the nature and location of the application in sufficient detail for a member of the public to be able to understand the nature, intent and scale of the project or request.

B. Method of Noticing. At least ten calendar days before the date of any public hearing, required

notice shall be given through all of the following actions:

JUSTIFICATION: Corrects a grammatical error.

1. Publishing such notice once in a newspaper of general circulation within the city, or if none, in a newspaper of general circulation within the county of Napa; and

2. Mailing or delivering notice, postage prepaid, to all of the following:

JUSTIFICATION: Corrects a grammatical error.

a. The property owner,

b. The applicant,

c. The owners of all property within ~~three~~ five hundred (500) feet of the exterior boundaries of the property which is the subject of the application, and

JUSTIFICATION: Increasing the public notification radius from 300 to 500 feet is consistent with Council direction on October 18, 2022.

d. Address tenant notification with all physical addresses within five hundred (500) feet of the exterior boundaries of the property which is the subject of the application, and

JUSTIFICATION: The Planning Commission on August 24, 2023 requested that all tenants residing within 500 feet of the boundaries of the project receive a hardcopy public hearing notice.

~~e.~~ Any person who has filed a written request for such notice;

JUSTIFICATION: Corrects a punctuation error.

3. For ~~the~~ purposes of this mailed or delivered notice, the last known names and addresses of property owners on the last county assessment roll shall be used. If the number of owners to whom notice would be mailed or delivered is greater than one thousand, a display advertisement of at least one-eighth page in at least one newspaper of general circulation may be published at least ten days prior to the hearing in lieu of mailed or delivered notice. (Ord. 2002-07 Exh. A, 2002; Ord. 2001-02 § 1, 2001).

JUSTIFICATION: Language simplification.

SECTION 1. CEQA FINDINGS. The City Council finds the municipal code amendment is not subject to the California Environmental Quality Act ("CEQA") pursuant to Section 15060(c)(2) because the activity will not result in a direct or reasonably foreseeable indirect physical change in the environment; and Section 15060(c)(3) because the activity is not a project as defined in Section 15378 of the CEQA Guidelines, California Code of Regulations, Title 14, Chapter 3 because it has no potential for resulting in physical change to the environment, directly or indirectly. In addition, the municipal code amendment is not a project under CEQA Regulation Section 15061(b)(3) because it has no potential for causing a significant effect on the environment.

SECTION 2. EFFECTIVE DATE. This ordinance shall become effective effect 30 days after its final passage pursuant to Government Code section 36937.

SECTION 3. SEVERABILITY. If any section, sentence, clause or phrase of this Ordinance is for any reason held to be invalid or unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have passed this Ordinance and adopted this Ordinance and each section, sentence, clause or phrase thereof, irrespective of the fact that one or more sections, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

SECTION 4. CUSTODIAN OF RECORDS. The documents and materials that constitute the record of proceedings on which this Ordinance is based are located at the City Clerk’s office located at 4831 Broadway, Suite 201, American Canyon, CA 94503. The custodian of these records is the City Clerk.

The foregoing Ordinance was introduced at a regular meeting of the City Council of the City of American Canyon, State of California, held on the 5th day of September, 2023, by the following vote:

- AYES:
- NOES:
- ABSTAIN:
- ABSENT:

The foregoing Ordinance was adopted at a regular meeting of the City Council of the City of American Canyon, State of California, held on the ____ day of _____, 2023, by the following vote:

- AYES:
- NOES:
- ABSTAIN:
- ABSENT:

ATTEST:

Leon Garcia, Mayor
APPROVED AS TO FORM:

Taresa Geilfuss, CMC, City Clerk

William D. Ross, City Attorney

RESOLUTION NO. 2023-_____

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON, CALIFORNIA
APPROVING A DISCRETIONARY PROJECT SITE POSTING POLICY**

WHEREAS, on October 18, 2022, the City Council and Planning Commission, in a joint meeting conducted a workshop to discuss the City’s public hearing notification requirements for discretionary development; and

WHEREAS, the workshop reviewed existing municipal code notification requirements, State law requirements, and outreach practices above and beyond municipal code requirements; and

WHEREAS, on October 18, 2022, the City Council directed staff to prepare a policy that would require development project sites to post an on-site sign that informs the public that a development application is under review at that location; and

WHEREAS, pursuant to the California Environmental Quality Act (CEQA), the proposed Discretionary Project Site Posting Policy qualifies for a Categorical Exemption CEQA Guidelines §15061(b)(3) as the public hearing notification procedures have no potential to cause a significant effect on the environment; CEQA Guidelines §15060(c)(2) as a project that will not result in a direct or reasonably foreseeable indirect physical change in the environment; and

WHEREAS, on August 24, 2023, the City of American Canyon Planning Commission recommended City Council approval of the proposed Discretionary Project Site Posting Policy; and

WHEREAS, a duly-noticed public hearing was held by the City of American Canyon City Council on September 5, 2023 on the subject application, at which time all those in attendance were given the opportunity to speak on this proposal, and all comments were reviewed and considered.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of American Canyon approves a Discretionary Project Site Posting Policy as shown below:

Discretionary Project Site Posting Policy

Sections:

1. Purpose.
2. Applicability.
3. Definitions.
4. Project Site Posting Criteria.
5. Sign Installation and Maintenance Requirements.
6. Sign Removal.

1. Purpose.

The purpose of the Discretionary Project Site Posting Policy is to enhance transparency by providing physical site information about key elements of a discretionary development application under consideration and invite public comment about the project.

2. Applicability.

The requirements of this policy shall apply to all new discretionary development applications on private property.

3. Definitions.

For purposes of this section, the following terms shall have the following meanings:

“Discretionary Development Application” means any application that is subject to the Review and Approval Procedures described in the American Canyon Municipal Code Chapter 19.40.

“Site” means the parcel or parcels that encompass the boundaries of the discretionary development application.

4. Project Site Posting Criteria.

A. Sign Location.

- i. One sign shall be posted midpoint on the street frontage(s) from which the site is addressed.
- ii. Each sign shall be setback between 5 feet to 10 feet from the property line.
- iii. Each sign shall be located outside the “Controlled Area” triangle at a driveway entrance or street intersection, consistent with City of American Canyon Engineering Standard 3.09.
- iv. Applicants shall call 811 to site any utilities before placing their sign. 811 is a free service managed by Underground Service Alert and available to everyone.

B. Sign Construction.

- i. The sign(s) shall measure 4 feet by 6 feet.
- ii. The sign(s) shall be placed between 7 and 9 feet above grade at the top of the sign, making it accessible for CD Staff to add comment period dates.
- iii. The sign face shall be made of ½ inch plywood or alternative material with equivalent durability.
- iv. Each sign shall be permanently attached to the ground by direct attachment to a rigid wall, frame, or structure.

C. Minimum Sign Content.

- i. Brief project description with detailed bullet points (i.e.: number of units or square footage, type of land use).
- ii. Applicant name, phone number, email address.
- iii. List required approvals.
- iv. Show a picture of the proposed project.
- v. Include a map showing the location of the project in relationship to nearby streets and properties.
- vi. Include information on how to review the project online.
- vii. List the date of the comment period on the project.

Sign Template

The above image represents the applicant's proposal. Submittal may change

Site address: Southeast corner Crawford Way/SR-29 (Broadway)
Applicant: Pensacola Residential, LLC
4957 Allison Parkway, Suite 2
Vacaville, CA 95688
Application # PL23-0003

SIZE
4.28 acres
3 Stories
38 Feet Height

USE
100 townhomes
Includes a 20% State Density bonus
2,654 SF Community Building
23.4 Dwelling Units/Acre

PARKING
200 Resident parking spaces
16 Guest parking spaces
8 Bicycle Parking stalls

For information & to tell us what you think:

WRITTEN LETTER:
William He, Senior Planner
4381 Broadway, Suite 201
American Canyon, CA 94503

ONLINE:
<https://cityofamcan.org/ProjectReview>
Application # PL23-0003

IN PERSON:
A meeting has not yet been scheduled. Details will be posted here and online as they become available.

5. Sign Installation and Maintenance Requirements.

- A. Before the sign is fabricated, the applicant shall provide staff with a draft copy of the sign. Staff will review the draft to ensure applicable sign content is provided consistent with the sign template.
- B. The applicant shall inform staff that they have installed the sign by sending the project manager a photograph of the sign in its installed location.
- C. All signs shall be maintained in good structural condition.
- D. Graffiti and other forms of vandalism shall be repaired within 48 hours of applicant notification.

6. Sign Removal.

The applicant shall remove the public notice sign within 10 calendar days of either:

- A. The project approval effective date;
- B. The project withdrawal date; or
- C. The project denial date.

PASSED, APPROVED and ADOPTED at a regularly scheduled meeting of the City Council of the City of American Canyon held on the 5th day of September, 2023, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

Leon Garcia, Mayor

ATTEST:

APPROVED AS TO FORM:

Taresa Geilfuss, CMC, City Clerk

William D. Ross, City Attorney

**TITLE**

6th Cycle Draft Public Review Housing Element Workshop and Review of the Public Hearing Procedure Ordinance

RECOMMENDATION

Adopt a Minute Order authorizing staff to modify the public review draft 6th Cycle Housing Element consistent with public comments, submit the Housing Element to the State Department of Housing and Community Development for the mandatory 90-Day review, and provide staff direction on a potential amendment to the public hearing procedure ordinance.

CONTACT

Brent Cooper, AICP, Community Development Director

BACKGROUND & ANALYSIS*Public Hearing Notice Requirements*

On October 5, 2022, the City Council directed staff to return with information related to the City's public hearing notification requirements for discretionary development. This direction was provided in response to concerns from the public that hardcopy letter notification is sent to too few property owners and residents.

American Canyon Municipal Code (ACMC) sets forth public hearing procedures in Chapter 19.40.050. These requirements mirror State Law minimum requirements for public hearings:

- California Government Code section 65091(a)(4) requires mailing or delivery to all owners or real property (as shown on the latest equalized assessment roll) within 300 feet of the real property that is the subject of the hearing.
- Government Code section 65090(a) requires notice of the hearing provided in a newspaper of general circulation within the local agency's jurisdiction at least 10 days before the hearing.
- Government Code section 65091(a)(4) states that if the number of owners to whom notice would be mailed or delivered is greater than 1,000, the local agency, in lieu of mailing or delivery, may provide notice by placing a display advertisement of at least one-eighth page in at least one newspaper of general circulation within the local agency in which the proceeding is conducted at least 10 days prior to the hearing.

Public Hearing Notification Survey

Staff reviewed the public hearing requirements from Napa County and several nearby jurisdictions.

- The smaller Napa County jurisdictions require a 300-foot notification. These include Calistoga, St. Helena, and Yountville.

- Fairfield has a 300-foot notification, but they allow the Director to increase the notification to 500 feet. Staff would not recommend this approach because it would place the Director in a position of conflict because there is no objective standard to determine when a wider notification is warranted.
- The Cities of Benicia and Napa have a 500-foot notification standard. However, Napa increases the notification to 750 feet and 1,000 feet until the notification list achieves a minimum of 30.
- Napa County, with low density and a large territory uses a 1,000-foot notification standard.

The survey results are included as Attachment 7.

Sample Notification Map for 300, 500, and 1,000-feet

Any parcel that “touches” the required notification distance will be included in the notification list. Shown in Attachment 8 is an example of a map that depicts a 300-foot, 500-foot, and a 1,000-foot notification distance from the proposed carwash at 3885 Broadway.

As shown on the map, the number of parcels included in each notification list is:

- The 300-foot notification distance encompasses 16 parcels.
- The 500-foot notification distance encompasses 43 parcels.
- The 1,000-foot notification distance encompasses 164 parcels.

Due to varying parcel sizes, every situation will yield different results, however, more parcels will be covered as the size of the notification distance increases.

Additional Notification Practices In Place

In addition to public hearing notice required by the municipal code, staff reaches out to residents to provide information on development application submittals. These include:

- a. The project is added to the Active Development Project list. This list includes all active development applications. Each development project is identified by name, type of application (i.e.: subdivision map, design permit, conditional use permit, etc.), project description, the applicant, address, site acreage, milestone status with dates, and staff project manager. The Active Development Project list is updated monthly. It is made available on the City’s website and included as a standing item on each Planning Commission agenda. A copy of the latest Active Development Project list is included as Attachment 9.
- b. The City’s website includes an interactive map that depicts development projects. This map can be viewed at this link: <https://bit.ly/3evwzWM>.
- c. The City Manager’s Update and Community Development Department “Around Town” newsletter features new development projects.
- d. Staff informs Council when a new development project is submitted during the standing Council meeting agenda item “Management Updates.”
- e. On rare occasions for unusual projects, the Planning Commission and/or City Council will conduct a workshop. This workshop would include a letter sent to property owners within 300 feet of the project.
- f. City Council Resolution 2010-75 encourages development applicants to conduct outreach to neighbors prior to submitting an application to the City. A copy of Resolution 2010-75 is included as Attachment 10.

Potential New Notification Practices

Staff continually innovates public outreach to make it easy for residents to live a busy life and remain aware of important events happening at City Hall. Two new ideas staff is considering is requiring development applicants

to install a durable sign on their property that announces a proposed development application. This sign would need to be large enough to be visible from passing motorists and would include contact information and a brief description of the proposal.

Currently, staff sends a letter to nearby neighbors when a project is newly-submitted. A second idea is to send an electronic copy of this letter to the extensive email distribution list of residents that have requested regular email updates. This information would augment the existing practice of emailing public hearing notices to the email distribution list.

Both these potential new practices would provide notification to a resident regardless of where they live – potentially well beyond any Municipal Code-required public hearing notification radius.

COUNCIL PRIORITY PROGRAMS AND PROJECTS

Organizational Effectiveness: "Deliver exemplary government services."

FISCAL IMPACT

The Housing Element cost is included in the adopted 5-Year Capital Improvement Program. There is no cost associated with the public hearing notification workshop.

ENVIRONMENTAL REVIEW

The draft Housing Element is exempt from the California Environmental Quality Act in accordance with Statutory Exemption 15061(b)(3) as a project that has no potential to cause a significant effect on the environment; CEQA Guidelines § 15060(c)(2) as a project that will not result in a direct or reasonably foreseeable indirect physical change in the environment. This determination applies because the Housing Element would not entitle any new housing because the RHNA will be accomplished by Projects that have already complied with CEQA or are proposed consistent with the existing general plan and zoning designations. A copy of the filed Notice of Exemption is included as Attachment 11.

ATTACHMENTS:

None



Notice

A change has been proposed for this site.

A Development Application has been submitted to amend the Official Plan and Zoning By-law to allow a residential mixed-use building with retail.

Proposal summary:

SIZE XX.X Density XX Storeys XX metres	USE XXX Residential Units XXX m ² Retail	PARKING XX Cars XX Bikes

District & Ward: Toronto & East York - Ward 22
Site Address: 123 Any Street and 456 Other Avenue
Applicant: ABC Development Corporation and XYZ Investment Group
Application File #: 15 123456 STE 22 OZ

For information & to tell us what you think:



COMMUNITY PLANNER:
 Planner's Name
 416-392-1234
 email@toronto.ca



APPLICATION
 INFORMATION
 CENTRE

ONLINE:
www.toronto.ca/aic
 File #: 15 123456 STE 22 OZ



PUBLIC MEETING

IN PERSON:
 A meeting has not yet been scheduled. Details will be posted here and online as they become available.

REZONING APPLICATION

1523 Davie Street (Gabriola Mansion)

DETAILS:

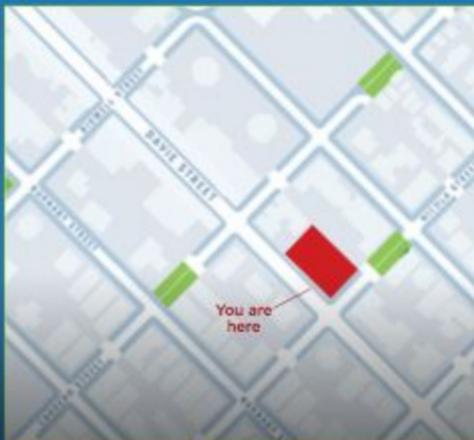
The City of Vancouver has received an application to amend the text of CD-1 (248) (Comprehensive Development) District for 1523 Davie Street (Gabriola Mansion). The proposal includes:

- the preservation and restoration of Gabriola Mansion;
- the conversion of the mansion into 16 market rental units;
- four new infill market rental townhouses to be developed in the northeast corner of the property;
- an increase in FSR from 0.35 to 0.89; and
- no increase in height from the existing CD-1 (248).

APPLICANT: Ankenman Marchand Architects
1645 West 5th Avenue
Vancouver, BC



PROPOSED DEVELOPMENT



LOCATION

What's happening:

Rezoning application submitted

Application review by City staff

Open House:

Best Western Plus Sands
1755 Davie Street

5-8 pm, June 8, 2017

Further application review

Public Hearing:

Information to come

Decision by Council

For more information:
vancouver.ca/rezapps
or phone 3-1-1

A New 7-Story Apartment Building

What is it?

- Units:
- Parking:
- Includes: (Use (if relevant))
- Demo of existing (if relevant)
- No more than 5 bullets

Required Approvals:

- Environmental Review
- Design Review
- Conditional Use

Submit comments to:

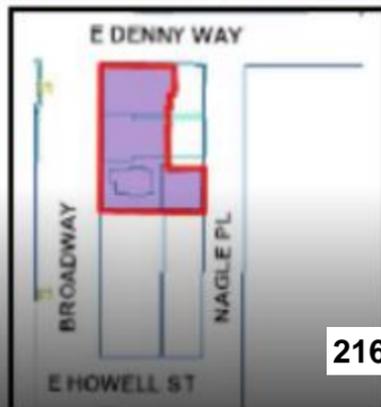
(The comment period may be extended by written request prior to the date below.)

- Email: PRC@seattle.gov
- Mail: SDCI
P.O. Box 34019
Seattle, WA 98124-4019

More Information:

- Online: Enter project number 3000000, 31000000, and 32000000 at www.seattle.gov/dpd/edms
- Phone: (206) 684-8467 (message line)

1830 Broadway



Submit comments by _____.

* All comments are posted on our website in their entirety.



Notice



The above image represents the applicant's proposal. Submittal may change

Site address: Southeast corner Crawford Way/SR-29 (Broadway)

Applicant: Pensacola Residential, LLC
4957 Allison Parkway, Suite 2
Vacaville, CA 95688

Application # PL23-0003

A change is proposed for this site.

A Design Permit Application has been submitted for 100 rental townhomes. The project is subject to vesting rights pursuant to the Housing Crisis Act of 2019 and the State Density Bonus Law.

Proposal Summary:



4.28 acres
3 Stories
38 Feet Height



100 townhomes
Includes a 20% State Density bonus
2,614 SF Community Building
23.4 Dwelling Units/Acre



200 Resident parking spaces
16 Guest parking spaces
8 Bicycle Parking stalls

For information & to tell us what you think:



WRITTEN LETTER:
William He, Senior Planner
4381 Broadway, Suite 201
American Canyon, CA 94503



ONLINE:
<https://cityofamcan.org/ProjectReview>
Application # PL23-0003



IN PERSON:
A meeting has not yet been scheduled. Details will be posted here and online as they become available.

