



Monday, February 3, 2025
Charles County Planning Commission Meeting

This agenda is tentative and subject to change without notice.

A portion of this meeting may be held in Closed Session.

The Planning Commission will be holding this public meeting as a "Hybrid meeting" which means it will be both virtual and limited in-person. The public can watch this meeting on Comcast 95 (SD), Verizon FIOS 10, Roku or Apple TV streaming devices (Charles County Government), and the web at <https://www.charlescountymd.gov/services/media-services/charles-county-government-television/ccgtv-live-stream>. Residents without internet service can listen to the meeting at 301-645-0500.

1. Call to Order/Roll Call

2. APPROVAL OF THE AGENDA - no public comments

3. APPROVAL OF THE MINUTES

3.a January 13, 2025 Minutes

4. CHAIRPERSON'S COMMENTS - no public comments

5. PERSONAL APPEARANCES (items not on the agenda): PUBLIC COMMENTS

6. PUBLIC HEARING: PUBLIC COMMENTS

7. PUBLIC MEETING: PUBLIC COMMENTS

7.a Bayside Kia of Waldorf, SDP-240001 - Adequate Public Facilities Findings

The Planning Commission will consider the Adequate Public Facilities findings for a proposed Site Development Plan. Review and approval of a Site Development Plan is an internal staff function (does not require Planning Commission approval); however, in accordance with § 297-256A, approval of a Major Site Plan does first require the Planning Commission to determine that the proposed project will not adversely affect the adequacy of public facilities serving the area, project, or development.

Applicant (Agent):

Wanamaker Properties VII, LLC (Lorenzi, Dodds, & Gunnill, Inc.)

Staff:

Heather Kelley, AICP, Planning Supervisor

Public Participation:

The Meeting is open to the public and may be attended in person or viewed on [CCGTV](#) (Comcast: 95 and Verizon FIOS: 10).

Written Public Comments can be submitted online by using the webform located [HERE](#). Written comments must be received by **4:30 p.m. on Friday, January 31, 2025** in order to allow the Planning Commission time to review them prior to the Meeting. Written comments received after this time and before the closing of the record will be included in the record, but are not guaranteed to be reviewed.

Those wishing to provide comments by speaking during the Public Comment portion of the Meeting may choose to either speak virtually or attend the Meeting in person. Virtual speaker registration forms can be submitted online by using the webform located [HERE](#). Virtual speaker registration forms must be received by **4:30 p.m. on Friday, January 31, 2025**.

[Bayside Kia of Waldorf APF Findings Staff Report](#)
[Location Map](#)
[Zoning Map](#)
[Aerial Photograph](#)
[Site Development Plan SDP-240001, Bayside Kia](#)
[Applicant's Traffic Mitigation Proposal](#)

8. WORK SESSIONS: No Public Comments

9. NEW BUSINESS: No Public Comments

9.a Economic Development Briefing and Update

Staff: Kelly Robertson-Shlagel
[Planning Commission Update Economic Development.pdf](#)

9.b Affordable Housing Update

Staff: Joel Binkley

9.c Poll of the Planning Commission Members for New Business

10. UNFINISHED BUSINESS: No Public Comments

11. DIRECTOR'S REPORT: No Public Comments

12. ADJOURNMENT

13. VIRTUAL MEETING INFORMATION

14. Signed Minutes

Item Cover Page

PLANNING COMMISSION AGENDA ITEM REPORT

DATE: February 3, 2025

SUBMITTED BY: Amy Brackett

ITEM TYPE: Minutes

AGENDA SECTION: APPROVAL OF THE MINUTES

SUBJECT: **January 13, 2025 Minutes**

SUGGESTED ACTION:

ATTACHMENTS:

Item Cover Page

PLANNING COMMISSION AGENDA ITEM REPORT

DATE: February 3, 2025

SUBMITTED BY: Amy Brackett

ITEM TYPE: Approval Item(s)

AGENDA SECTION: PUBLIC MEETING: PUBLIC COMMENTS

SUBJECT: **Bayside Kia of Waldorf, SDP-240001 - Adequate Public Facilities Findings**

The Planning Commission will consider the Adequate Public Facilities findings for a proposed Site Development Plan. Review and approval of a Site Development Plan is an internal staff function (does not require Planning Commission approval); however, in accordance with § 297-256A, approval of a Major Site Plan does first require the Planning Commission to determine that the proposed project will not adversely affect the adequacy of public facilities serving the area, project, or development.

Applicant (Agent):

Wanamaker Properties VII, LLC (Lorenzi, Dodds, & Gunnill, Inc.)

Staff:

Heather Kelley, AICP, Planning Supervisor

Public Participation:

The Meeting is open to the public and may be attended in person or viewed on [CCGTV](#) (Comcast: 95 and Verizon FIOS: 10).