RESOLUTION #2010- 75

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON
APPROVING A CITYWIDE DEVELOPMENT REVIEW POLICY**

WHEREAS, the City Council adopted a goal to revise and streamline the development review process; and

WHEREAS, the overall goal is to provide greater certainty to an inherently uncertain process by refining City procedures and policies so that the rules are clear and applied equally among the city's stakeholders; and

WHEREAS, the development review process provides greater certainty by allowing the applicant to take the lead in preparing Environmental Impact Reports along with guidelines for the timeframes for each step in the process; and

WHEREAS, on May 27, 2010, the Planning Commission considered the draft development review process and unanimously recommended its approval; and

WHEREAS, this item was considered on July 20, 2010 at a duly-noticed public meeting held by the American Canyon City Council at which time all those in attendance were given the opportunity to speak on the proposal; and

WHEREAS, the City Council considered all of the written and oral testimony presented at the public meeting in making its decision.

**NOW, THEREFORE, THE CITY COUNCIL DOES HEREBY RESOLVE
AS FOLLOWS:**

SECTION 1: DEVELOPMENT REVIEW PROCESS

Staff is authorized to process discretionary development applications that require an Environmental Impact Report (EIR) in accordance with the flowchart included as Exhibit A.

SECTION 2: DEVELOPMENT REVIEW TIMEFRAMES

Staff shall endeavor to comply with the review time guidelines included in the Development Review flowchart. In situations where workload and/or external factors prevent achievement of the review time guidelines, communication between the City and applicant shall ensure that the applicant is made aware of a reasonable date when work products are completed consistent with state-mandated timeframes of the Permit Streamlining Act.

SECTION 3: PUBLIC REVIEW

To enable stakeholders to learn about a project and express their concerns early in the review process, staff is authorized to provide notification of a discretionary project submittal to surrounding property owners early in the review process.

When a project requires an Environmental Impact Report (EIR), staff is authorized to conduct an EIR public workshop soon after the decision is made to prepare the EIR.

Applicants are encouraged to speak to stakeholders when conducting due diligence before the application is submitted. This process will give advance notice to interested stakeholders and help the applicant understand whether there are any significant issues associated with their project.

SECTION 4: CITY TRAFFIC ENGINEER

In the interest of using the best available consultants, when a project requires preparation of a Traffic Study, the applicant has the option to contract with the city's traffic engineer to prepare the traffic study for their project. The option requires approval of the Community Development Director and the City Attorney to ensure the contract is specifically tied to the project scope with indemnification for use of a city contract employee.

PASSED, APPROVED AND ADOPTED at a regular meeting of the City Council of the City of American Canyon on July 20, 2010, by the following vote:

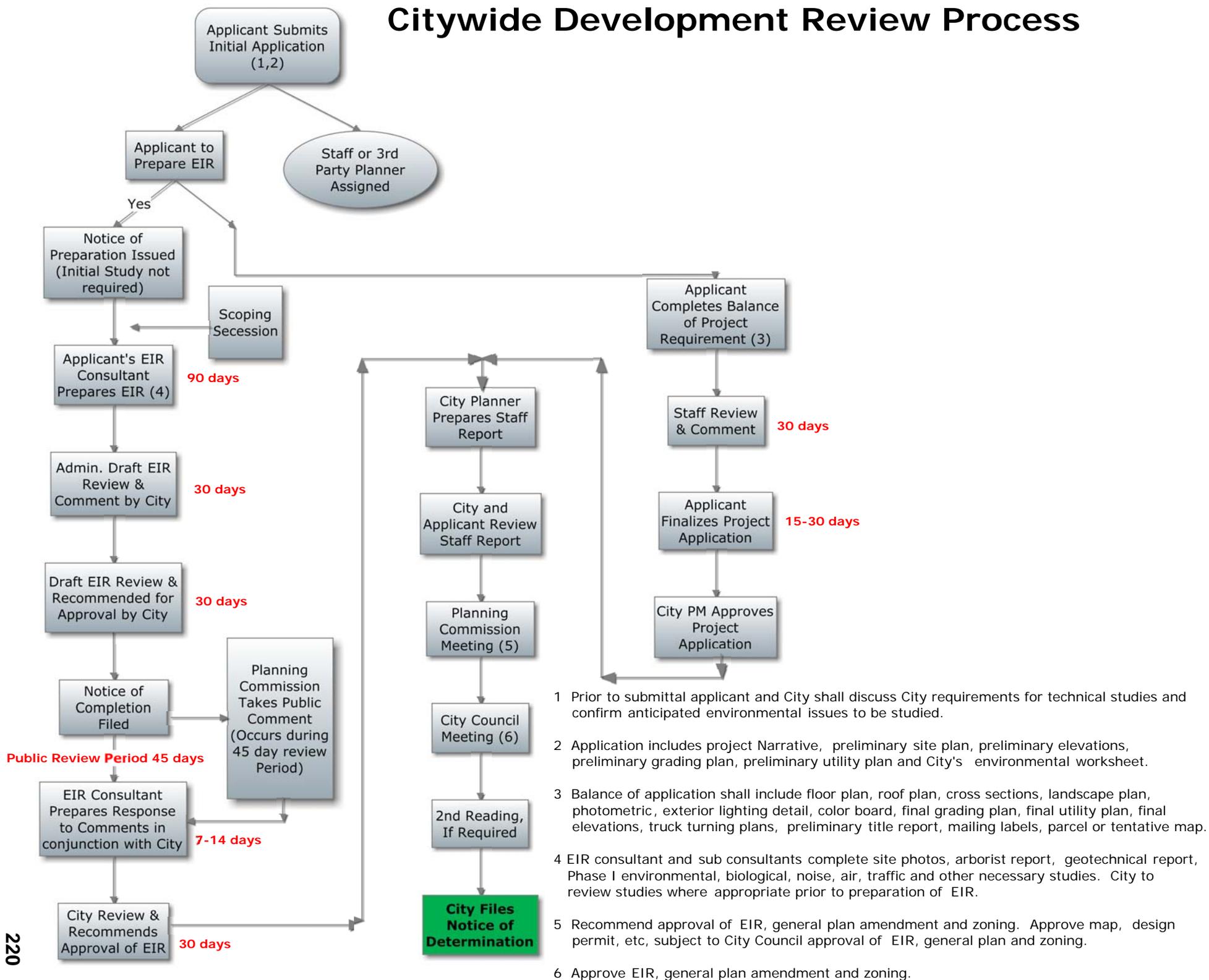
Mayor Garcia:	<u>yes</u>
Vice Mayor West:	<u>yes</u>
Council Member Bennett:	<u>yes</u>
Council Member Callison:	<u>yes</u>
Council Member Coffey:	<u>yes</u>

Leon Garcia
 Leon Garcia, Mayor

ATTEST:
Rebekah Barr
 Rebekah Barr, CMC, City Clerk

APPROVED AS TO FORM:
William D. Ross
 William D. Ross, City Attorney

Citywide Development Review Process



- 1 Prior to submittal applicant and City shall discuss City requirements for technical studies and confirm anticipated environmental issues to be studied.
- 2 Application includes project Narrative, preliminary site plan, preliminary elevations, preliminary grading plan, preliminary utility plan and City's environmental worksheet.
- 3 Balance of application shall include floor plan, roof plan, cross sections, landscape plan, photometric, exterior lighting detail, color board, final grading plan, final utility plan, final elevations, truck turning plans, preliminary title report, mailing labels, parcel or tentative map.
- 4 EIR consultant and sub consultants complete site photos, arborist report, geotechnical report, Phase I environmental, biological, noise, air, traffic and other necessary studies. City to review studies where appropriate prior to preparation of EIR.
- 5 Recommend approval of EIR, general plan amendment and zoning. Approve map, design permit, etc, subject to City Council approval of EIR, general plan and zoning.
- 6 Approve EIR, general plan amendment and zoning.



TITLE

Vehicle Miles Traveled (VMT) policy to implement Senate Bill (SB) 743

RECOMMENDATION

Adopt a Resolution of the City Council of the City of American Canyon, California approving a Citywide policy establishing Vehicle Miles traveled (VMT) as the standard of measurement for potential vehicle traffic impacts consistent with the California Environmental Quality Act (CEQA).

CONTACT

Brent Cooper, AICP, Community Development Director

BACKGROUND & ANALYSIS

A project's impact on transportation is one of the key environmental topics reviewed under California Environmental Quality Act (CEQA). When it is determined that a project may have a "significant impact," mitigation measures may be required, such as changes in project scope and scale, or construction of new transportation infrastructure.

Public agencies in California, such as American Canyon, have historically attempted to combat traffic congestion by relying on a metric known as "Level of Service" (LOS) standards. Developed in the post-war US specifically for highway travel, the LOS standard assesses the relationship between traffic speed, volume and density, putting a priority on how well automobiles flow through the street network.

Ultimately, the LOS standard has failed to combat congestion in the long run because the short-term convenience of a free-flowing roadway only encourages more people to drive their single occupant automobile to places they need to go. Furthermore, LOS views mobility very narrowly - a single person in a car counts as much as 50 people in a bus, even though 50 people in a single vehicle contribute far less to congestion than 50 people in 50 vehicles. Thus, measuring only the vehicles on a crowded roadway misses the fact that some of those vehicles are causing a congestion problem.

There is also a growing awareness of a connection between LOS standards enforcement and greenhouse gas emissions (GHG) and climate change. The California Air Resources Board (CARB) has documented that transportation is the leading source of GHG pollution in California of which passenger vehicles represent the largest single source of transportation GHG emissions. Thus,

continued emphasis of LOS standards, which favors single occupant automobile use to the exclusion of other more efficient forms of transportation, will lead to greater transportation-related GHG emissions.

With this in mind, in 2013, the State of California Legislature passed, and the Governor signed Senate Bill (SB) 743 (Steinberg). SB 743 seeks to reduce greenhouse gas (GHG) emissions overall by promoting integrated land uses that facilitate transportation through many forms, such as bicycle, walking, transit, and carpooling. American Canyon has been in step with SB 743 by approving the Broadway District Specific Plan (BDSP) in 2019. The BDSP furthers the intent of SB743 by promoting integrated land uses that facilitate transportation through many forms, such as bicycle, walking, transit, and carpooling.

SB 743 also requires local jurisdictions to reduce automobile travel by replacing LOS from transportation analysis under CEQA with Vehicle Miles Traveled (“VMT”), or another measure that “promote(s) greenhouse gas emissions reduction, development of multimodal transportation networks, and a diversity of land uses.”

American Canyon has a long history of promoting multimodal transportation networks, and a diversity of land uses. Examples include the Broadway District Specific Plan, bicycle master plan, pedestrian master plan, and many other "smart growth" policies. The next step to comply with SB 743 is to adopt a VMT standard appropriate to the City of American Canyon. Establishing a VMT standard will further accomplish SB 743 by measuring mobility at a “holistic” level, such as the amount and distance people drive, taking the number of passengers within a vehicle into account.

On August 24, 2023, the Planning Commission unanimously recommended City Council approval of the VMT Policy.

What does VMT Mean for Life in American Canyon?

Even though LOS is no longer a significant impact from a CEQA perspective, the City’s General Plan includes a Circulation Map of existing and future roadways. Policies that require roadway construction in balance with land use will remain in place along with the City’s Traffic Impact Fee, conditions of approval for frontage improvements, land dedications, and other programs to help fund roadways, traffic calming, sidewalks, trails and mobility infrastructure.

Proposed VMT Standard

GHD prepared a Technical Memorandum that provides supporting documentation for establishing an appropriate VMT Threshold in the City of American Canyon. A copy of the Technical Memorandum is included as Exhibit A to Attachment 1. The GHD Technical Memorandum uses the framework suggested by the Governor’s Office of Planning and Research (OPR) in its Technical Advisory on Evaluating Transportation Impacts in CEQA.

The City’s Traffic Model calculates the existing rate of residential VMT per capita to be 16.6 miles

and the existing daily rate of VMT per employee is estimated to be 34.1 miles. The VMT policy would require new development to improve the existing citywide VMT rate by 19 percent. The 19 percent citywide standard was selected because it is consistent with the California Air Resources Board (CARB) 19 percent reduction target in per capita greenhouse gas emissions from passenger vehicles by 2035, when compared to 2005 for the nine-county San Francisco Bay Area region. Once adopted, this VMT threshold standard will apply to all General Plan amendments, long-range plans, discretionary development applications, and transportation projects.

Proposed VMT Standard Exemptions

State law allows exemptions to certain development projects that, by their nature, reduce VMT by the citywide 19 percent citywide standard. The VMT policy lists these projects and it is shown below:

- Any project exempt from CEQA.
- Any project entitled under the terms of a Development Agreement currently in effect.
- Any project consistent with a Program EIR for which a VMT analysis has been conducted.
- Any Project located within ½ mile of an existing major transit stop or an existing stop along a high-quality transit corridor (CEQA Guidelines section 15064.3(b)(1)) (OPR Technical Advisory).
- Housing affordable to residents earning up to 120% of the Napa County Area Median Income (AMI) shall be presumed to have a less-than-significant impact. (OPR Technical Advisory)
- A local-serving retail/service or recreational project with a documented trade area up to 6 miles. (OPR Technical Advisory)
- A mixed-use project will have each component independently apply the significance threshold for each project type (residential /retail). An off-site trip reduction may be calculated using the Institute of Transportation Engineer’s (ITE’s) internal capture methodology. (OPR Technical Advisory)

The following screening criteria shall exempt City-initiated Transportation projects from a VMT analysis:

- Projects addressed at a programmatic level, such as in a Regional Transportation Plan EIR, whose impacts have been mitigated to less-than-significant. (CEQA Guidelines Section 15064.3(b))
- Rehabilitation, maintenance, replacement, safety, and repair projects on existing transportation assets (e.g., highways; roadways; bridges; culverts; Transportation Management System field elements such as cameras, message signs, detection, or signals; tunnels; transit systems; and assets that serve bicycle and pedestrian facilities) that do not add additional motor vehicle capacity.
- Roadside safety devices or hardware installation such as median barriers and guardrails.
- Roadway shoulder enhancements to provide “breakdown space,” dedicated space for use only by transit vehicles, to provide bicycle access, or to otherwise improve safety, but which will not be used as automobile vehicle travel lanes.

- Auxiliary lane additions to improve roadway safety if less than one mile in length.
- Traffic lane installation, removal, or reconfiguration not for through traffic, such as left, right, and U-turn pockets, two-way left turn lanes, or emergency breakdown lanes that are not utilized as through lanes.
- Roadway capacity addition on local or collector streets when the project substantially improves pedestrian, cyclist, and, if applicable, transit conditions.
- General-purpose lane (including ramps) conversion to managed lanes or transit lanes, or changing lane management in a manner that would not substantially increase vehicle travel.
- Addition of a new permanently restricted transit vehicle lane.
- Reduction in number of through lanes.
- Grade separation to separate vehicles from rail, transit, pedestrians or bicycles, or to replace a lane in order to separate preferential vehicles (e.g., HOV, HOT, or trucks) from general vehicles.
- Traffic control device installation, removal, or reconfiguration, including Transit Signal Priority (TSP) features.
- Traffic metering system installation, detection systems, cameras, changeable message signs and other electronics designed to optimize vehicle, bicycle, or pedestrian flow.
- Signal timing to optimize vehicle, bicycle, or pedestrian flow.
- Roundabout or traffic circle installation.
- Traffic calming device installation or reconfiguration.
- Adoption of or increase in tolls.
- Addition of tolled lanes, where tolls are sufficient to mitigate VMT increase.
- New transit service initiation.
- Conversion of streets from one-way to two-way operation with no net increase in number of traffic lanes.
- Off-street or on-street parking space removal or relocation.
- On-street parking or loading restrictions (including meters, time limits, accessible spaces, and preferential/reserved parking permit programs) adoption or modification. Traffic wayfinding signage.
- Rehabilitation and maintenance projects that do not add motor vehicle capacity.
- Addition of new or enhanced bike or pedestrian facilities on existing streets/highways or within existing public rights-of-way.
- Addition of Class I bike paths, trails, multi-use paths, or other off-road facilities that serve non-motorized travel.
- Installation of publicly available alternative fuel/charging infrastructure.
- Addition of passing lanes, truck climbing lanes, or truck brake-check lanes in rural areas that do not increase overall vehicle capacity along the corridor.

Amendments to VMT Screening Criteria Exemptions

The VMT CEQA Screening Criteria exemptions listed above are intended to identify most, but not all potential exemptions. To give flexibility to accommodate additional exemptions that are warranted, the VMT policy includes a process to consider development applications and transportation project

exemptions. These are listed below:

Interpretation for General Plan, Long-range Plans, and Discretionary Development Applications:

Interpretation of VMT exemptions for project types not specifically listed above shall be conducted by the Community Development Director or designee, consistent with the process codified in American Canyon Municipal Code Section 19.01.120 Interpretation.

Interpretation for City-Initiated Transportation Projects: Interpretation of VMT exemptions for project types not specifically listed above shall be conducted by the Public Works Director or designee consistent with the process codified in American Canyon Municipal Code Section 19.01.120 Interpretation.

Staff recognizes that the science and policy related to VMT will evolve over time. The proposed VMT policy adds an administrative amendment process when there are changes required by State Law, such as the California Environmental Quality Act or other legislation. These administrative changes would revise the VMT policy to keep it current by incorporated State Law changes into the VMT Screening Exemption Resolution by reference.

COUNCIL PRIORITY PROGRAMS AND PROJECTS

Infrastructure: "Develop and maintain infrastructure resources to support sustainable growth."

FISCAL IMPACT

N/A

ENVIRONMENTAL REVIEW

The VMT Policy is exempt from review under the California Environmental Quality Act ("CEQA") pursuant to Public Resources Code Section 21065 (definition of a CEQA "project"), CEQA Guidelines Section 15064.7 (requirements for adopting thresholds of significance), and CEQA Guidelines Section 15061(b)(3) (commonsense exemption), CEQA Guidelines Section 15307 Actions by Regulatory Agencies for Protection of Natural Resources, and CEQA Guidelines Section 15308 Actions by Regulatory Agencies for Protection of the Environment. CEQA Guidelines Section 15307 and 15308 are applicable because the proposed VMT Policy will fulfill the City's implementation of SB 743 which is intended to address climate change impacts that pose an immediate and growing threat to California's economy, environment, and public health.

ATTACHMENTS:

[1. VMT CC Resolution](#)

[1.1 Tech Memo - Supporting Materials for VMT Thresholds_Draft for City review](#)

RESOLUTION NO. 2023-_____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON, CALIFORNIA APPROVING A CITYWIDE POLICY ESTABLISHING VEHICLE MILES TRAVELED (VMT) AS THE STANDARD OF MEASUREMENT FOR POTENTIAL VEHICLE TRAFFIC IMPACTS CONSISTENT WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

WHEREAS, the California Environmental Quality Act (CEQA) was enacted by the State of California in 1970 to ensure the long-term protection of the environment and requires public agencies to analyze and disclose the effects of their actions on the environment; and

WHEREAS, in spite of more than 50 years of CEQA regulation, climate change caused in part by policies that favor the single occupant automobile represents a growing danger to human health, safety, economic prosperity, basic services, and natural resources; and

WHEREAS, the State of California as a whole, and Napa County residents, the economy, and environment have experienced adverse effects associated with climate change, such as a prolonged wildfire seasons and firestorms, rising temperatures, mudslides, severe droughts, property destruction and damage to infrastructure; and

WHEREAS, American Canyon has a long history of supporting policies to protect the environment. In 2013, American Canyon’s Energy Efficiency Climate Action Plan (EECAP) was the first Climate Action Plan adopted in Napa County; and

WHEREAS, on June 18, 2019, the City Council approved a Countywide Commitment to Address Climate Change Proclamation declaring the City’s support of local actions to address climate change including joining the Napa Countywide Climate Action Committee (CAC); and

WHEREAS, in July 2019, the City approved the Broadway District Specific Plan, which furthers the intent of SB743 by promoting integrated land uses that facilitate transportation through many forms, such as bicycle, walking, transit, and carpooling; and

WHEREAS, on November 16, 2021, the City Council approved a Climate Emergency Proclamation; and

WHEREAS, on February 1, 2022, the City Council approved a Climate Emergency Resolution; and

WHEREAS, public agencies in California have historically attempted to combat traffic congestion by relying on a metric known as “Level of Service” (LOS) standards. Developed in the post-war US specifically for highway travel, the LOS standard assesses the relationship between traffic speed, volume and density, putting a priority on how well automobiles flow through a street network; and

WHEREAS, the LOS standard fails to combat congestion in the long run because it considers all vehicles equally: a single person in a car counts as much as 50 people in a bus, even though 50 people in a single vehicle contribute far less to congestion than 50 people in 50 vehicles. The convenience of a short term free-flowing roadway only encourages more single occupant automobiles. Thus, measuring only the vehicles on a crowded roadway misses the fact that some of those vehicles are causing a real problem; and

WHEREAS, policies, such as LOS that prioritize use of the single occupant automobile result in expensive road improvements and encourage urban sprawl to the detriment of other mobility alternatives, such as walking, biking, and transit; and

WHEREAS, according to the California Air Resources Board (CARB), emphasis on prioritizing single-occupant automobile convenience has resulted in transportation as the leading source of GHG pollution in California of which passenger vehicles represent the largest single source of transportation GHG emissions in California; and

WHEREAS, in 2013, the State of California Legislature passed, and Governor Brown signed Senate Bill (SB) 743 (Steinberg). SB 743 helps reduce greenhouse gas (GHG) emissions overall by promoting integrated land uses that facilitate transportation through many forms, such as bicycle, walking, transit, and carpooling; and

WHEREAS, in furtherance of its intent, Senate Bill 743 directs OPR to produce CEQA guidance for cities to reduce automobile travel by replacing LOS from transportation analysis under CEQA with Vehicle Miles Traveled (“VMT”), or another measure that “promote(s) greenhouse gas emissions reduction, development of multimodal transportation networks, and a diversity of land uses”; and

WHEREAS, the California Office of Planning and Research (OPR) develops CEQA Guidelines to interpret CEQA statutes and published court decisions, including several appendices to the CEQA Guidelines that contain forms and guidance for lead agencies when performing environmental review; and

WHEREAS, public agencies are encouraged to develop standards and procedures to implement CEQA Guidelines, such as replacing LOS from transportation analysis under CEQA with VMT by adopting local CEQA thresholds of significance; and

WHEREAS, California cities, as of July 1, 2020, began implementing the new law on applicable projects.; and

WHEREAS, State law allows lead agencies to set VMT thresholds of significance based either on local or regional per capita averages; and

WHEREAS, effective October 1, 2018, the California Air Resources Board (CARB) target for the nine-county San Francisco Bay Area region is a 19 percent reduction in per capita greenhouse gas emissions from passenger vehicles by 2035, when compared to 2005; and

WHEREAS, when a public agency develops a local threshold of significance, CEQA Guidelines require the threshold of significance be adopted through a public review process and supported by substantial evidence; and

WHEREAS, establishing a VMT standard will further accomplish SB 743 by measuring mobility at a “holistic” level, such as the amount and distance people drive, taking the number of passengers within a vehicle into account; and

WHEREAS, General Plan Goal 1U directs the City to “Conduct decisive near-term action to reduce greenhouse gas emissions in American Canyon”; and

WHEREAS, General Plan Policy 1.37.4 directs the City to “Reduce vehicle miles travelled by encouraging future land uses that feature a compact mixed-use urban form connected with pedestrian and bicycle trails”; and

WHEREAS, adopting a VMT policy will fulfill General Plan goals and policies to reduce GHG emissions and bring the City of American Canyon’s transportation analysis methodology in line with State and City goals pursuant to Senate Bill 743 and the General Plan; and

WHEREAS, CEQA Guidelines Section 15064.7(b) directs the City to adopt thresholds of significance based on substantial evidence by ordinance, resolution, rule or regulation through a public process; and

WHEREAS, on behalf of the City, GHD prepared a Technical Memorandum that provides supporting documentation for appropriate VMT Thresholds in the City of American Canyon; and

WHEREAS, the GHD Technical Memorandum would establish the City follow the framework suggested by the Governor’s Office of Planning and Research (OPR) in its Technical Advisory on Evaluating Transportation Impacts in CEQA; and

WHEREAS, the City’s Traffic Model, as prepared by GHD, calculates the existing rate of residential VMT per capita is estimated to be 16.6 miles and the existing daily rate of VMT per employee is estimated to be 34.1 miles; and

WHEREAS, on August 24, 2023, the City of American Canyon Planning Commission unanimously recommended approval of the VMT policy; and

WHEREAS, on September 5, 2023, the City Council considered the VMT policy, at which time all those in attendance were given the opportunity to speak on this proposal, and all comments were reviewed and considered.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of American Canyon accepts the Technical Memorandum prepared by GHD that provides supporting documentation for appropriate VMT Thresholds in the City of American Canyon. A copy of the Technical Memorandum is incorporated into this Resolution as Exhibit A.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the City Council of the City of American Canyon approves a Senate Bill (SB) 743 California Environmental Quality Act (CEQA) VMT Policy as follows:

SECTION 1: CEQA FINDINGS

The VMT Policy is exempt from review under the California Environmental Quality Act (“CEQA”) pursuant to Public Resources Code Section 21065 (definition of a CEQA “project”), CEQA Guidelines Section 15064.7 (requirements for adopting thresholds of significance), and CEQA Guidelines Section 15061(b)(3) (commonsense exemption), CEQA Guidelines Section 15307 Actions by Regulatory Agencies for Protection of Natural Resources, and CEQA Guidelines Section 15308 Actions by Regulatory Agencies for Protection of the Environment. CEQA Guidelines Section 15307 and 15308 are applicable because the proposed VMT Policy will fulfill the City’s implementation of SB 743 which is intended to address climate change impacts that pose an immediate and growing threat to California’s economy, environment, and public health.

SECTION 2: BASELINE VEHICLE MILES TRAVELLED (VMT)

The VMT for the General Plan, long-range plans, discretionary development applications, and transportation projects shall be evaluated in relation to the City of American Canyon Average VMT which is 16.6 miles per resident and 34.1 miles per employee.

SECTION 3. CEQA VMT THRESHOLDS

The VMT threshold of significance shall be 19% below the Citywide average. Unless exempt as described in this Resolution, this VMT threshold standard shall apply to all General Plan amendments, long-range plans, discretionary development applications, and transportation projects.

SECTION 4. CEQA LAND USE VMT SCREENING CRITERIA EXEMPTIONS

The following screening criteria shall exempt General Plan, long-range plans, and discretionary development applications from a VMT analysis:

TYPE	SCREENING CRITERIA
CEQA Exemption	Any project exempt from CEQA.
Development Agreement	Any project entitled under the terms of a Development Agreement currently in effect.
Program Environmental Impact Report (EIR)	Any project consistent with a Program EIR for which a VMT analysis has been conducted.
Near transit station	Any Project located within ½ mile of an existing major transit stop or an existing stop along a high-quality transit corridor (CEQA Guidelines section 15064.3(b)(1)) (OPR Technical Advisory)
Affordable Housing	Housing affordable to residents earning up to 120% of the Napa County Area Median Income (AMI) shall be presumed to have a less-than-significant impact. (OPR Technical Advisory)
Local-Serving Retail/Service/Recreational Land Use	A local-serving retail/service or recreational project with a documented trade area up to 6 miles. (OPR Technical Advisory)
Mixed-Use Projects	Evaluate each mixed-use component independently and apply the significance threshold for each project type (residential /retail). An off-site trip reduction may be calculated using the Institute of Transportation Engineer’s (ITE’s) internal capture methodology. (OPR Technical Advisory)

SECTION 5. CEQA LAND USE VMT SCREENING CRITERIA EXEMPTIONS

The following screening criteria shall exempt City-initiated Transportation projects from a VMT analysis:

TYPE
<ul style="list-style-type: none">• Projects addressed at a programmatic level, such as in a Regional Transportation Plan EIR, whose impacts have been mitigated to less-than-significant. (CEQA Guidelines Section 15064.3(b))• Rehabilitation, maintenance, replacement, safety, and repair projects on existing transportation assets (e.g., highways; roadways; bridges; culverts; Transportation Management System field elements such as cameras, message signs, detection, or signals; tunnels; transit systems; and assets that serve bicycle and pedestrian facilities) that do not add additional motor vehicle capacity.• Roadside safety devices or hardware installation such as median barriers and guardrails.• Roadway shoulder enhancements to provide “breakdown space,” dedicated space for use only by transit vehicles, to provide bicycle access, or to otherwise improve safety, but which will not be used as automobile vehicle travel lanes.• Auxiliary lane additions to improve roadway safety if less than one mile in length.• Traffic lane installation, removal, or reconfiguration not for through traffic, such as left, right, and U-turn pockets, two-way left turn lanes, or emergency breakdown lanes that are not utilized as through lanes.• Roadway capacity addition on local or collector streets when the project substantially improves pedestrian, cyclist, and, if applicable, transit conditions.• General-purpose lane (including ramps) conversion to managed lanes or transit lanes, or changing lane management in a manner that would not substantially increase vehicle travel.• Addition of a new permanently restricted transit vehicle lane.• Reduction in number of through lanes.• Grade separation to separate vehicles from rail, transit, pedestrians or bicycles, or to replace a lane in order to separate preferential vehicles (e.g., HOV, HOT, or trucks) from general vehicles.• Traffic control device installation, removal, or reconfiguration, including Transit Signal Priority (TSP) features.• Traffic metering system installation, detection systems, cameras, changeable message signs and other electronics designed to optimize vehicle, bicycle, or pedestrian flow.• Signal timing to optimize vehicle, bicycle, or pedestrian flow.• Roundabout or traffic circle installation.• Traffic calming device installation or reconfiguration.• Adoption of or increase in tolls.• Addition of tolled lanes, where tolls are sufficient to mitigate VMT increase.• New transit service initiation.• Conversion of streets from one-way to two-way operation with no net increase in number of traffic lanes.• Off-street or on-street parking space removal or relocation.• On-street parking or loading restrictions (including meters, time limits, accessible spaces, and preferential/reserved parking permit programs) adoption or modification.

TYPE
<ul style="list-style-type: none"> • Traffic wayfinding signage. • Rehabilitation and maintenance projects that do not add motor vehicle capacity. • Addition of new or enhanced bike or pedestrian facilities on existing streets/highways or within existing public rights-of-way. • Addition of Class I bike paths, trails, multi-use paths, or other off-road facilities that serve non-motorized travel. • Installation of publicly available alternative fuel/charging infrastructure. • Addition of passing lanes, truck climbing lanes, or truck brake-check lanes in rural areas that do not increase overall vehicle capacity along the corridor.

SECTION 6. AMENDMENTS TO CEQA LAND USE VMT SCREENING CRITERIA EXEMPTIONS

The VMT CEQA Screening Criteria exemptions listed in Section 4 and 5 of this Resolution are intended to identify most, but not all potential exemptions.

Interpretation for General Plan, Long-range Plans, and Discretionary Development Applications: Interpretation of VMT exemptions for project types not specifically listed above shall be conducted by the Community Development Director or designee, consistent with the process codified in American Canyon Municipal Code Section 19.01.120 Interpretation.

Interpretation for City-Initiated Transportation Projects: Interpretation of VMT exemptions for project types not specifically listed above shall be conducted by the Public Works Director or designee consistent with the process codified in American Canyon Municipal Code Section 19.01.120 Interpretation.

Administrative Modification: Amendments to VMT Screening Criteria exemptions by State Law, including but not limited to the California Environmental Quality Act (Public Resources Code §§21000 et seq.) shall be incorporated into the VMT Screening Exemption Resolution by reference.

SECTION 7. SEVERABILITY. If any section, sentence, clause or phrase of this Resolution is for any reason held to be invalid or unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Resolution. The City Council declares that it would have passed this Resolution and adopted this Resolution and each section, sentence, clause or phrase thereof, irrespective of the fact that one or more sections, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

PASSED, APPROVED and ADOPTED at a regularly scheduled meeting of the City Council of the City of American Canyon held on the 5th day of September, 2023, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

Leon Garcia, Mayor

ATTEST:

APPROVED AS TO FORM:

Taresa Geilfuss, CMC, City Clerk

William D. Ross, City Attorney

EXHIBITS

- A. Technical Memorandum Supporting Documentation for VMT Thresholds Resolution

EXHIBIT A



DRAFT Technical Memorandum

July 18, 2023

To	Brent Cooper, City of American Canyon	Email	bcooper@cityofamericancanyon.org
From	Don Hubbard and Colin Burgett, GHD	Project No.	11207243
Project Name	American Canyon General Plan Update		
Subject	Supporting Documentation for VMT Thresholds Resolution		

1. Introduction

SB 743 changed the way that transportation impacts are to be evaluated under CEQA. The key change was that vehicle delay, as measured using vehicular level-of-service (LOS), is no longer considered an impact under CEQA. It has been replaced as a metric with vehicle-miles traveled (VMT), with increases in VMT being considered a significant impact under CEQA.

The change in how transportation impacts are to be assessed has triggered a need for the City to develop thresholds of significance compatible with the new system. The consulting team has worked with City staff to develop these thresholds. The purpose of this memo is to document the key elements of the thresholds.

2. State Reduction Targets

SB 375 assigned the task of setting regional targets for greenhouse gas (GHG) emission reductions from passenger vehicles to the California Air Resources Board (CARB). These targets are based on extensive analysis by CARB on what actions are needed for different sectors (transportation, energy generation, building energy use, etc.) for the state to reach its GHG reduction goals. CARB has set a target reduction for GHGs from passenger vehicles in the MTC/ABAG region at a 19% reduction by 2035¹. Based on the substantial evidence provided by CARB, we can assume that if future developments in American Canyon generate 19% fewer VMT/unit than the corresponding existing units, then those developments will have a less-than-significant VMT impact.

3. Analysis of VMT Impacts of Residential, Office and Industrial Developments

The choice of which baseline to use for VMT evaluation is left to the lead agency. The VMT generated by new projects could, for example, be compared to the existing regional average, or the existing county-wide average, or the existing local (city-wide) average. In this case we recommend that new

¹ See: <https://ww2.arb.ca.gov/our-work/programs/sustainable-communities-program/regional-plan-targets>

developments in American Canyon be compared to existing developments in American Canyon. The recommendation to use a local average is based on the fact that the community character and physical characteristics of American Canyon are so different from the most populous parts of the MTC region (San Jose, San Francisco, Oakland, etc.) that using a regional average dominated by the largest jurisdictions would not be an apples-to-apples comparison. Moreover, so long as each jurisdiction in the region makes a 19% reduction from its existing uses, the aggregate result for the region as a whole would meet the State's goals. Regarding the development types being evaluated, we recommend that the City follow the framework suggested by the Governor's Office of Planning and Research (OPR) in its Technical Advisory on Evaluating Transportation Impacts in CEQA²:

- Residential developments should be evaluated using VMT/capita as the metric. This includes trips of all types where one end of the trip is the traveler's residence (work commute trips, shopping trips, school trips, etc.).
- Office and industrial developments should be evaluated using VMT/employee as the metric. This VMT is for the employees' own trips to work, and does not include trips made by other people to the employee's worksite.

GHD utilized the American Canyon Travel Demand Model (ACTDM), incorporating U.S. Census data relevant to work commute patterns, and regional trip length data relevant to VMT attributable to trips outside the model area, to estimate the average existing rates of residential VMT per Capita, and non-residential VMT per Employee, and the associated thresholds:

Residential VMT (attributable to homes in American Canyon):

Residential VMT	/ Population	=	Existing VMT/Capita	*	19% reduction	=	Threshold VMT/Capita
381,468	22,959		16.6		81%		13.5

The residential VMT is the sum of the VMT attributable to all home-based trip productions to/from dwelling units located in American Canyon, incorporating the distance for each trip from the distance matrix in the model. This includes VMT taking place outside the area of the model's traffic analysis zones. These outer areas are represented with external links representing the average distance trips to/from American Canyon make in their respective directions.

Employment VMT (attributable to work trips to/from jobs in American Canyon):

Employment VMT	/ Jobs	=	Existing VMT/Job	*	19% reduction	=	Threshold VMT/job
151,916	4,442		34.2		81%		27.7

The employment VMT is the sum of all home-based work trips to and from places of employment in American Canyon. This includes VMT taking place outside the area of the model's traffic analysis zones (trips to and from homes located in other jurisdictions outside of the City and model area). These outer areas are represented with external links representing the average distance trips from the outer areas to work sites in American Canyon.

4. Analysis of VMT Impacts of Retail & Hotel Developments

Retail, hotel and service developments need to be handled somewhat differently for other types of non-residential land uses, such as office or industrial buildings. This is because they attract trips from customers, patrons, or visitors in addition to commute trips made by their own employees. These two components of the traffic generated by retail and service uses need to be handled separately:

² See: https://opr.ca.gov/docs/20190122-743_Technical_Advisory.pdf

- ***VMT attributable to retail, hotel and service employees should be evaluated in the same manner as for office or industrial uses.*** That is, the average automobile commute lengths should be compared to the target reduction from the city-wide average rate of VMT/job.
- ***VMT attributable to retail customers should be assessed based on the anticipated net change in total regional VMT attributable to customer trips to/from the proposed development.*** OPR recommends that the impact of visitor/customer trips for retail uses be assessed based on whether overall VMT for the region increases or decreases as a result of the development. For local-serving retail and service developments, a presumption may be made that visitor trips to these uses will be short trips that substitute for longer trips to a more distant store of a similar type. The International Council of Shopping Centers has developed a classification system for retail developments of various sorts³ and performed research into the trade area size of each (see Table 1). Based on this information, we have determined that Community Centers, Neighborhood Centers, and Strip/Convenience Centers are local-serving and can be presumed to have less-than-significant VMT impacts. Other types of retail developments would require quantitative assessment of their trade area to determine their VMT impacts.
- ***VMT attributable to hotel guests should be assessed in a similar manner as retail customers, based on whether total VMT for the region would increase as a result of guest VMT attributable to the hotel development.*** Most hotels provide lodging for guests that, in the absence of a proposed new hotel, would simply stay at another hotel or lodging facility in the area. The assessment of VMT attributable to hotel guests should therefore be based on the net effect of the proposed hotel location, compared to the VMT that would be generated by those guests if they stayed at other lodging options in the area..