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SUGGESTED ACTION:

ATTACHMENTS:

[Bayside Kia of Waldorf APF Findings Staff Report](#)
[Location Map](#)
[Zoning Map](#)
[Aerial Photograph](#)
[Site Development Plan SDP-240001, Bayside Kia](#)
[Applicant's Traffic Mitigation Proposal](#)



Charles County Planning Commission Meeting of February 3, 2025

Department of Planning and Growth Management Staff Report

**Project Name: Bayside Kia of Waldorf
Adequate Public Facilities Findings
Site Development Plan (SDP-240001)**

**Prepared by Melissa Hively, Planner II
For questions, contact Planning at 301-645-0692**

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ii. Zoning Map	
iii. Aerial Photograph	
iv. SDP-240001 – Bayside Kia of Waldorf	
v. Applicant's Mitigation Proposal	

I. Applicant & Project Information:

- a. Project Name: Bayside Kia of Waldorf
 - i. Applicant: Wanamaker Properties VII, LLC
 - ii. Engineer: Lorenzi, Dodds & Gunnill, Inc.
- b. Project Number: SDP-240001
- c. Subject Property: The Project encompasses Tax Map 15, Parcel 776 and has access off Crain Highway (US 301) in Waldorf, Maryland.
- d. Zoning Category: CC (Community Commercial)
- e. Background / History of the Property: The Applicant is proposing to develop the parcel with a 35,216 square-foot automotive dealership as Phase 1 of the South Park East project. Review and approval of a Site Development Plan is an internal staff function (does not require Planning Commission approval); however, in accordance with § 297-256A, approval of a major site plan that requires mitigation does first require the Planning Commission to determine that the proposed project will not adversely affect the adequacy of public facilities serving the area, project, or development.

II. Adequacy of Public Facilities:

As referenced above, § 297-256A of the Charles County Zoning Ordinance requires the Planning Commission to determine that the proposed development, as shown on a major site plan, will not adversely affect the adequacy of public facilities serving the area, project, or development. In the Zoning Ordinance, there are four (4) categories of public facilities that must be evaluated prior to the approval of a major site plan: (1) roads; (2) schools; (3) water supply; and (4) rural fire suppression.

APF-Roads:

The Applicant submitted a Traffic Impact Study (TIS) which analyzed four (4) nearby intersections and one (1) road link, utilizing the guidelines set forth in the Adequate Public Facilities Manual (APFM). The following intersections and road links were analyzed in the TIS to assess the impacts of the Project.

The proposed project is located in the "Development District" of the Charles County Comprehensive Plan, which requires a "C" or better level of service. However, the US 301 roadway is a "Waldorf Transportation District Route." The Adequate Public Facilities Manual indicates that the Waldorf Transportation District Routes (US 301) level of service requirement is "D" or better. If the facility will not meet the LOS standard and/or its volume to capacity ratio is degraded by a factor of 0.01 or more (a one percent change), then mitigation will be required to offset the development's impact.

1. **Intersection of US 301 @ Smallwood Drive (Signalized):**

Traffic analyses under signalized conditions show that in the Future condition the intersection is projected to function at a Level of Service (LOS) “D” in the AM and Saturday peak hours, and a Level of Service (LOS) “E” during the PM peak hour. However, as noted in the Adequate Public Facilities Manual, if the volume to capacity is degraded by less than 0.01 (1 percent), mitigation is not required. In this case, the volume to capacity ratio is not degraded by 0.01 or more; therefore, mitigation is not required. **This intersection meets the standards as set forth in Section 257 of the Zoning Ordinance.**

2. **Intersection of US 301 @ Northern Median Break (Unsignalized):**

Traffic analyses under *future unsignalized condition* show that in the Future condition, the intersection is projected to function with reserve capacity on the stop-controlled movement during the AM peak hour. However, the intersection is projected to function without reserve capacity for the Westbound Left/Right and Southbound Left/U-turn movement during the PM and Saturday peak hours. Level of Service was calculated to be “B” during AM peak, and “C” during the PM and Saturday peak hours.

As indicated in the APF Manual, when reserve capacity is not available, additional studies should be conducted to determine the potential means to fix the deficiency. The Applicant is proposing to conduct a traffic signal warrant analysis at a future date to determine if traffic signalization is necessary. Staff notes that MDOT-SHA has agreed to this proposal. **Therefore, this intersection meets the standards as set forth in Section 257 of the Zoning Ordinance.**

3. **Intersection of US 301 @ Southern Median Break (site access) (Unsignalized):**

Traffic analyses under *future unsignalized condition* show that in the Future condition, the intersection is projected to function with reserve capacity on the stop-controlled movement during the AM peak hour. However, the intersection is projected to function without reserve capacity for the Westbound Left movement during the PM and Saturday peak hours. Level of Service was calculated to be “B” during AM peak, and “C” during the PM and Saturday peak hours.

As indicated in the APF Manual, when reserve capacity is not available, additional studies should be conducted to determine the potential means to fix the deficiency. The Applicant is proposing to conduct a traffic signal warrant analysis at a future date to determine if traffic signalization is necessary. Staff notes that MDOT-SHA has agreed to this proposal. **Therefore, this intersection meets the standards as set forth in Section 257 of the Zoning Ordinance.**