5. Analysis of VMT Impacts of Transportation Projects

Transportation projects serve trips but do not generate them the way that land development projects do. They may, however, increase VMT by inducing more or longer vehicle trips than would have taken place if the project were not constructed. OPR recommends that the VMT impacts of a transportation project should be considered significant if the project results in a net increase in regional VMT. This can be determined using the City's traffic model. However, model analysis are not needed for certain types of projects that OPR has determined are not likely to lead to a measurable and substantial effect on VMT⁴. These include:

- Projects addressed at a programmatic level, such as in a Regional Transportation Plan EIR, whose impacts have been mitigated to less-than-significant. (CEQA Guidelines Section 15064.3(b))
- Rehabilitation, maintenance, replacement, safety, and repair projects on existing transportation assets (e.g., highways; roadways; bridges; culverts; Transportation Management System field elements such as cameras, message signs, detection, or signals; tunnels; transit systems; and assets that serve bicycle and pedestrian facilities) that do not add additional motor vehicle capacity.
- Roadside safety devices or hardware installation such as median barriers and guardrails.
- Roadway shoulder enhancements to provide "breakdown space," dedicated space for use only by transit vehicles, to provide bicycle access, or to otherwise improve safety, but which will not be used as automobile vehicle travel lanes.

³ See: https://www.icsc.com/uploads/research/general/US_CENTER_CLASSIFICATION.pdf

⁴ See: https://opr.ca.gov/docs/20190122-743_Technical_Advisory.pdf

- Auxiliary lane additions to improve roadway safety if less than one mile in length.
- Traffic lane installation, removal, or reconfiguration not for through traffic, such as left, right, and U-turn pockets, two-way left turn lanes, or emergency breakdown lanes that are not utilized as through lanes.
- Roadway capacity addition on local or collector streets when the project substantially improves pedestrian, cyclist, and, if applicable, transit conditions.
- General-purpose lane (including ramps) conversion to managed lanes or transit lanes, or changing lane management in a manner that would not substantially increase vehicle travel.
- Addition of a new permanently restricted transit vehicle lane.
- Reduction in number of through lanes.
- Grade separation to separate vehicles from rail, transit, pedestrians or bicycles, or to replace a lane in order to separate preferential vehicles (e.g., HOV, HOT, or trucks) from general vehicles.
- Traffic control device installation, removal, or reconfiguration, including Transit Signal Priority (TSP) features.
- Traffic metering system installation, detection systems, cameras, changeable message signs and other electronics designed to optimize vehicle, bicycle, or pedestrian flow.
- Signal timing to optimize vehicle, bicycle, or pedestrian flow.
- Roundabout or traffic circle installation.
- Traffic calming device installation or reconfiguration.
- Adoption of or increase in tolls.
- Addition of tolled lanes, where tolls are sufficient to mitigate VMT increase.
- New transit service initiation.
- Conversion of streets from one-way to two-way operation with no net increase in number of traffic lanes.
- Off-street or on-street parking space removal or relocation.
- On-street parking or loading restrictions (including meters, time limits, accessible spaces, and preferential/reserved parking permit programs) adoption or modification.
- Traffic wayfinding signage.
- Rehabilitation and maintenance projects that do not add motor vehicle capacity.
- Addition of new or enhanced bike or pedestrian facilities on existing streets/highways or within existing public rights-of-way.
- Addition of Class I bike paths, trails, multi-use paths, or other off-road facilities that serve non-motorized travel.
- Installation of publicly available alternative fuel/charging infrastructure.
- Addition of passing lanes, truck climbing lanes, or truck brake-check lanes in rural areas that do not increase overall vehicle capacity along the corridor.

Table 1: U.S. Shopping Center Classifications and Trade Area Size

U.S. Shopping-Center Classification and Characteristics												
Type of Shopping Center	Concept	Center Count	Aggregate GLA (Sq. Ft.)	% Share of Industry GLA	Average Size (Sq. Ft.)	Typical GLA Range (Sq. Ft.)	Acres	# of Anchors	% Anchor GLA	Typical Number of Tenants	Typical Type of Anchors	Trade Area Size
General-Purpose Centers		112,520										
Super-Regional Mall	Similar in concept to regional malls, but offering more variety and assortment.	620	778,336,548	10.2%	1,255,382	800,000+	60-120	3+	50-70%	NA	Full-line or junior department store, mass merchant, discount department store and/or fashion apparel store.	5-25 miles
Regional Mall	General merchandise or fashion-oriented offerings. Typically, enclosed with inward-facing stores connected by a common walkway. Parking surrounds the outside perimeter.	600	353,795,548	4.7%	589,659	400,000-800,000	40-100	2+	50-70%	40-80 stores	Full-line or junior department store, mass merchant, discount department store and/or fashion apparel store.	5-15 miles
Community Center ("Large Neighborhood Center")	General merchandise or convenience-oriented offerings. Wider range of apparel and other soft goods offerings than neighborhood centers. The center is usually configured in a straight line as a strip, or may be laid out in an L or U shape, depending on the site and design.	9,776	1,930,849,736	25.4%	197,509	125,000-400,000	10-40	2+	40-60%	15-40 stores	Discount store, supermarket, drug, large-specialty discount (toys, books, electronics, home improvement/furnishings or sporting goods, etc.)	3-6 miles
Neighborhood Center	Convenience oriented.	32,588	2,340,711,371	30.8%	71,827	30,000-125,000	3-5	1+	30-50%	5-20 stores	Supermarket	3 miles
Strip/Convenience	Attached row of stores or service outlets managed as a coherent retail entity, with on-site parking usually located in front of the stores. Open canopies may connect the store fronts, but a strip center does not have enclosed walkways linking the stores. A strip center may be configured in a straight line, or have an "L" or "U" shape. A convenience center is among the smallest of the centers, whose tenants provide a narrow mix of goods and personal services to a very limited trade area.	68,936	911,202,922	12.0%	13,218	< 30,000	<3	Anchor-less or a small convenience-store anchor.	NA	NA	Convenience store, such as a mini-mart.	<1 mile
Specialized-Purpose Centers		3,275										
Power Center	Category-dominant anchors, including discount department stores, off-price stores, wholesale clubs, with only a few small tenants.	2,258	990,416,667	13.0%	438,626	250,000-600,000	25-80	3+	70-90%	NA	Category killers, such as home improvement, discount department, warehouse club and off-price stores	5-10 miles
Lifestyle	Upscale national-chain specialty stores with dining and entertainment in an outdoor setting.	491	164,903,247	2.2%	335,852	150,000-500,000	10-40	0-2	0-50%	NA	Large format upscale specialty	8-12 miles
Factory Outlet	Manufacturers' and retailers' outlet stores selling brand-name goods at a discount.	367	87,368,113	1.2%	238,060	50,000-400,000	10-50	NA	NA	NA	Manufacturers' and retailers' outlets	25-75 miles
Theme/Festival	Leisure, tourist, retail and service-oriented offerings with entertainment as a unifying theme. Often located in urban areas, they may be adapted from older--sometimes historic--buildings and can be part of a mixed-use project.	159	23,498,769	0.3%	147,791	80,000-250,000	5-20	Unspecified	NA	NA	Restaurants, entertainment	25-75 miles
Limited-Purpose Property		62										
Airport Retail	Consolidation of retail stores located within a commercial airport	62	15,452,860	0.2%	249,240	75,000-300,000	NA	NA	NA	NA	No anchors; retail includes specialty retail and restaurants	NA
Total Industry		115,857										
Total Industry	Traditional + Specialty + Special Purpose	115,857	7,596,535,781	100.0%	65,568							

Sources: ICSC Research and CoStar Realty Information, Inc. (www.costar.com)



TITLE

Support for ACA 13 (Ward)

RECOMMENDATION

Adopt a Minute Order authoring the Mayor to sign a letter of support for ACA 13 (Ward)

CONTACT

Jason Holley, City Manager

BACKGROUND & ANALYSIS

The California Constitution provides a variety of mechanisms for updates/revision (ie. "Constitutional Amendment"). Since the Constitution was originally adopted in 1849, it has been amended more than 500 times.

One method for amending the Constitution - commonly known as "direct democracy" - provides that any registered voter may propose a Constitutional Amendment, and after having gather a sufficient number of signatures from other register voters, may take that initiative directly to the voters by placing in on a statewide ballot. It is know as 'direct democracy' because it the initiative bypasses the Legislature (and Governor) to become law (once approved by the voters). One of the most famous examples of this process includes Prop. 13, which limits local governments ability to generate revenue to pay for services.

Another method for for amending the Constitution provides that the Legislature can also pass legislation to place an issue on the statewide ballot. ACA 13 (Ward) is a proposed constitutional amendment currently pending in the Legislature. If ACA 13 passes, then the measure would appear on the Presidential Primary Election ballot in March 5, 2024.

The Constitution provides for a variety of voting thresholds for a measure to pass - often the content of the matter determines the requisite threshold. Some measure only require a "simple majority" (ie. "50% +1") to pass. Others require a "super majority" (66.7%). Obviously, it is easier to pass a measure if only a simple majority is required. Conversely, it is easier to defeat a measure if a super majority is required. Thus, if a registered voter wanted to increase their chances of defeating certain types of measures in the future, they would seek to increase the voting threshold of those types of measures.

Currently, Constitutional Amendment Initiatives only require a simple majority to pass. Ironically, an initiative to increase the voting threshold from simple majority to super majority for new taxes only requires a simple majority to pass. Said differently, currently only 50% of the voters need to agree that 67% of the voters are required to approve certain new taxes. If these types of measures pass, disproportionate power is afforded a small number of voters, because only 1/3 of voters can obstruct the enactment of laws that a majority of voters agree upon.

In fact, local revenue measures dedicated to funding critical services such as affordable housing, fire, public safety, and roads have been less successful than other measures. For example, from 2001-2018, local measures with a 2/3 vote requirement have only passed 51% of the time, compared to a 70% passage rate of all local revenue measures over the same period of time.

ACA 13 would affect future constitutional amendments proposed by initiative. Specifically, any future voter initiative that proposes to increase a voter thresholds (ie. from 50% to 67%) would itself be required to be approved by the same proportion of votes cast as the measure would require. Said differently, if ACA 13 is placed on the ballot and ultimately passed by the voter, then for example, any new initiative that proposes to increase voter thresholds from 50% to 67% would need to be passed by 67% of the voters to become law in the first place.

CalCities supports ACA 13 and staff recommends the Council authorize the Mayor to sign a letter indicating the City's support for ACA 13 (Attachment 1).

COUNCIL PRIORITY PROGRAMS AND PROJECTS

Organizational Effectiveness: "Deliver exemplary government services."

FISCAL IMPACT

Not applicable

ENVIRONMENTAL REVIEW

15378(b) - The action is not a "Project" subject to the California Environmental Quality Act ("CEQA") because it does not qualify as a "Project" under Public Resources Code Sections 21065 and 21080 and in Section 15378(b) of Title 14 of the California Code of Regulations.

ATTACHMENTS:

1) [ACA 13 Letter of Support](#)

September 5, 2023

The Honorable Gail Pellerin
Chair, Assembly Elections Committee
1020 N Street, Room 365
Sacramento, CA 95814

RE: ACA 13 (Ward) Voting Thresholds
Notice of SUPPORT *(As Amended 8/17/2023)*

Dear Assembly Member Pellerin:

The City of American Canyon supports ACA 13 (Ward), which would require any constitutional amendment proposed by initiative, which increases a vote requirement for future measures, be approved by the same proportion of votes cast as the measure would require.

Under current law a simple majority of voters (50% plus 1 vote) may vote to require a super majority (for example, 66 and 2/3%) voter approval for future action. This framework has given disproportionate power to a small number of voters, with a little more than 1/3 of the voters being able to prevent the enactment of laws that a majority of voters agree upon.

In fact, at the local level this has made it unnecessarily difficult to pass revenue measures dedicated to critical services such as homelessness and housing, fire, public safety, and roads. For example, from 2001-2018, city revenue measures with a 2/3 vote requirement have enjoyed only a 51% passage rate, compared to a 70% passage rate of all local revenue measures over the same period of time. The status quo has prevented cities from providing desperately needed services.

ACA 13 addresses this issue by simply requiring any initiative that enhances a voter requirement to pass by that same voter requirement. ACA 13 is good government, fair proposal that protects the principles of democracy and majority rule.

For these reasons, the City of American Canyon supports ACA 13.

Sincerely,

Leon Garcia, Mayor

cc. Your Senator & Assembly Member
Your League Regional Public Affairs Manager (via email)
Meg Desmond, League of California Cities, cityletters@cacities.org



Public Works Department

City Council Update

September 5, 2023

Streets and Storm Drain Operations Highlights

- New street name sign replacement program was started. 237 street name signs have been replaced between May 23, 2023, and August 4, 2023.
- Two new Radar Speed Signs were installed. The first sign was a replacement for the existing sign on west bound Benton Way. The second sign was installed in a new location on north bound Newell Dr. in front of the American Canyon High School.
- Restriping of 300 Napa Junction Rd parking lot.
- Patch paving on Green Island Road.
- Storm drain inlet inspections and cleaning started in August.



Radar Speed Signs

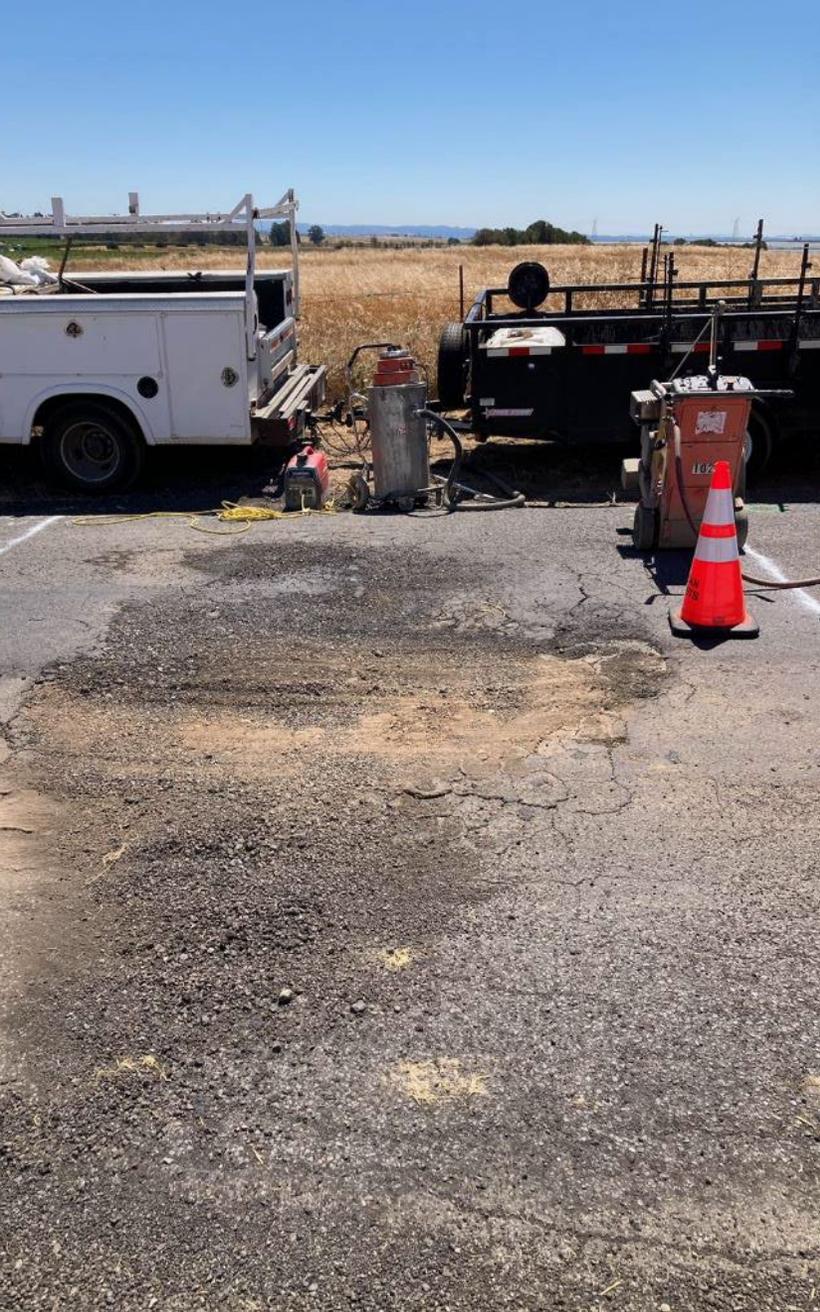


Staff installed two new Radar Speed Signs. A replacement for the existing sign was installed on Benton Way in front of the Aquatics Center. The second sign was installed on Newell Dr. in front of the American Canyon High School.



300 Napa Junction Road

Street staff restriped the
Parking Lot at 300 Napa
Junction Rd.



Green Island Road Patch Paving

Streets Maintenance Staff patch paved approximately 280 sq. ft. of asphalt on Green Island Rd at the City limits.



Capital Projects in Construction



**RECYCLED WATER EXPANSION
PROJECT**



WETLANDS EDGE COURT



**GREEN ISLAND ROAD
UNDERGROUNDING**



**2022 ANNUAL PAVEMENT
PROJECT**



**PLAYGROUND REPLACEMENTS
(COORDINATION WITH PARKS)**



**SKATEPARK RELOCATION
(COORDINATION WITH PARKS)**

Recycled Water System Expansion

Project Manager: Ron Ranada

The Project consists of approximately 9,162 LF total of new recycled water mains and includes the conversion of existing services from the domestic water system to the new recycled water system. Project scope was reduced with assignment of Lombard-Hess to Oat Hill development project.

Schedule

- ✓ Contract award: June 21, 2022
- ☐ Construction Completion: November 30, 2023

Change Orders/Risks

- Claims
- Underground conflicts
- Weather delays and impacts

Funding Sources

- Local – Zero Water Footprint and Interfund Loan Water Ops
- Federal – Napa County: ARPA, and North Bay Water Reuse Authority: Bureau of Reclamation



Wetlands Edge Court Extension

Project Manager: Kristine Delos Santos

The Wetlands Edge Court Extension project will improve storm drainage flow and provide for a paved cul-de-sac north of Eucalyptus Drive.