4. **Intersection of US 301 @ Billingsley Road (Signalized):**

Traffic analyses under signalized conditions show that in the Future condition, the intersection is projected to function at a Level of Service (LOS) “E” in the AM, PM, and Saturday peak hours. As noted in the Adequate Public Facilities Manual, if the volume to capacity is degraded by less than 0.01 (1 percent), mitigation is not required. The impact on the volume to capacity ratio during the PM peak hour (1.1 %) and during the Saturday peak hour (1.9%) exceeds 1 % under Phase I of the project; therefore, mitigation is required. The Applicant proposes to address the inadequacy and the results of the queuing analysis, as discussed below, by constructing an additional left turn bay to provide double-left turn bays on the eastbound approach of Billingsley Road at US 301. The additional turn lane will be constructed to the extent practicable within the existing median area of Billingsley Road. With this improvement in place, the volume to capacity ratio will be improved to a better level than background conditions. **Therefore, with this improvement, this intersection meets the standards as set forth in Section 257 of the Zoning Ordinance.**

5. **Road Link of US 301 – Smallwood Drive and Billingsley Road:**

Traffic analyses under the multi-lane highway condition show that in the Future condition, this road link will operate at LOS “B” and “C” during the AM, PM, and Saturday peak hours. **Because the LOS is “C” or better, this road link meets the standards as set forth in Section 257 of the Zoning Ordinance.**

Queuing Analysis:

The Applicant conducted a queuing analysis based on a presumed build-out of the entire site that accompanied the Traffic Impact Study (TIS). Based on the analysis provided, the following was determined:

- The eastbound left turn bay from Billingsley Road onto US 301 is approximately 430'. The projected queue length will reach 370' under background conditions and as long as 456' during future phase conditions.
- The westbound double left turn bays from Billingsley Road onto US 301 are approximately 400' long. The projected queue length will reach 667' through future phase conditions. Since the proposed development will not add traffic to these turn bays, the Applicant has not proposed improvements at this location.
- The northbound left turn bay from US 301 to Billingsley Road is approximately 990' long. The projected queue length will reach 1359' through future phase conditions. Since the proposed development will not add traffic to this turn bay, the Applicant has not proposed improvements at this location.

The Applicant proposes to address the results of the queuing analysis by constructing the additional turn lane discussed above.

APF - Water Supply:

The project will connect to the public water system and will obtain a water allocation prior to receiving Development Services Permit approval.

APF - Rural Fire Suppression:

The project is commercial and located within a W1 service area; therefore, it does not require rural fire suppression.

APF – Schools:

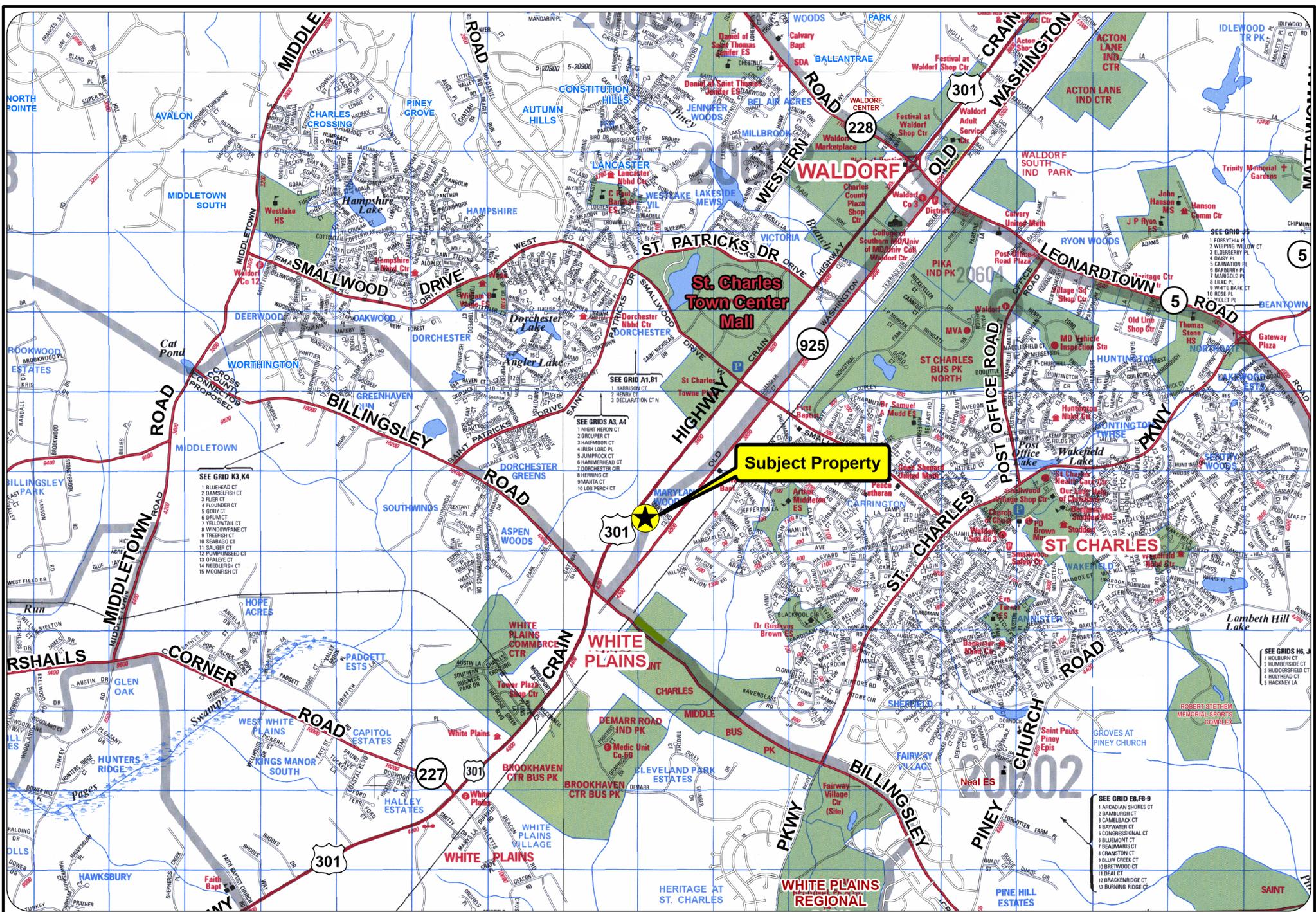
The project is commercial and does not require school allocations.