Schedule

- ✓ Contract award: October 4, 2022
- ☐ NOC: Sept. 19, 2023

Change Orders/Risks

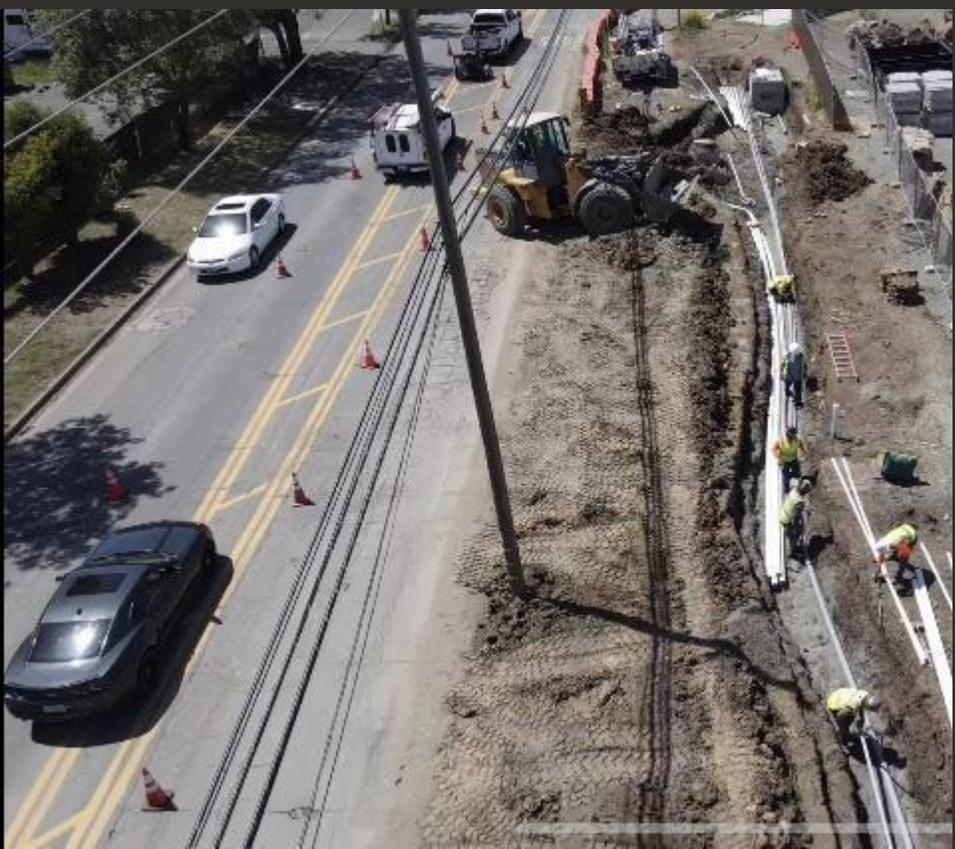
- Underground conflicts
- Weather delays and impacts

Funding Sources

- Local – Measure A
- State – Gas Tax



Green Island Road Undergrounding



Project Manager: Ron Ranada

The project will include relocating approximately 4,500 feet of overhead utility lines and poles to a new underground system known as a “joint trench” on Green Island Road from Paoli Loop to Commerce Boulevard.

Status

- Joint trench work substantially complete
- Project currently suspended pending two service conversions

Change Orders/Risks

- Underground conflicts
- Weather Delays and impacts
- Availability for AT&T to remove poles

Schedule

- ✓ Construction Contract award - November 15, 2022
- ☐ Estimated completion - October 2023

Green Island Road Widening and Reconstruction



Project Manager: Ron Ranada

Following the undergrounding of Green Island Road, approximately 11,900 feet of existing roadways in the Green Island Industrial District will be reconstructed. Reconstruction of Green Island Road will include widening and the installation of approximately 4,200 feet of Class I facilities, including a portion of the Napa Valley Vine Trail.

Schedule

- Advertise: March 2024
- Contract award: June 2024

Funding Sources

- Local – Green Island Road Community Facilities District
- Federal – Economic Development Administration, and One Bay Area Grant 3



Project Manager: Alexandra Ikeda

Skatepark Relocation

The Project consists of relocating the existing skatepark from Benton Way to the Veterans Park.

Schedule

- ✓ Contract award: February 7, 2023
- ✓ Demolition Completed: August 15, 2023
- Begin Construction: October 2, 2023
- Construction Completion: December 31, 2023

Change Orders/Risks

- Claims
- Underground conflicts
- Weather delays and impacts

Funding Sources

- Local – Park Impact Fees
- Federal – ARPA

Playground Replacement

Project Manager: Alexandra Ikeda

The Project consists of replacing playground equipment at Linwood Park, Via Bellagio Park, and Northampton Park. Northampton will include all-inclusive play equipment.

Schedule

- ✓ Contract award: November 15, 2022
- ✓ Construction Completed at Via Bellagio Park and Northampton Park: August 2023
- Construction Completion of Linwood Park: October 31, 2023

Change Orders/Risks

- Playground Fabrication Delays/Delivery
- Weather delays and impacts

Funding Sources

- Local – LLAD for Via Bellagio Park and Park Impact Fees
- State – 2018 Parks Bond Act Per Capital Grant



2022 Annual Pavement Management Project

Project Manager: Ron Ranada

The project will rehabilitate the pavement on Donaldson Way from Eucalyptus to Rio Del Mar, Elke Drive, Gisela Drive, Rita Court, Jana Way, Rebecca Court, West Carolyn Drive from Spikerush to Donaldson, Donaldson Way from Elliott to Amarillo, Donaldson Way from Elliott to Carolyn Drive. Work will include ADA curb ramp upgrades, rubberized cape seal, new striping, bike facilities, and enhanced crosswalk striping.

Cost

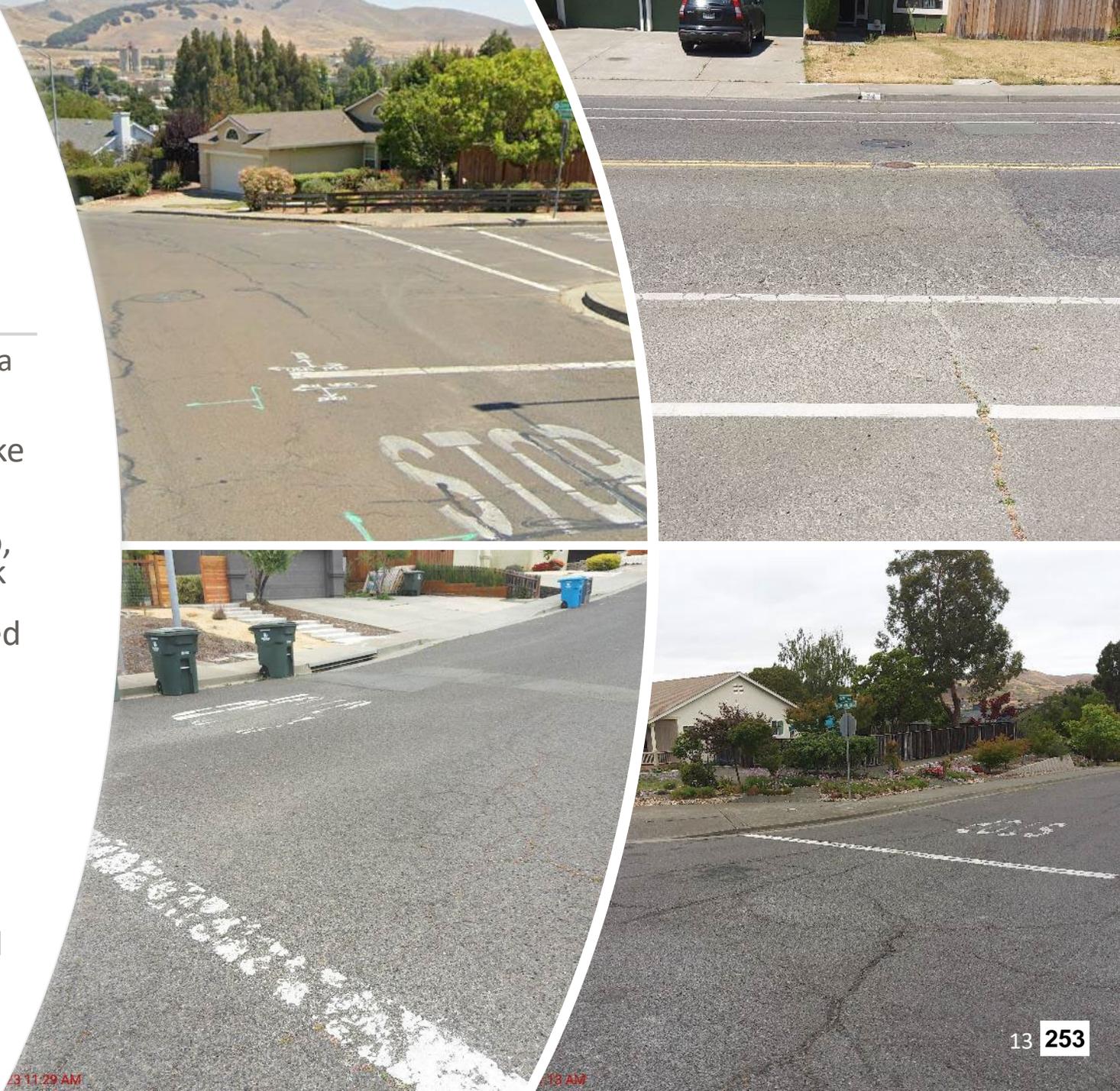
- Pre-construction: \$58,000
- Construction: \$1,814,000

Schedule

- ✓ Contract award: August 2023
- ☐ Begin Construction: September 2023

Funding Source

- Local – Measure T, Water Operations, and Wastewater Operations
- State – RMRA/SB1



Capital Projects in Design

**Newell Trail Open
Space/Parking**

**Benton Way Paving
and Striping**

**Wetlands Restoration
Plan**

**ARPA Melvin Road
Sidewalk**

**Broadway Sewer
Rehabilitation**

**Stormwater Master
Plan**

**Wetlands Edge
Enhancement**

**ARPA Knightsbridge
Reconstruction**

**ARPA Rancho Del Mar
Area**

Wetlands Restoration Plan

Project Manager: Erica Ahmann Smithies

The purpose of the project is to develop a Wetlands Restoration Plan to guide restoration, protection, and improvement of the North Slough and Lower Napa River wetlands and uplands along the City's shoreline. In order to establish a baseline for the Wetland Restoration and Monitoring Plans, we need to conduct three feasibility studies: The North Slough Levee Flood Protection and Habitat Restoration Study, The Corp Yard Wastewater Overflow Pond and Environmental Education Facilities Opportunities Study, and The Kayak Launch Recreational Facility Study.

Schedule

- ✓ Three Feasibility Studies: August 31, 2023
- ❑ Draft Work Plan: September 30, 2023
- ❑ Final Plan: December 31, 2023
- ❑ Grant Completion: May 1, 2024

Funding Source

- State – Measure AA



Benton Way Rehabilitation and Enhanced Crosswalk

Project Manager: Ron Ranada

The project will rehabilitate the pavement on Benton Way from Wetlands Edge to Elliot Drive. Work will include ADA curb ramp upgrades, pavement reconstruction, new striping, enhanced crosswalk striping, and crosswalk improvements at the intersection of Chaucer and Benton Way.

Cost

- Pre-construction: \$102,100
- Construction: \$1,332,500

Schedule

- Complete Design: December 2023
- Advertise: February 2024
- Begin Construction: June 2024

Funding Source

- Local – Measure T
- State – RMRA/SB1
- Federal – ARPA Fund





Melvin Road Sidewalk and Drainage Improvements

Project Manager: Ginny Leija

The project will close sidewalk gaps and provide for drainage improvements between Melvin from Cassayre to James Road as well as close a couple of short sidewalk gaps on James Road to Donaldson.

Cost

- Pre-construction: \$150,000
- Construction: \$900,000

Schedule

- Complete Design: December 2023
- Advertise: January 2024
- Begin Construction: March 2024

Funding Source

- Local – Measure A
- Federal – ARPA

Newell Trail Open Space

Project Manager: Ron Ranada

The project will provide ADA accessible improvements to the Newell Open Space parking lot and improved trail access up to the fenced woodland area. Project scope includes design, right of way acquisition and construction.

Cost

- Pre-construction: \$340,000
- Construction: \$525,000

Tentative Timeline

- Property Acquisition: December 2023
- Advertise: March 2024
- Construction: June 2024

Funding Source

- Local – General Fund and Park Impact Fees
- State – Habitat Conservation Grant



Stormwater Master Plan

Project Manager: Ron Ranada

The Stormwater Master Plan will identify system deficiencies and potential solutions to address them. The plan will also provide for a long-term capital improvement program and potential drainage impact fee on new private development.

Cost

- Design: \$196,397

Milestones

- Draft Model Results: September 2023
- Draft CIP List: November 2023
- Draft Plan: March 2024
- Final Plan: May 2024

Funding Source

- Local – Measure A





Wetlands Edge Enhancement

Project Manager: Parks/Public Works

This project will implement an outdoor educational area (open air seating area), landscape area and bathroom.

Cost

- Construction: \$250,000

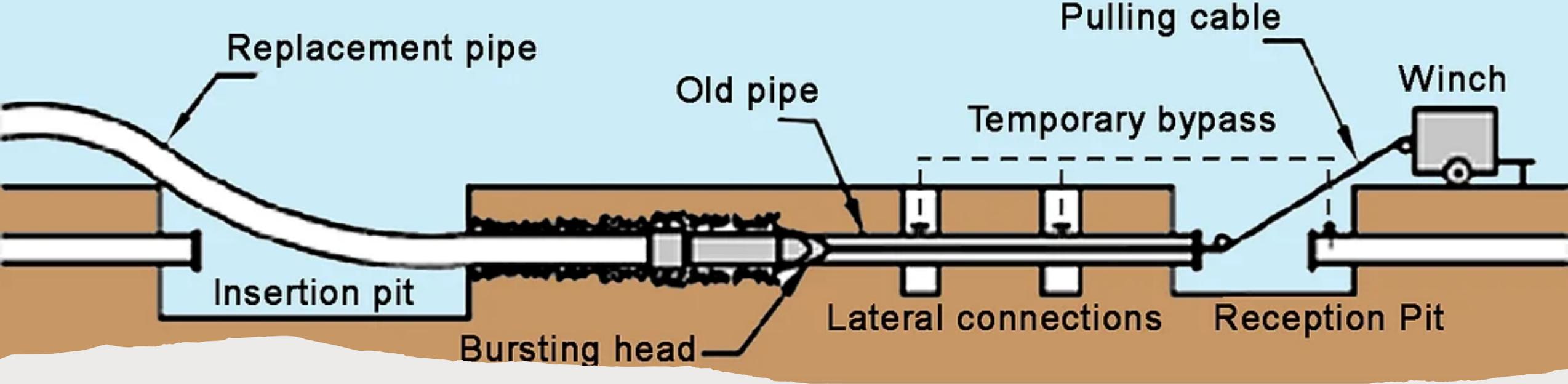
Milestones

- Advertise: December 2023
- Construction Award: March 2024
- Completion: September 2024

Funding Source

- Local – General Fund
- State – Educational Facilities Grant





Project Manager: Ron Ranada

This project will upsize the existing sewer main on Broadway between Crawford and Donaldson through a trenchless method.

Cost

- Design: \$50,000
- Construction: \$950,000

Schedule

- Complete Design: January 2024
- Advertise: February 2024
- Begin Construction: June 2024

Funding Source

- Local – Wastewater Capacity Fees

SS4 Broadway Sewer Rehabilitation

Knightsbridge Rehabilitation and Water Main Improvements

Project Manager: Ginny Leija

The project will replace the 8" water main on Knightsbridge between Danrose and Elliott including all the courts: Regent Court, Park Lane, Lansford Court, Arden Court, Linwood Lane. Work will include ADA ramp upgrades, pavement treatments on Knightsbridge and the courts, striping and crosswalk improvements.

Cost

- Design: \$210,000
- Construction: \$1,640,000

Milestones

- Complete Design: January 2024
- Advertise: February 2024
- Begin Construction: May 2024

Funding Source

- Local – Water Operations
- Federal – ARPA

Rancho Del Mar Paving and Utility Improvements

Project Manager: Ginny Leija

The project includes Rio Del Mar from Highway 29 to Carolyn Drive, all of Los Altos Place east of Carolyn Drive, Cassayre Drive, Flamingo Court, Del Rey Court, Alta Loma Drive, Joan Drive from Carolyn Drive to Los Altos Place, and Carolyn Drive from Rio Del Mar to Los Altos Place. Work includes ADA curb ramp upgrades, various pavement treatments, sewer CIPP and water main upgrades, striping and pavement marking upgrades.

Cost

- Design: \$590,000
- Construction: \$5,300,000

Milestones

- Complete Design: March 2024
- Advertise: May 2024
- Begin Construction: July 2024

Funding Source

- Local – Measure T, Water Operations, and Wastewater Operations
- Federal – ARPA





Commercial Development in 2023



Canyon Estates

Project Manager: Edison Bisnar

The project is for the development of 35 lots custom estate homes. Work includes rough grading of lots. New onsite private roads, and utilities. Work also includes public improvements comprising of new sidewalk with landscaping on the east side of Newell Drive from Donaldson Way intersection up to the property line under the PG&E tower, improvement of the City's access road to the recycled water tank, installation of flashing beacons at crosswalks, and relocation of the City's recycled water and potable water pipelines.

Milestones

- ✓ Begin Construction: March 2021
- ✓ On-site work completion: December 2022
- ✓ PG&E Connection: June 2023





Watson Ranch



Watson Ranch Backbone

Project Manager: Edison Bisnar

The Watson Ranch Project is anticipated to be completed in four Phases. Phase 1 includes new public streets comprising of Rio Del Mar East, extension of Rolling Hills Drive, Mallen Way connecting to the northern terminus of Newell Drive, Marcus Road and installing public utilities to serve all the developments within Watson Ranch. Also include in this phase are offsite utilities.

Milestones

- ✓ Begin Construction: July 2021
- ☐ Work in progress





Lemos Pointe

186-unit affordable apartments

Began Construction: July 2021

Occupancy Planned: December 2023



Artisan Subdivision (Lots 14&15)

98 Lot Subdivision

Began Construction: July 2021

**Work in Progress: 40 Homes
Occupied**



Harvest Subdivision (Lot 10)

219 Lot Subdivision

Began Construction: July 2021

Work in Progress: 25 Homes
Occupied





Napa Cove Apartments

66 affordable dwelling units
distributed over 3-two story and
3-story buildings at Melvin Road
next to the tennis court.

Began Construction: April 4, 2022

Work in Progress



Home2Suites

Four story 102-rooms hotel with an outdoor pool and spa, along Broadway at the former strawberry fields.

Began Construction: Oct. 13, 2022

Work in Progress





PG&E Regional Service Center

Project is for the development of 98,000 square foot Regional Service Center at Boone Drive in Napa Logistics Park. The facility comprise of 4 buildings, laydown yard. Storage and parking lot.

Began Construction: Oct. 13, 2022

Work in Progress



Copart

Project is for an automotive storage facility at 1660 Green Island Road.

Began Construction: February 7, 2022

Work in Progress



Oat Hill

Project for is for the development of 291-unit apartment within two parcels at Oat Hill along Napa Junction Road west. Amenities includes parking, community building, swimming pool and a public trail. The project will also undertake public improvements including, extending approximately 1,300 lineal feet of a new 8-inch recycled water main from end of Lombard Road to Little League Fields, replace approximately 1,800 lineal feet of the existing 6-inch sanitary sewer mains with a new 10-inch between Napa Junction Road, top of the hill, to Theresa Ave. across the senior housing, replace approximately 590 lineal feet of the existing 6-inch water main with a new 12-inch between Napa Junction at Hess Drive and Theresa Ave., and extend Napa Junction Road from the frontage of the old Napa Junction School to Hess Drive intersection.

Began Construction: December 19, 2022

Work in Progress: Mass grading





SDG 217

Project is for a new 217,294 square foot wine Warehouse on a 10.39 Acre site at 1075 Commerce Court. This project will extend the bike trail at end of Commerce Court to Eucalyptus and Wetlands Edge intersection.

Began Construction: April 24, 2023

Work in Progress

WE SAFE STREETS



CityofAmericanCanyon.org/SafeStreets



CITY OF
AMERICAN
CANYON

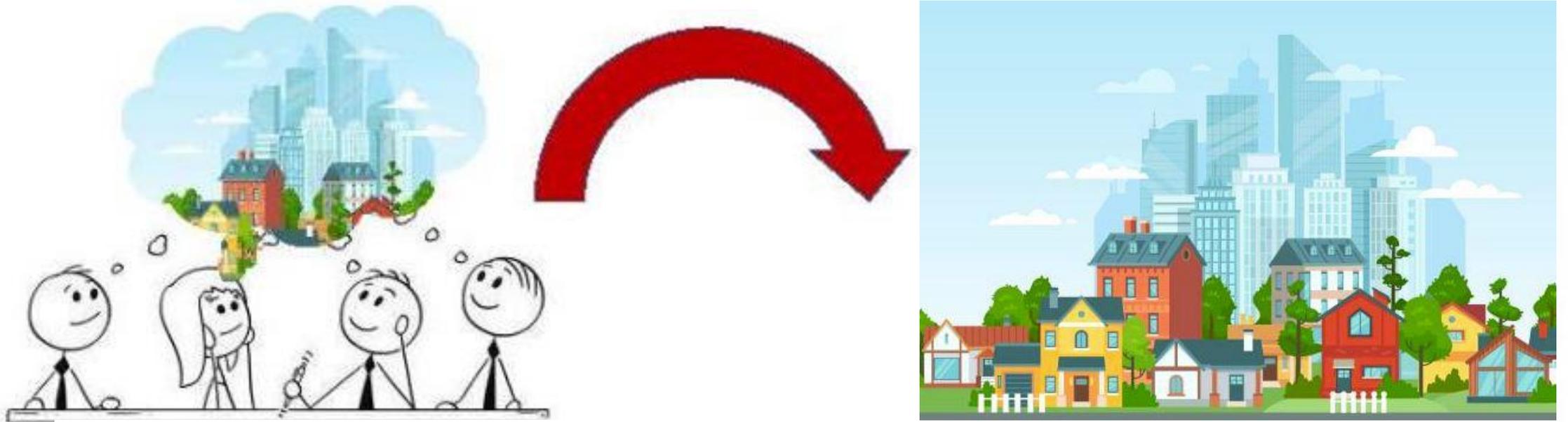
Questions?



Community Development Department
Building & Safety Division

City Council Update
September 5, 2023

The Community Development Department: Guides City Growth From Vision to Reality

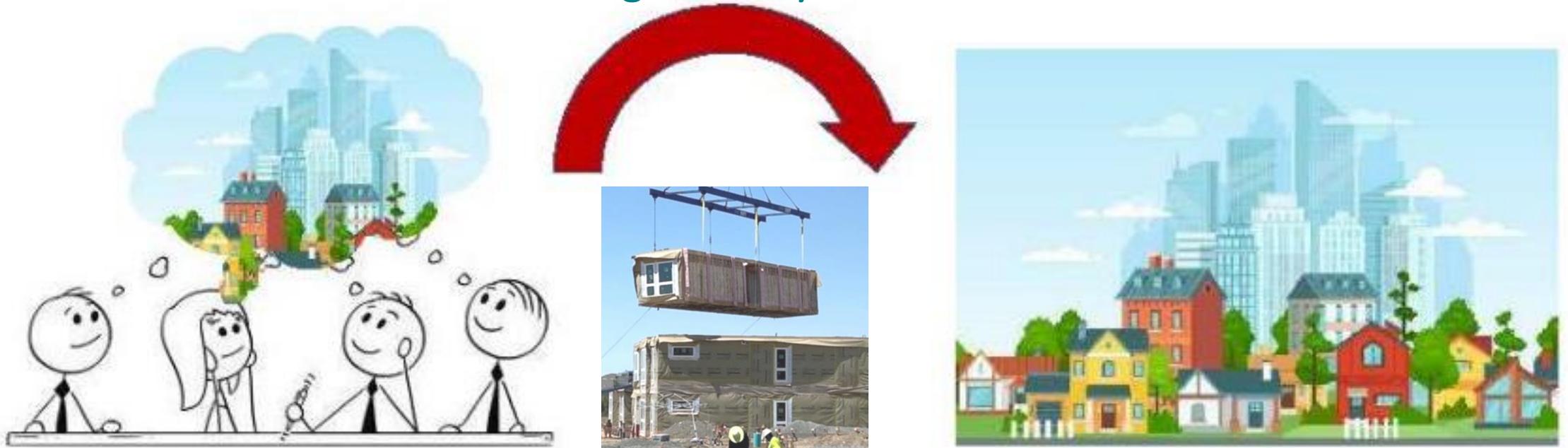


LOVE WHERE YOU LIVE.

AMERICAN CANYON GENERAL PLAN WWW.AMCAN2040.ORG

The Community Development Department: Guides City Growth From Vision to Reality

Building & Safety Services



LOVE WHERE YOU LIVE.

AMERICAN CANYON GENERAL PLAN WWW.AMCAN2040.ORG

Our Building Division Team



Joshua Anderson
Contract Building Official



Tiffany Ford
Code Enforcement



Laura Welch
Permit Technician



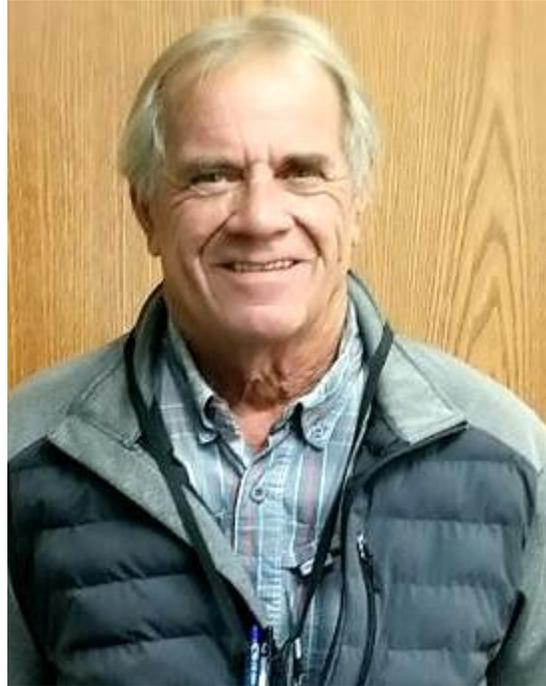
Stacy Barker
Contract Permit
Technician

Our Building Division Team



Nicolle Hall

Administrative Technician



Curtis Thibodeau

Contract Building Inspector



Fred Saavadra

Contract Building Inspector



Julianne Geilfuss

Office Assistant

Building & Safety Services

Ensure buildings are constructed and operated to protect life and safety:

- Update Building and Construction Ordinances
- Public Information Resources
- Online Processing
- Building Permit Plan Check
- Approve alternative “means and methods” for special circumstances
- Collect Processing and Impact fees
- Building Inspection and Sign-Off (Final Inspection)

Building Permit Review Process

Permit Technician Coordinates this Process



Single Family Building Inspection

Every home is inspected at each stage of construction

- Foundation Footing
- Concrete slab reinforcing steel
- Concrete slab
- Frame
- Drywall Nailing
- Fire and Smoke Penetration Inspection
- HVAC Ducts, Water Heater
- Ceiling
- Roof Plywood, Nailing,
- Roof Weather proofing paper
- Roof shingle inspection
- Rough and Final Electrical
- Rough and Final Plumbing
- Rough and Final Mechanical
- Stucco/Exterior Cladding
- Window and Door

16 Inspections



Tilt Up Concrete Building Inspection

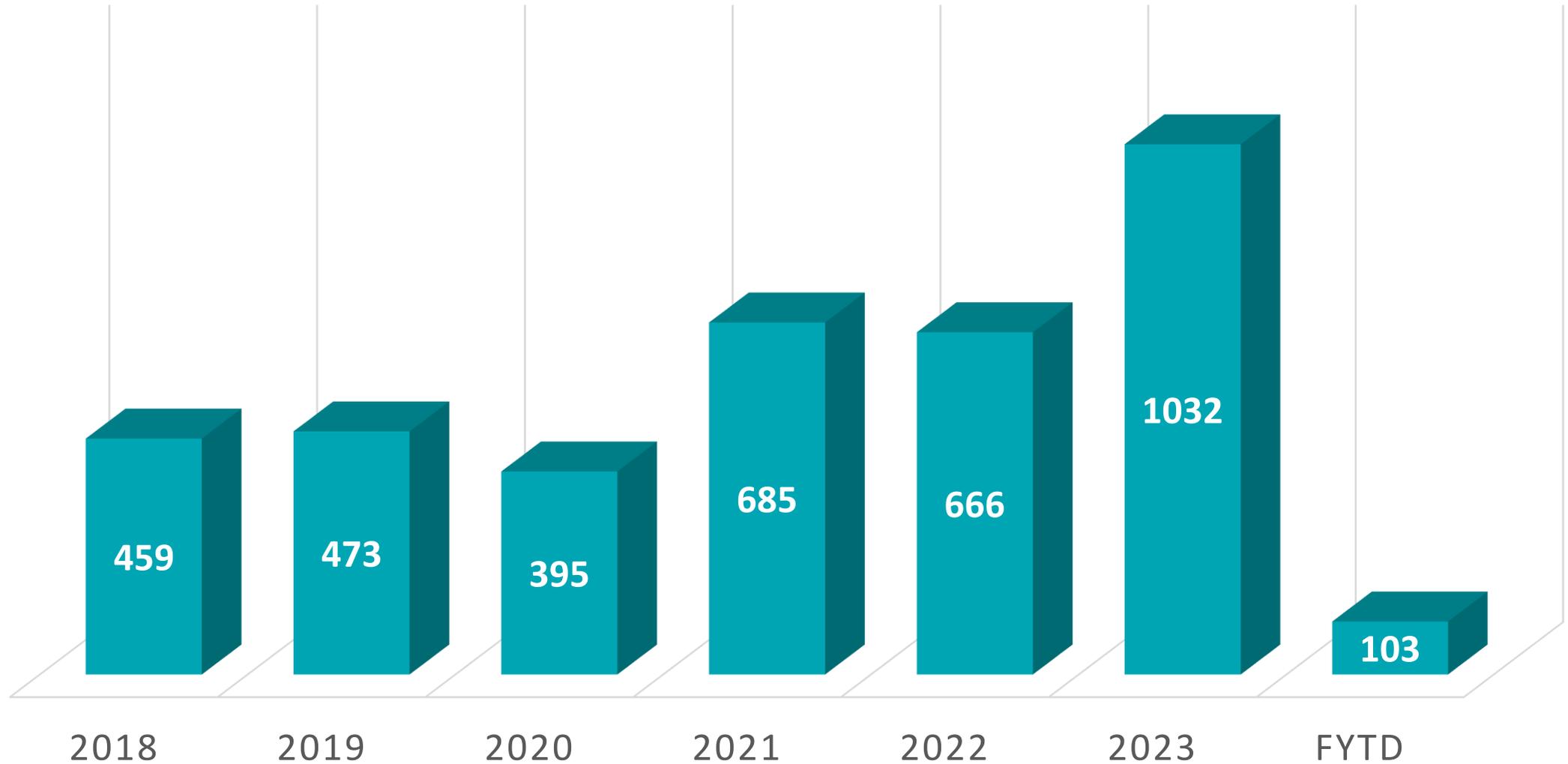
Same as Single Family **PLUS** -

- Reinforce steel ties
- Concrete Pour
- Wall connection to foundation
- Parking lot pavement
- Handicap Accessibility
- Restroom
- Energy Efficiency