III. Conclusion & Recommendation:

Based upon the analysis presented above, Planning Staff has concluded that, with mitigation to solve the deficiency at US 301 @ Billingsley Road, the proposed development will not adversely affect the adequacy of public facilities; therefore, Planning Staff recommends that the Planning Commission adopt these findings subject to the conditions below so that the major site plan can be approved by the Planning Director when all other components of the plan are deemed to be complete.

1. A traffic signal warrant analysis for the intersection of US 301 @ Northern Median Break will be submitted with each future Site Development Plan for the South Park East project to determine if traffic signalization is necessary.
2. A traffic signal warrant analysis for the intersection of US 301 @ Southern Median Break (site access) will be submitted with each future Site Development Plans (SDP) for the South Park East project to determine if traffic signalization is necessary.
3. The Applicant shall construct an additional left turn bay to create a double-left turn bay on the eastbound approach of Billingsley Road at US 301. The off-site road improvements must reach "substantial completion" prior to the Bayside Kia of Waldorf property receiving a Use and Occupancy Permit. Substantial Completion means the completion of base pavement, and installation of drainage facilities.

IV. Appendices: Attached and [Online](#).



- SEE GRID K3,K4
- 1 BLUEHEAD CT
 - 2 DANIELFISH CT
 - 3 FLEET CT
 - 4 FLOUNDER CT
 - 5 GOBY CT
 - 6 BRIM CT
 - 7 YELLOWTAIL CT
 - 8 WINDOWPANE CT
 - 9 TRESHER CT
 - 10 SEABAGO CT
 - 11 SAUGER CT
 - 12 POMPINOSEED CT
 - 13 SPINNYE CT
 - 14 NEEDLEFISH CT
 - 15 WOODFISH CT

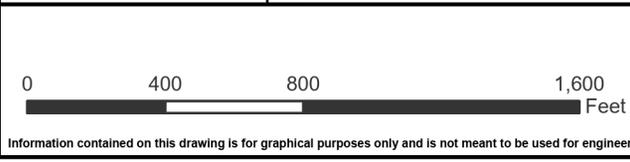
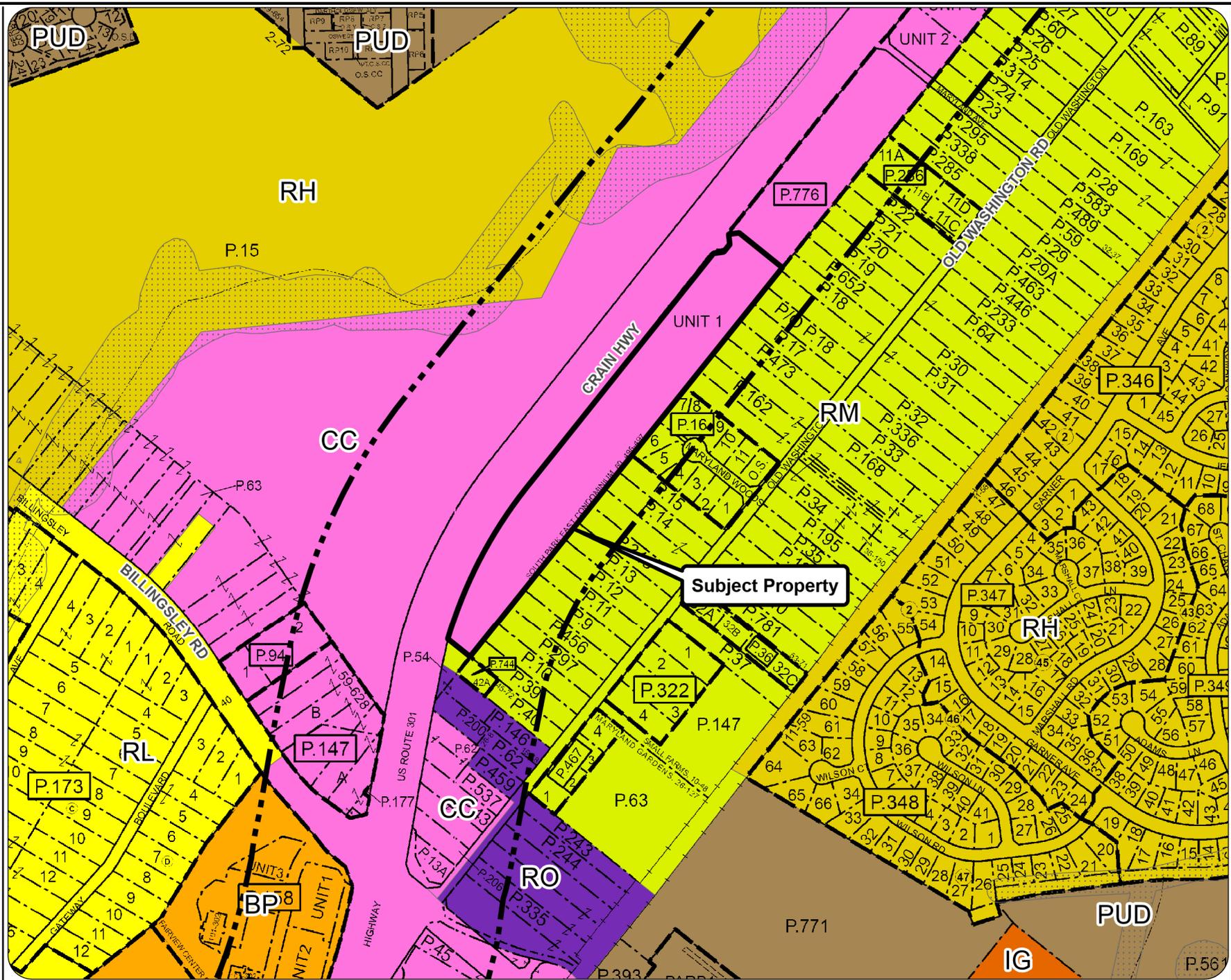
- SEE GRID A1,B1
- 1 HARRISON CT
 - 2 HENRY CT
 - 3 DECLARATION CT N
- SEE GRIDS A3, A4
- 1 NIGHT HERON CT
 - 2 HALLMOON CT
 - 3 HIGH LORE PL
 - 4 JUMPPOCK CT
 - 5 HAMMERHEAD CT
 - 6 ZOOKEEPER CR
 - 7 HERRING CT
 - 8 MANITA CT
 - 9 WILSON PK CT