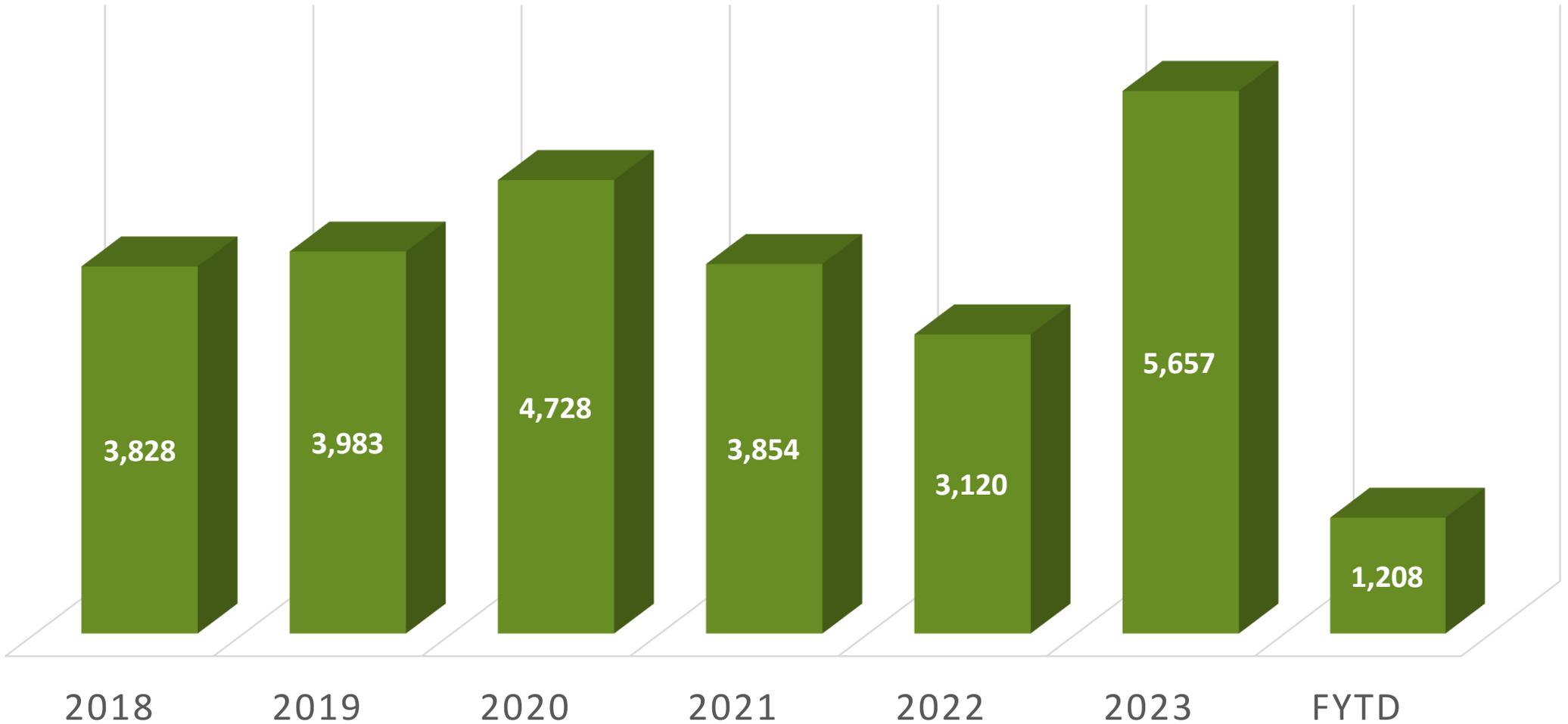
KPI Dashboard

BUILDING PERMITS ISSUED



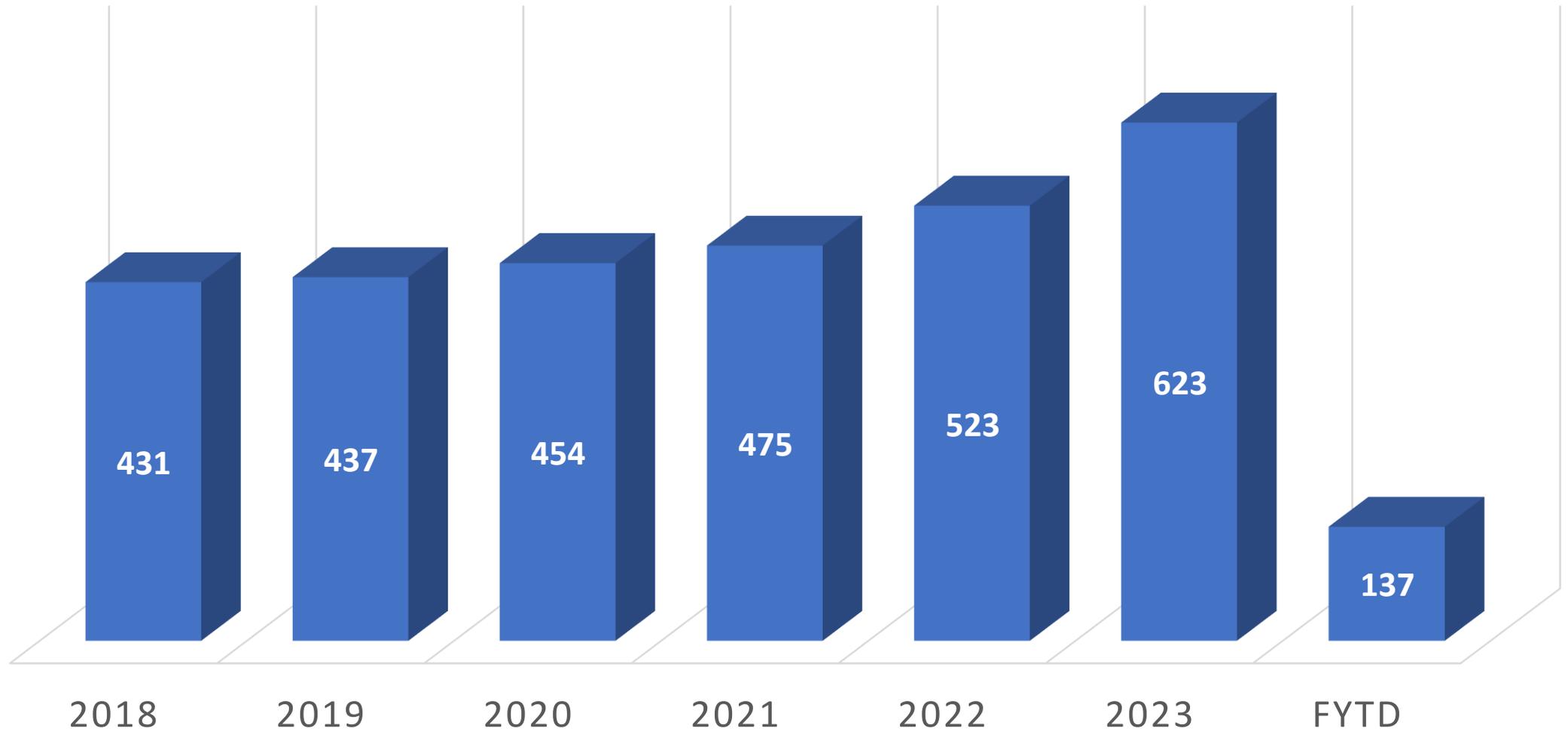
KPI Dashboard

BUILDING INSPECTIONS



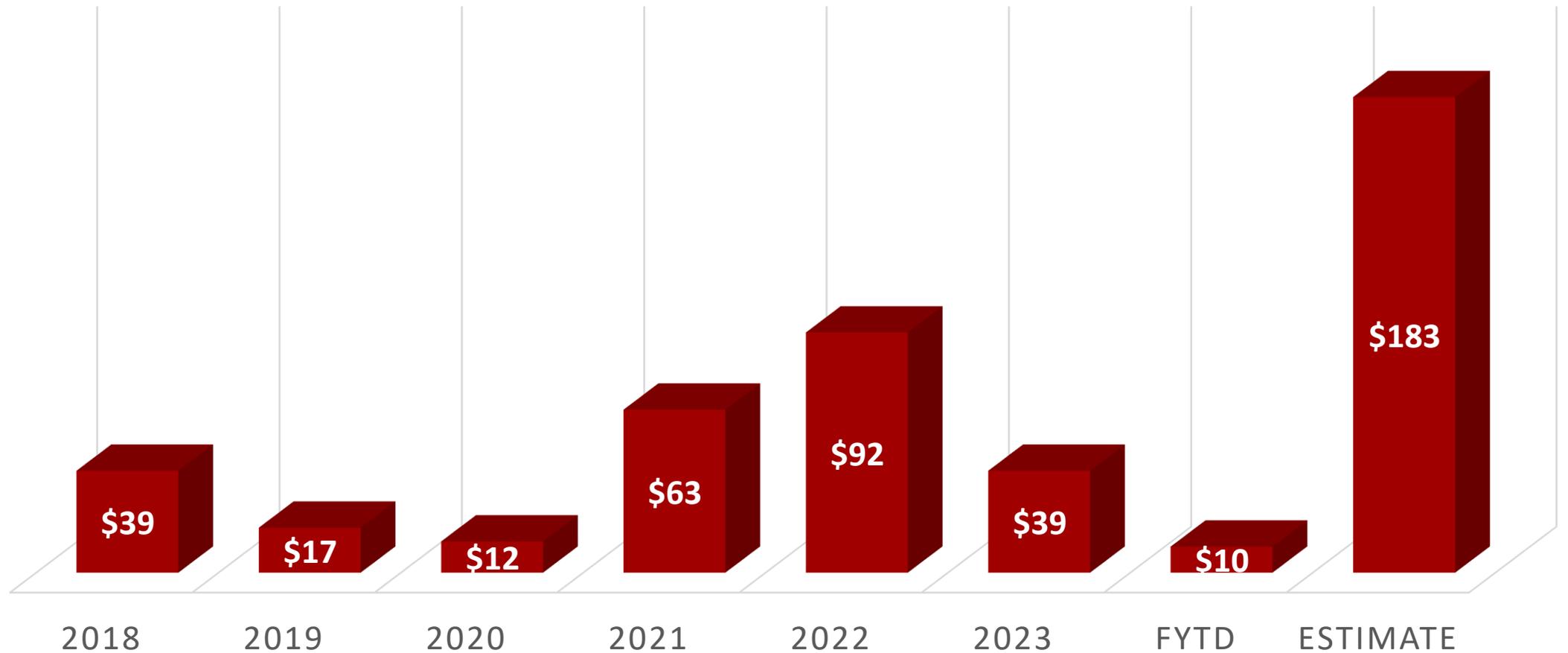
KPI Dashboard

BUILDING PERMITS FINALED



KPI Dashboard

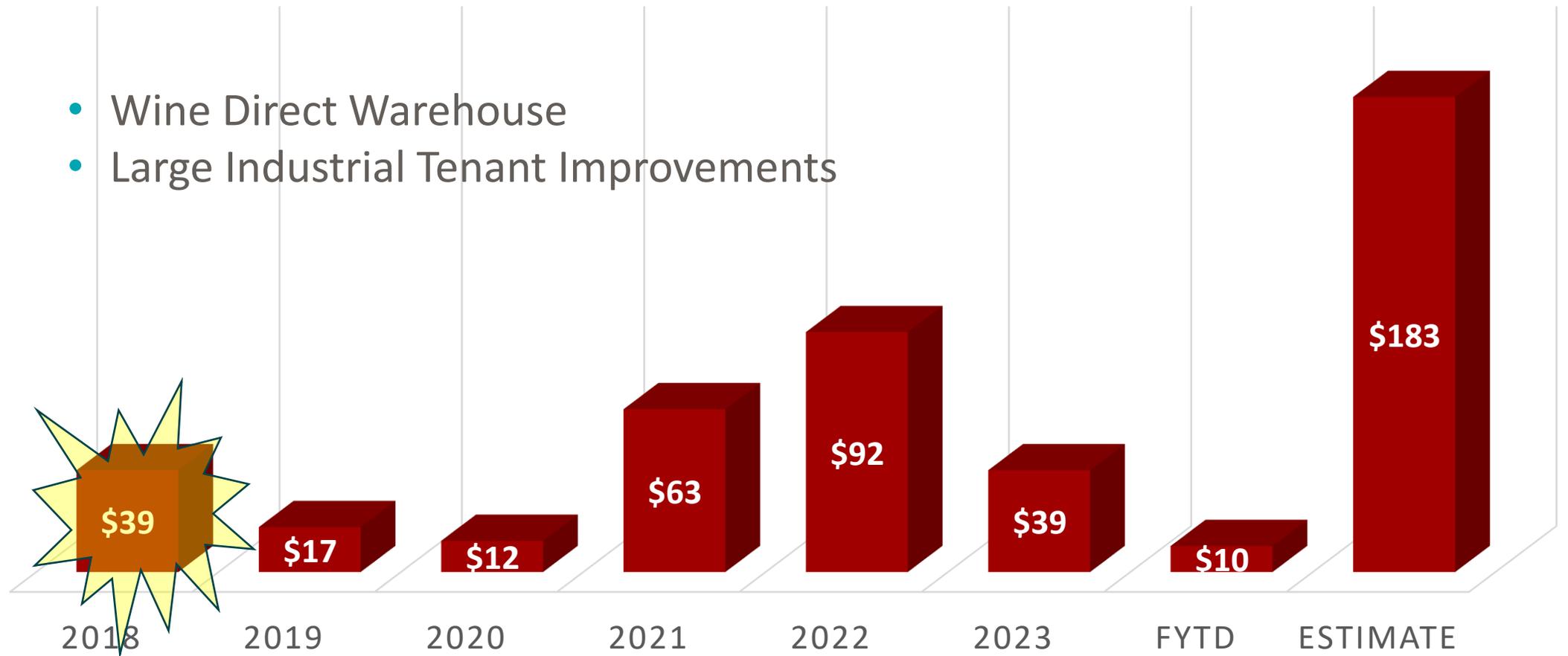
FINALED CONSTRUCTION VALUATION (\$MILLION)



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

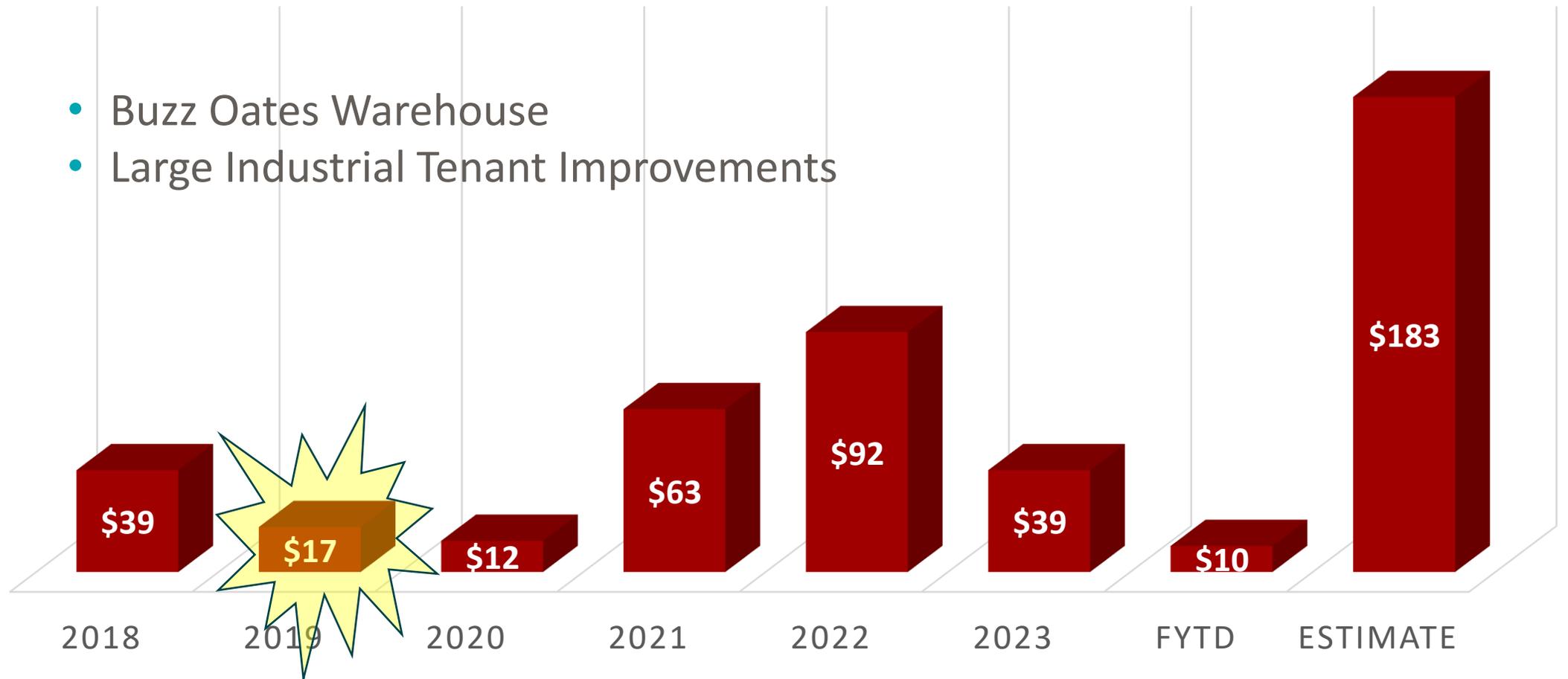
- Wine Direct Warehouse
- Large Industrial Tenant Improvements



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

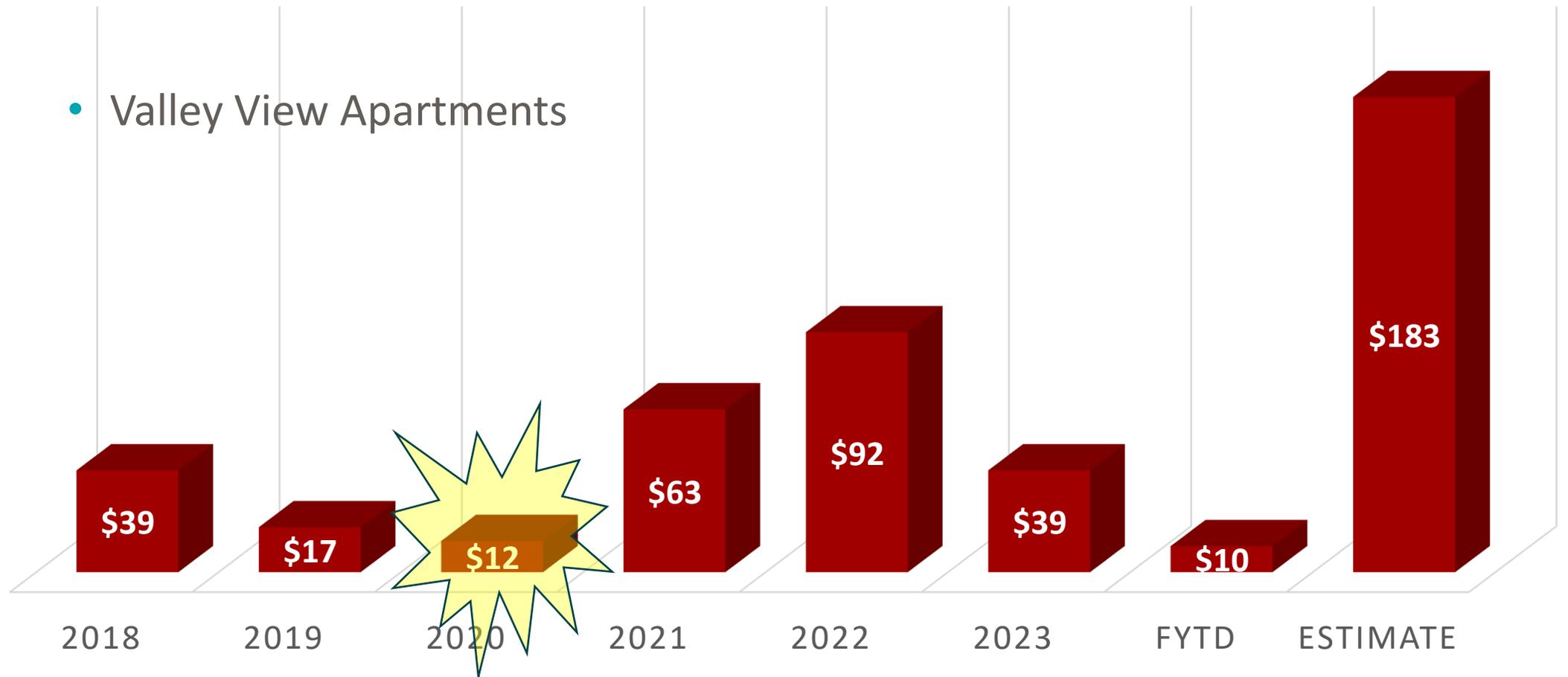
- Buzz Oates Warehouse
- Large Industrial Tenant Improvements



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

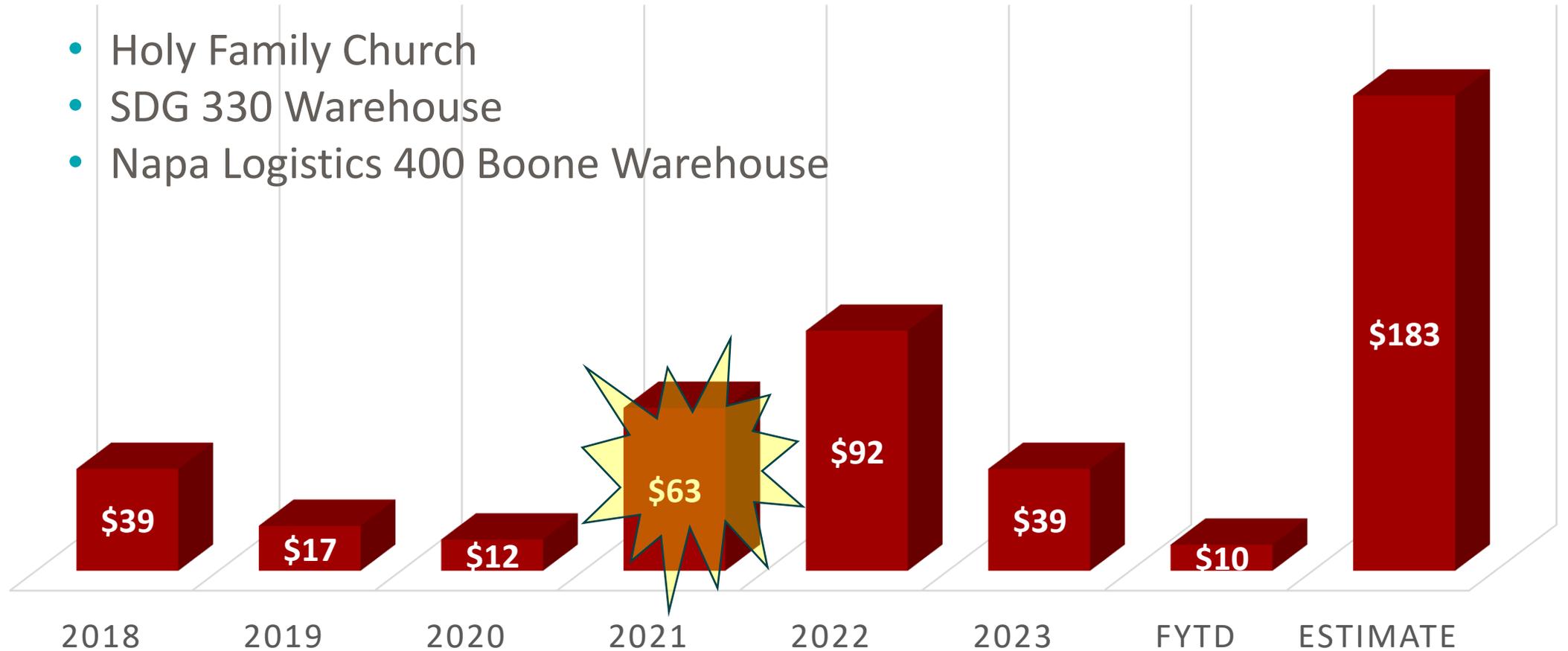
- Valley View Apartments



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

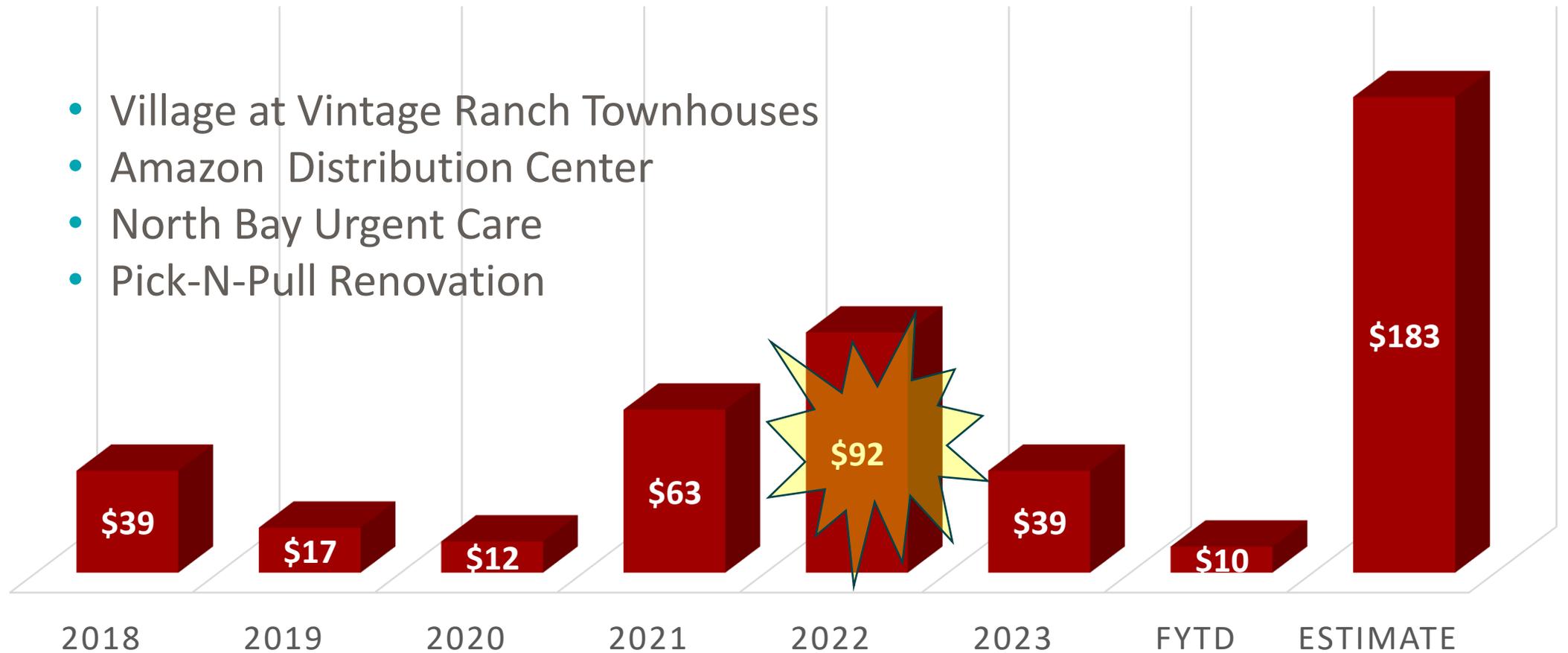
- Holy Family Church
- SDG 330 Warehouse
- Napa Logistics 400 Boone Warehouse