- SEE GRIDS H6, J
- 1 HOLBURN CT
 - 2 HAMBROCK CT
 - 3 HULLHEAD CT
 - 4 HACKNEY LA

- SEE GRID EB,F8-9
- 1 ARCADIAN SHORES CT
 - 2 DAMBURGH CT
 - 3 CAMERBACK CT
 - 4 BANWATER CT
 - 5 CONGRESSIONAL CT
 - 6 BLENDMONT CT
 - 7 TREASURERS CT
 - 8 CRANSTON CT
 - 9 BLUFF CREEK CT
 - 10 BREEWOOD CT
 - 11 IDEAL CT
 - 12 BRACKENRIDGE CT
 - 13 BURNING RIDGE CT

		SCALE CHARLES COUNTY GOVERNMENT Department of Planning and Growth Management 200 Baltimore St La Plata, MD 20646 (301)645-0627	DATE January 2025	Bayside Kia SDP 240001 LOCATION MAP CHARLES COUNTY, MD	11
		DRAWN BY: CHECKED BY:	Information contained on this drawing is for graphical purposes only and is not meant to be used for engineering purposes.		

BASE ZONES	
AC	AGRICULTURAL CONSERVATION
RC	RURAL CONSERVATION
WCD	WATERSHED CONSERVATION DISTRICT
RR	RURAL RESIDENTIAL
RV	VILLAGE RESIDENTIAL
RL	LOW DENSITY SUBURBAN RESIDENTIAL
RM	MEDIUM DENSITY SUBURBAN RESIDENTIAL
RH	HIGH DENSITY SUBURBAN RESIDENTIAL
RO	RESIDENTIAL / OFFICE
CER	CORE DEVELOPMENT / RESIDENTIAL
CMR	CORE MIXED RESIDENTIAL
CRR	CORE RETAIL RESIDENTIAL
CN	NEIGHBORHOOD COMMERCIAL
CC	COMMUNITY COMMERCIAL
CB	CENTRAL BUSINESS
CV	VILLAGE COMMERCIAL
BP	BUSINESS PARK
IG	LIGHT INDUSTRIAL
IH	HEAVY INDUSTRIAL
AUC	ACTON URBAN CENTER
WC	WALDORF CENTRAL
OVERLAY ZONES	
HIGHWAY CORRIDOR	
RESOURCE PROTECTION	
CRITICAL AREA BOUNDARY	
FLOATING ZONES	
PRD	PLANNED RESIDENTIAL DEVELOPMENT
PMH	PLANNED MOBILE HOME PARK
PEP	PLANNED EMPLOYMENT/INDUSTRIAL PARK
MX	PLANNED MIX USE
PUD	PLANNED UNIT DEVELOPMENT
WPC	WATERFRONT PLANNED COMMUNITY
TOD	TRANSIT ORIENTED DEVELOPMENT



Information contained on this drawing is for graphical purposes only and is not meant to be used for engineering purposes.