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

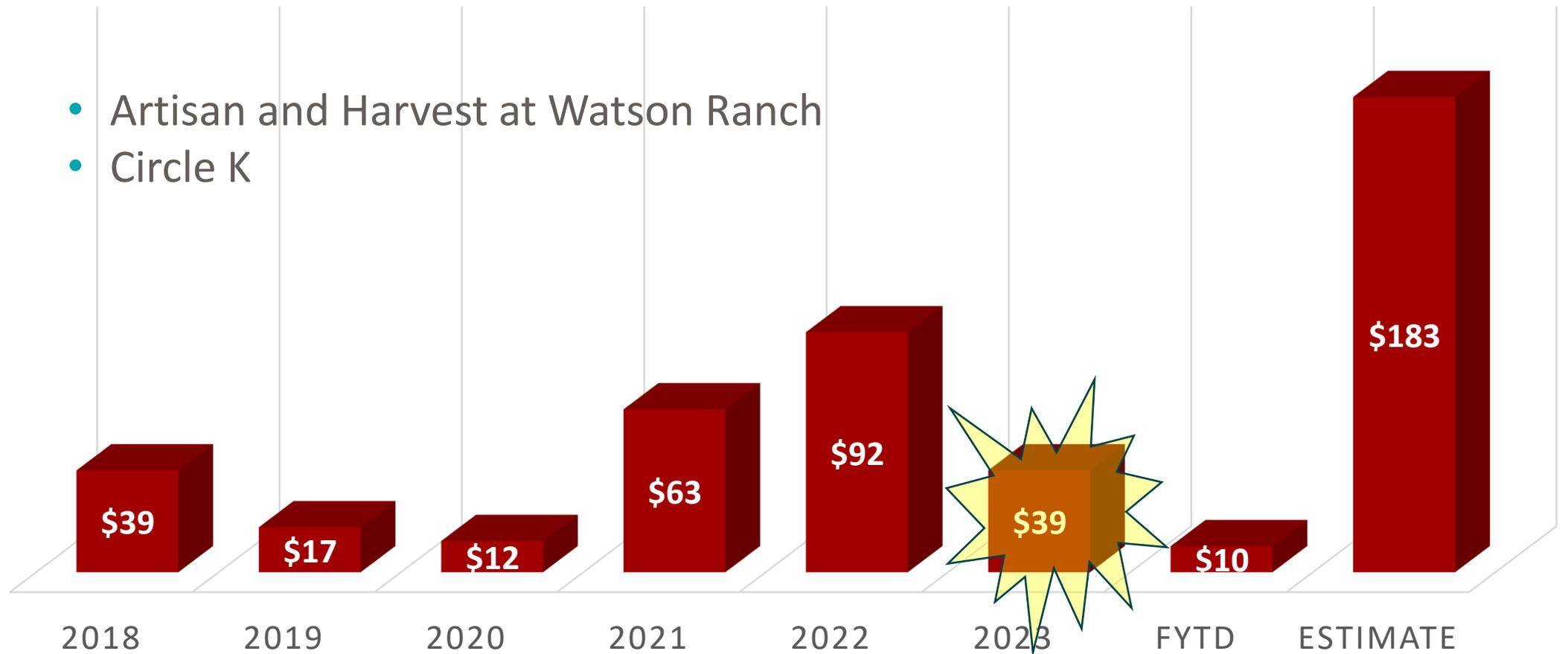
- Village at Vintage Ranch Townhouses
- Amazon Distribution Center
- North Bay Urgent Care
- Pick-N-Pull Renovation



KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

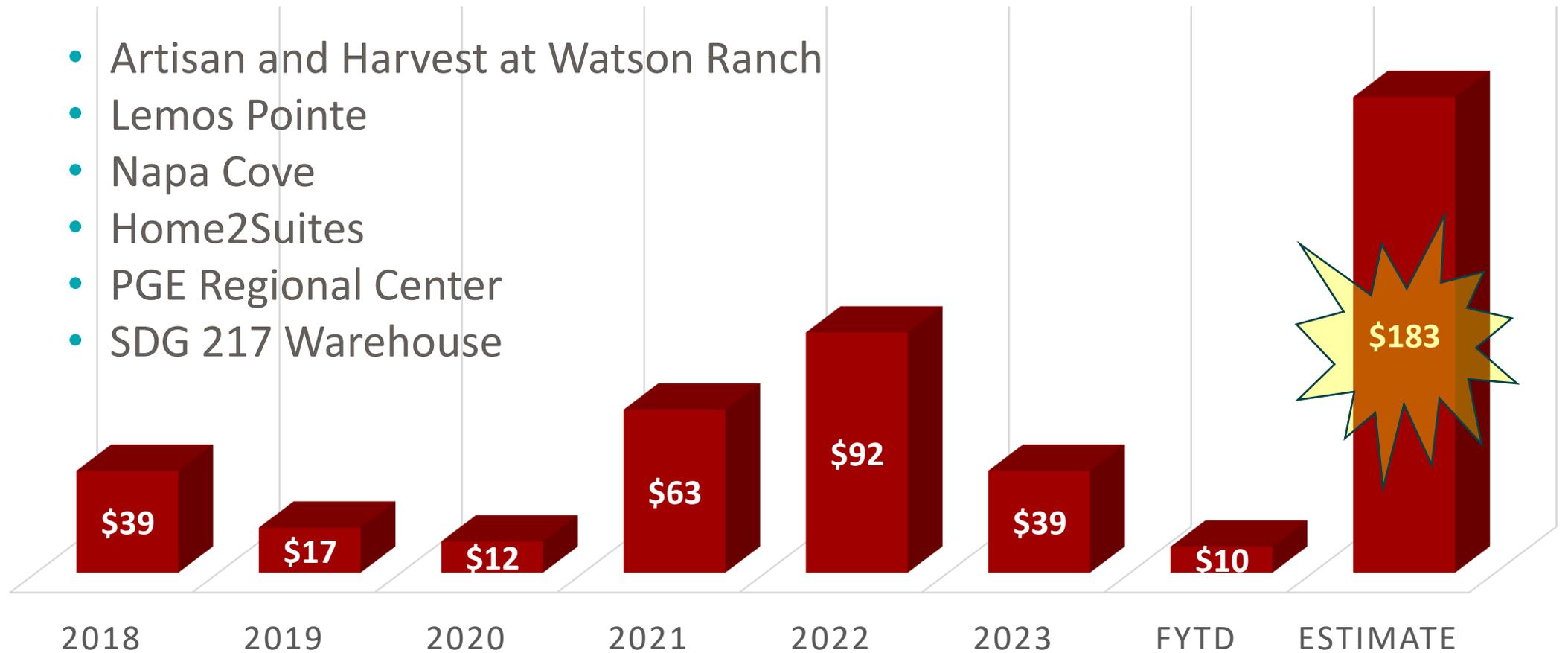
- Artisan and Harvest at Watson Ranch
- Circle K



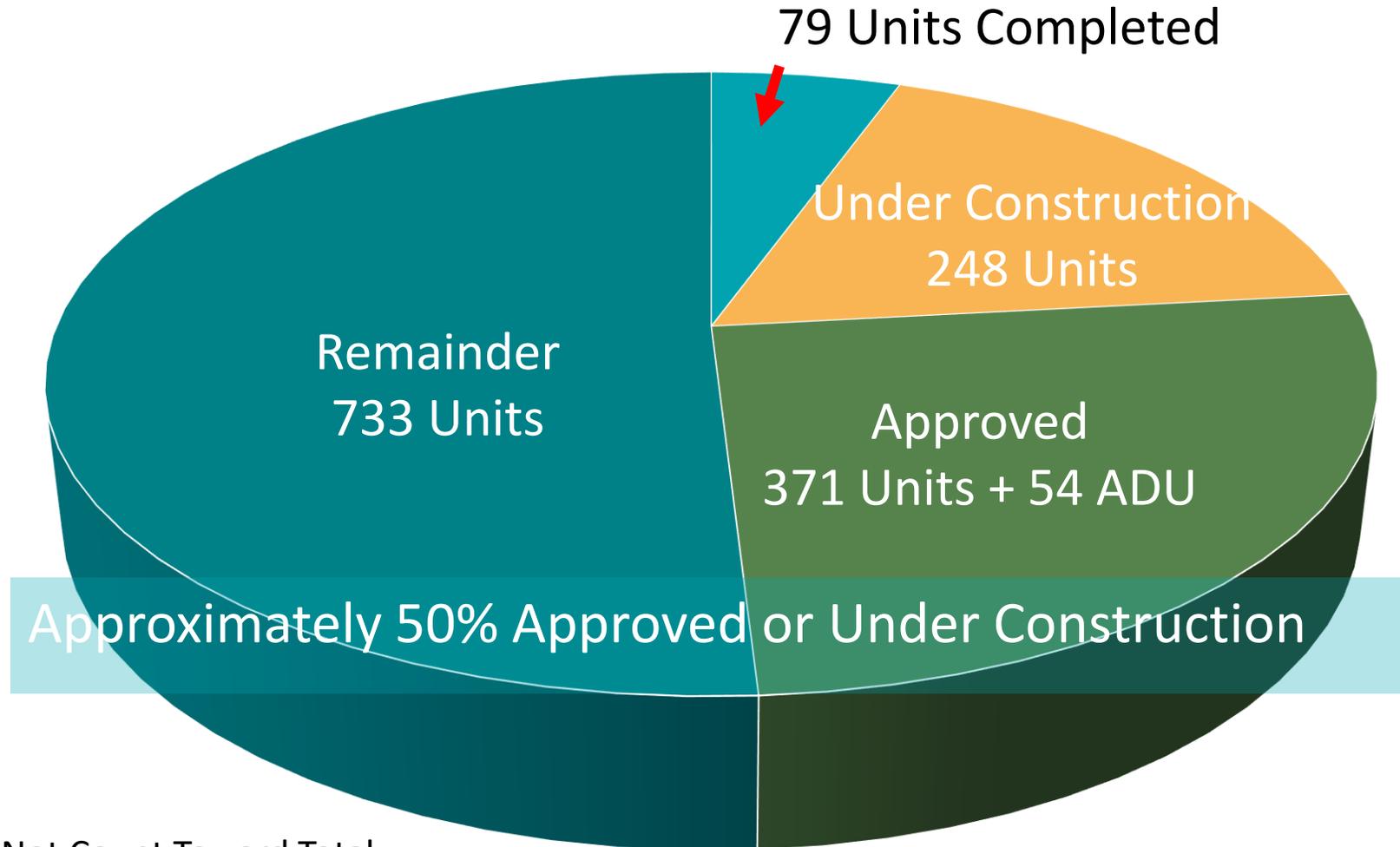
KPI Dashboard

FINALED CONSTRUCTION VALUATION (\$MILLION)

- Artisan and Harvest at Watson Ranch
- Lemos Pointe
- Napa Cove
- Home2Suites
- PGE Regional Center
- SDG 217 Warehouse



Watson Ranch Residential Construction Status (1,439 Total*)



*ADUs Do Not Count Toward Total

Lemos Pointe at Watson Ranch

186 Affordable Apartments

Interest List Started



Artisan at Watson Ranch

98 Single Family Homes

For Sale Now

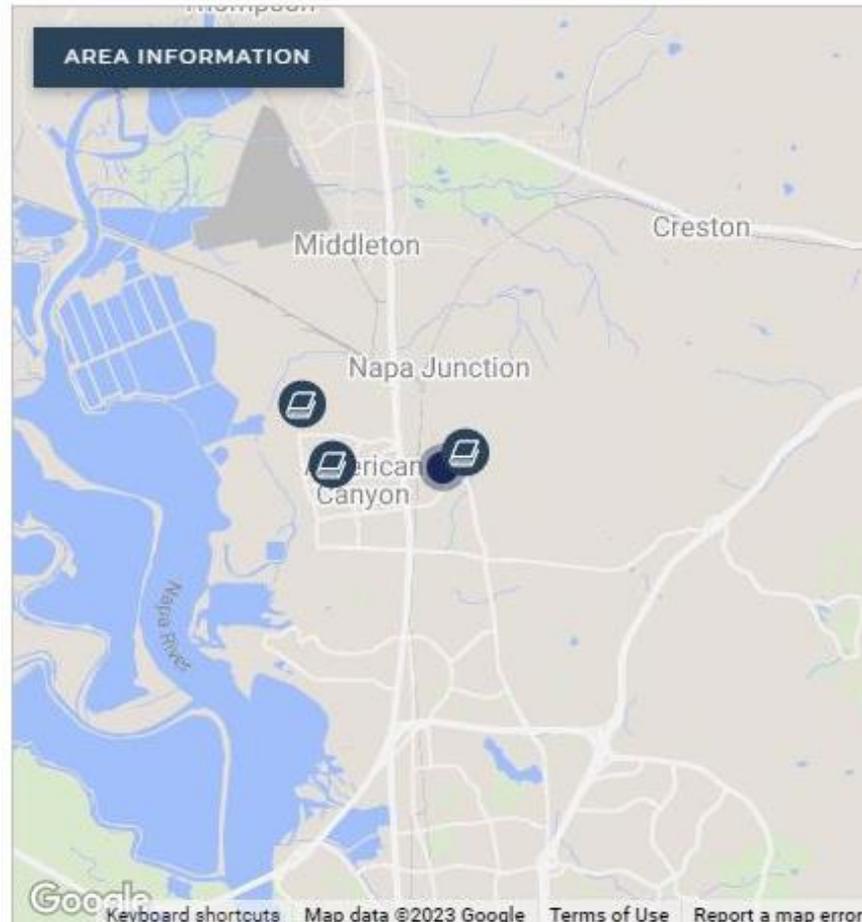
Residents Moved In

From **\$733,990** | **1,701 - 2,311** sq.ft.

[DIRECTIONS](#)

[REQUEST
INFO](#)

[SCHEDULE
TOUR](#)



RIGHT TIME. RIGHT HOME. RIGHT PRICE.



NOW SELLING

Harvest at Watson Ranch

219 Single family homes

From \$699,990 | 1,583 - 1,859 sq.ft.

For Sale Now
Residents Moved In



PLAN 1583
Elevation 'C' Farmhouse
Color Scheme 3B

PLAN 1799
Elevation 'A' Ranch
Color Scheme 1B

PLAN 1859
Elevation 'B' Traditional
Color Scheme 3BC

Napa Cove Apartments at BDSP

66 Affordable Apartments

Under Construction



Hilton Home2Suites Hotel @ BDSP

102 Rooms

Under Construction



PG&E Regional Center at Napa Logistics Park

Consolidates Vallejo and Napa service centers

Under Construction



SDG 217 Commerce Court Warehouse

217,000 Sqft Wine Warehouse



Development Project Web Sources

Construction and Project Updates Interactive map of Public Works Projects and Private Projects in Development. <https://bit.ly/3Fgt292>

Private Development Projects Under Review Includes a photo and brief project description. <https://bit.ly/3JBcl5l>

List of Active Projects under review and construction in the CD Department (Updated Monthly) Status of discretionary projects and where it is in the process along with construction permit status and major policy initiatives. <https://bit.ly/3mR4bSM>

Environmental Review Documents Lists approved and active California Environmental Quality Act environmental review documents. <https://bit.ly/3YJU7Z2>



Thank You!

Brent Cooper, AICP, Community Development Director

Joshua Anderson, CBO, Contract Building Official



City Council Committee Report

Submitted by: * Councilmember Mark Joseph

Council Meeting Date: 09/05/2023

Event Date: 2023-08-25 **Event Type:** Committee Report

Event Title: * Climate Action Committee

Event Report: At the regular meeting, we had a discussion regarding Reach Codes and a consensus around "Flex-Path" Reach Code was arrived at, which encourages a greater reliance on electrification through incentives and setting a higher standard that is likely to result in electrification. We also directed staff to explore a Home Energy Use report to be added to any prospective home sale, so the buyer would know how energy efficient the home is.

We also received a presentation on the last two elements of the GHG Inventory: Short Lived Climate Pollutants (primarily, Black Carbon and Methane), and the Carbon Sequestration Report. Although late, these two reports provide some insights for addressing Climate Change at this time.

Lastly, we are moving forward with an RFP to hire a consultant to complete a Regional Climate Action Plan. Thanks to State Senator Dodd's help, we received an allocation to cover most or all of the costs, But the study won't be finished until late 2024. It will include a "Dashboard" of metrics that we can use to monitor our progress or identify areas to focus on.

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Event Date: 2023-08-30 **Event Type:** Committee Report

Event Title: * Napa Valley Transportation Authority (NVTA)

Event Report: We canceled our August Board meeting, but I did attend the third and final task force relating to a Vision Zero Study. We received the draft report and have a month to provide comments. The next step is to present it for action to the NVTA Board and then request each jurisdiction to endorse the plan.

There was a good discussion about some of the policy implications--that is, Vision Zero is critical, but it needs to be integrated with other values, such as to improve traffic flow (and thus, how do policy makers arbitrate between the two), how to reduce Vehicle Miles Traveled (VMT), and reducing Greenhouse Gases.

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Event Date: 2023-08-23 **Event Type:** Committee Report

Event Title: * BCDC Bay Adapt Local Electeds Task Force

Event Report: This committee is intended to provide input on a Bay Area-wide study on addressing sea level rise. After several months, the task force met in person at the West County Wastewater Treatment Plant in Richmond. We heard an update on the study and toured the site, which will include an Horizontal Levee (basically, a levee with a sloping dimension between the levee and the wetlands).

It was interesting to note that there were far more staff (from various agencies) than electeds. However, I was able to tentatively schedule a community outreach in American Canyon as part of the Pumpkin Path event at Wetlands Edge Park on October 7.

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Event Date: **Event Type** Community Event

Event Title: * Various Community Events and Activities

- Event Report:**
- I helped with the monthly Kiwanis Food Distribution. Also worked on the prospects of opening a "Choice Model" Food Pantry at the old Napa Junction elementary school site. Attended the regular Board meeting.
 - With the Arts Foundation, we received notice that we received a \$5000 grant to produce a Cultural Diversity Art Show in 2024. We skipped the August Board meeting, but we did hold the first "Battle of the Bands" event at the Meet Me in the Street August event (grant funded, thanks to Napa County). And worked on leasing space at the old NJE site for an art center.
 - Attended the Napa Community Benefits Coalition meeting. We reviewed the upcoming Health Care Workers Meeting at Napa Valley College on Sept. 8, 9am to 1pm. We also reviewed a new grant program, CERF (Community Economic Resilience Fund), which may offer some support for livable wage job development.
 - Attended the Meet Me in the Streets Thank You party.

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Event Date: 2023-08-23 **Event Type** Other

Event Title: * One on One with Lisa Chu

Event Report: I met our NVUSD Trustee for coffee. We discussed the upcoming Mayacamas Charter School and the latest variation: a Countywide Charter School. Both projects seem to be counter productive to NVUSD's long-term financial success. It is particularly galling since American Canyon, which has the most robust enrollment status, would not be included in either the Mayacamas or the Countywide options. As Lisa points out, if the charter advocates wanted a successful program, they could offer it in American Canyon, in which there would be two, appropriate sized campuses with above average scores (instead of one almost oversized campus with above average scores!).

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