SCALE	<p>CHARLES COUNTY GOVERNMENT Department of Planning and Growth Management 200 Baltimore St La Plata, MD 20646 (301)645-0627</p>	DATE
DRAWN BY:		January 2025
CHECKED BY:		

<p>Bayside Kia SDP 240001 ZONING MAP CHARLES COUNTY, MD</p>	<p>12</p>
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LEGEND:

PROPERTY BOUNDARY	---
ADJACENT PROPERTY BOUNDARY	---
EXISTING CONTOUR	--- 210 ---
NON-TIDAL WETLANDS	--- W ---
25' NON-TIDAL WETLAND BUFFER AND RPZ	--- W ---
SEWER MANHOLE	⊙
EXISTING SEWER LINE	--- S ---
PROPOSED SEWER LINE	--- S ---
EXISTING WATER LINE	--- W ---
PROPOSED WATER LINE	--- W ---
EXISTING GUARDRAIL	--- G ---
ASSIGNED 911 ADDRESS NUMBER	4090
FOREST CONSERVATION EASEMENT	--- F ---
PROPOSED CONTOUR	--- 209 ---
15% TO 25% SLOPES	--- S ---
25% AND GREATER SLOPES	--- S ---
EXISTING TREELINE	--- T ---
PROPOSED TREELINE	--- T ---
PROPOSED 5' CONCRETE SIDEWALK	--- S ---
EXISTING OVERHEAD ELECTRIC LINES	OHE
PROPOSED POLE LIGHT AND WALL LIGHT	⊙

ABBREVIATIONS:

CMP	CORRUGATED METAL PIPE
CO	SANITARY SEWER CLEANOUT
D.A.	DRAINAGE AREA
EDU	ESTIMATED DAILY USE
ELEC.	ELECTRIC
EX.	EXISTING
FH	FIRE HYDRANT
HB	HORIZONTAL BEND
HOPE	HIGH DENSITY POLYETHYLENE PIPE
HP	HIGH POINT
INV.	INVERT
LF	LIBERFOLIO
LP	LOW POINT
LOD	LIMIT OF DISTURBANCE
MAX.	MAXIMUM
MH	MANHOLE
M.P.H.	MILES PER HOUR
NDC	NOSE DOWN CURB
PB/PG	PLAT BOOK/PAGE
P.C.	POINT OF CURVATURE
P.T.	POINT OF TANGENCY
PROP.	PROPOSED
PVI	POINT OF VERTICAL INTERSECTION
RCP	REINFORCED CONCRETE PIPE
R.O.W.	RIGHT OF WAY
SF	SQUARE FEET
SSFC	SANITARY SEWER FLUSHING CONNECTION
SSV	SANITARY SEWER VALVE
STA.	STATION
SWM	STORMWATER MANAGEMENT
T.M./P.	TAX MAP/PARCEL
TRANS.	TRANSFORMER
TYP.	TYPICAL
UC	UTILITY CONDUIT
WM	WATER METER
WV	WATER VALVE

- GENERAL NOTES:**
- TAX MAP 15, GRID 13, P/O PARCEL 776, UNIT 1A
 - SITE AREA: 11.5576 AC.
 - ZONE: CC (COMMUNITY COMMERCIAL) WITH HC (HIGHWAY CORRIDOR) OVERLAY
 - DEVELOPMENT PROPOSAL: AUTOMOTIVE DEALERSHIP USE CODE 6.03.120.
- A. THIS USE IS PERMITTED WITH CONDITIONS IN THE CC, CB, CV, BP, PE, MX AND TOD ZONES, AND PERMITTED BY SPECIAL EXCEPTION IN THE CER ZONE, SUBJECT TO THE FOLLOWING REGULATIONS:
- (1) A LOT MAY BE USED FOR THE STORAGE, SALE AND RENTAL OF ONLY THE FOLLOWING VEHICLES: AUTOMOBILES, LIGHT TRAILERS OF SUCH LIMITED SIZE AND CAPACITY SO AS TO BE CAPABLE OF BEING SAFELY TOWED BY A PASSENGER MOTOR VEHICLE DESIGNED FOR CARRYING FEWER THAN 10 PASSENGERS, MANUFACTURED HOMES (CLASSES A AND B) AND LIGHT- AND MEDIUM-DUTY TRUCKS.
 - (2) GASOLINE PUMPS, OTHER SERVICE APPLIANCES, MAJOR REPAIRS, SPRAY PAINT OPERATION OR BODY OR FENDER REPAIR SHALL ONLY BE PERMITTED AS ACCESSORY USES.
 - (3) VEHICLES SHALL BE STORED OR PARKED ONLY WITHIN A HARD-SURFACED AREA CONSTRUCTED OF MATERIAL THAT WILL ASSURE A SURFACE RESISTANT TO EROSION AND ADEQUATELY TREATED TO PREVENT DUST EMISSION, SURROUNDED BY A RAISED CURB. THE CURB SHALL BE LOCATED SO THAT NO VEHICLE CAN BE PARKED OR STORED WITHIN 25 FEET OF ANY STREET LINE, NOR WITHIN 25 FEET OF ANY PROPERTY LINE ADJOINING LAND IN A RESIDENTIAL ZONE, NOR WITHIN THREE FEET OF ANY PROPERTY LINE.
 - (4) SIGNS, PRODUCT DISPLAYS, PARKED VEHICLES AND OTHER OBSTRUCTIONS THAT WOULD ADVERSELY AFFECT VISIBILITY AT INTERSECTIONS OR TO DRIVEWAYS SHALL BE PROHIBITED.
 - (5) LIGHTING SHALL BE LOW-LEVEL AND SO ARRANGED AS NOT TO REFLECT OR TO CAUSE GLARE INTO ANY RESIDENTIAL ZONE.
- WATER CATEGORY: W1
 - THIS SITE LIES IN SEPTIC TIER 1 AS PER THE SUSTAINABLE GROWTH & AGRICULTURAL PRESERVATION ACT TIER AREA DESIGNATION MAP DATED 7/12/16.
 - DEED REFERENCE: L 12432 F. 1702
 - ZONE REGULATIONS:
- | DESCRIPTION | REQUIRED | PROVIDED |
|------------------------------|-------------|--------------|
| AREA | 10,000 S.F. | 503,554 S.F. |
| FRONT B.R.L. (US 301) | 40 FEET | 40 FEET |
| FRONT B.R.L. GCE | 30 FEET | 30 FEET |
| REAR B.R.L. | 10 FEET | 10 FEET |
| SIDE B.R.L. | 6 FEET | 6 FEET |
| MAX. HEIGHT | 40 FEET | 30 FEET |
| INTENSITY (F.A.R.) | 0.40 | 0.07 |
| MAX. ISR | 0.85 | 0.47 |
| PROPOSED BUILDING FOOTPRINT: | - | 35,216 S.F. |
| PROPOSED PAVEMENT: | - | 193,561 S.F. |

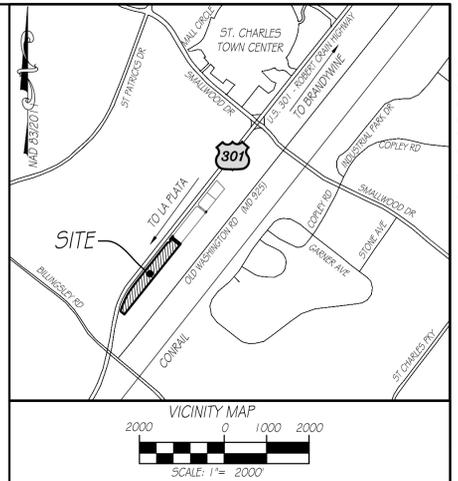
- PARKING REQUIRED PER USE CODE 6.03.100, MOTOR VEHICLE SALES OR RENTAL; MOBILE HOME SALES:
 - 1 SPACE PER 300 SQUARE FEET OF ENCLOSED SALES AREA, PLUS
 - 1 SPACE PER 1000 SQUARE FEET OPEN DISPLAY AREA, PLUS
 - 1 SPACE PER EMPLOYEE, PLUS
 - 2 SPACES PER SERVICE BAY
- PARKING PROVIDED:
 - ENCLOSED SALES AREA = 5,527 SF / 300 = 19 SPACES
 - OPEN DISPLAY AREA = 0 SF / 1000 = 0 SPACES
 - SERVICE BAYS = 30 X 2 = 60 SPACES
 - TOTAL NUMBER OF EMPLOYEES = 26 SPACES
 - TOTAL NUMBER OF PARKING SPACES = 105 SPACES
 - (NOTE: ALL OTHER DEDICATED SPACES ARE "VEHICLE STORAGE AREA")
- ACCESSIBLE PARKING PROVIDED: 4 SPACES (ONE MUST BE VAN ACCESSIBLE)
- ACCESSIBLE PARKING PROVIDED: 4 SPACES (4 ARE VAN ACCESSIBLE)
- HANDICAPPED ACCESSIBLE PARKING IS TO BE DESIGNED AND INSTALLED IN ACCORDANCE WITH THE CURRENT RULES AND REGULATIONS REGARDING COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND MARYLAND ACCESSIBILITY CODE (MAC). COMPLIANCE WILL BE BASED UPON PROVIDING THE MINIMUM NUMBER OF ACCESSIBLE PARKING SPACES, CORRECT DIMENSIONS, SIGNAGE, STRIPING, SLOPES, ETC. ANY INCONSISTENCIES IDENTIFIED ON SITE THAT ARE NOT MEETING ESTABLISHED REQUIREMENTS ARE TO BE CORRECTED BY THE OWNER/DEVELOPER PRIOR TO FINAL SITE APPROVAL.
- PER ARTICLE XX, SECTION 297-335, SUBSECTION A(1), THERE ARE NO EXTERIOR PARKING SPACES PROPOSED THAT WILL EXCEED THE MINIMUM NUMBER REQUIRED; THEREFORE, NO PERVIOUS PAVEMENT IS REQUIRED.
- PARKING SPACES AND DRIVE AISLES: 47,898 S.F.
- PER ARTICLE XX, SECTION 297-335, SUBSECTION O (4), ALL STORMWATER RUNOFF IS FILTERED THROUGH BIORETENTION FACILITIES IN ORDER TO PROVIDE MITIGATION FOR THE PARKING AREA AND DRIVEWAYS.
- SITE WILL BE REQUIRED TO PROVIDE ENVIRONMENTAL SITE DESIGN STORMWATER MANAGEMENT (ESDM). SWM WILL INCLUDE SUBMERGED GRAVEL WETLANDS. REFER TO CONCEPT SWM PLAN (CSWM-230007) APPROVED JUNE 8, 2023 AND SITE SWM PLAN (SSWM-230028) APPROVED AUGUST 29, 2024.
- THIS PROJECT IS REQUIRED TO COMPLY WITH THE CHARLES COUNTY FOREST CONSERVATION ORDINANCE. REFER TO FILE FC-230027.
- A DETAILED TRANSPORTATION ENGINEERING REVIEW OF THIS PLAN HAS NOT BEEN DONE AT THIS TIME. ROADS AND/OR ENTRANCES ARE TO BE DESIGNED IN ACCORDANCE WITH THE LATEST CHARLES COUNTY ROAD ORDINANCE, STANDARD SPECIFICATIONS FOR CONSTRUCTION MANUAL, AND STANDARD DETAIL MANUAL. APPROVAL OF THIS PLAN DOES NOT GRANT A VARIANCE, OR WAIVER, OF ANY ROAD ORDINANCE REQUIREMENTS, UNLESS SPECIFICALLY GRANTED IN WRITING. ANY DISCREPANCY BETWEEN THIS PLAN AND THE ORDINANCE WILL BE CORRECTED AT FINAL ENGINEERING.

- ARCHITECTURE AND LANDSCAPING IS TO BE SUBMITTED FOR REVIEW AND APPROVAL BY SDAR PRIOR TO BUILDING PERMIT APPROVAL. ARCHITECTURE AND LANDSCAPING WILL BE APPROVED BASED UPON CONSISTENCY WITH THE CHARLES COUNTY ZONING ORDINANCE.
- BOUNDARY INFORMATION SHOWN HEREON IS FROM DEEDS AND PLANS OF RECORD.
- TOPOGRAPHY SHOWN HEREON HAS BEEN OBTAINED FROM 2014 USGS CMG/LIDAR; POST SANDY, NCR (DC, MD, VA) WITH 2' BASE CONTOUR INTERVALS WITH NORTH AMERICAN VERTICAL DATUM 1988 (NAVD88).
- EXISTING UTILITIES AND OBSTRUCTIONS SHOWN ARE FROM THE BEST AVAILABLE RECORDS AND SHALL BE VERIFIED BY THE CONTRACTOR TO HIS SATISFACTION PRIOR TO CONSTRUCTION.
- WETLANDS ON-SITE WERE FIELD DELINEATED BY LORENZI, DODDS & GUNNILL, INC. IN 2017 AND 2024.
- THERE ARE APPROXIMATELY 2.8 ACRES OF NON-TIDAL WETLANDS ON THIS PROPERTY.
- THERE IS NO 100-YEAR FLOODPLAIN ON-SITE PER FIRM PANEL 2401700185C, EFFECTIVE SEPTEMBER 4, 2013.
- SCOPE OF WORK: THIS PLAN PROPOSES ONE (1) ONE-STORY BUILDING WITH PAVED PARKING LOT AND VEHICLE STORAGE AREA, PRIVATE STORMWATER MANAGEMENT FACILITIES, PUBLIC WATER AND SEWER SYSTEMS, AND LANDSCAPING.
- REQUIRED APPROVALS AND PERMITS:
 - CHARLES COUNTY PGM - PLANNING
 - CHARLES COUNTY PGM - DEVELOPMENT SERVICES
 - CHARLES SOIL CONSERVATION DISTRICT
 - IDE LETTER OF AUTHORIZATION (WETLANDS PERMIT)
 - IDE GENERAL PERMIT FOR STORMWATER (NOI)
 - SHA ACCESS PERMIT 24APC0004X
- VARIANCES REQUIRED / REQUESTED: NONE
- DESIGN WAIVERS OR EXCEPTIONS REQUESTED / OBTAINED: NONE
- THE APPROVAL OF THIS PLAN IS BASED ON THE APPROVAL OF THE DESIGN, CONSTRUCTION AND POINTS OF DISCHARGE OF THE WATER SUPPLY AND SEWAGE DISPOSAL SYSTEM BY THE DEPARTMENT OF THE ENVIRONMENT WHICH IS PLANNED FOR THE DEVELOPMENT AND WILL BE AVAILABLE WHEN NEEDED TO ALL LOTS OFFERED FOR SALE. BUILDING PERMITS MAY NOT BE ISSUED UNTIL THE PLANNED WATER AND SEWER FACILITIES ARE CONSTRUCTED AND DETERMINED TO BE ADEQUATE TO SERVE THE PROPOSED DEVELOPMENT BY THE APPROVING AUTHORITY. THE OWNER SHALL ENSURE THAT THE FACILITIES ARE MAINTAINED AND OPERATED SO AS TO PROVIDE FOR CONTINUOUS AND ACCEPTABLE WATER SUPPLY AND SEWAGE DISPOSAL SERVICE TO ALL PROPERTIES SERVED.
- THE PUBLIC WATER SYSTEM AND PUBLIC SEWER SYSTEM IS IN CONFORMANCE WITH THE CHARLES COUNTY COMPREHENSIVE WATER AND SEWER PLAN.
- BEARINGS AND COORDINATES SHOWN HEREON REFERENCE MARYLAND STATE PLANE COORDINATE SYSTEM, NORTH AMERICAN DATUM (NAD 83/2011).
- VERTICAL DATUM REFERENCES NAVD 88.
- BI-SIDE AUTO GROUP WILL BE SECURING ACCESS TO U.S. 301/CRAIN HIGHWAY IN TWO LOCATIONS AS SHOWN HEREON.

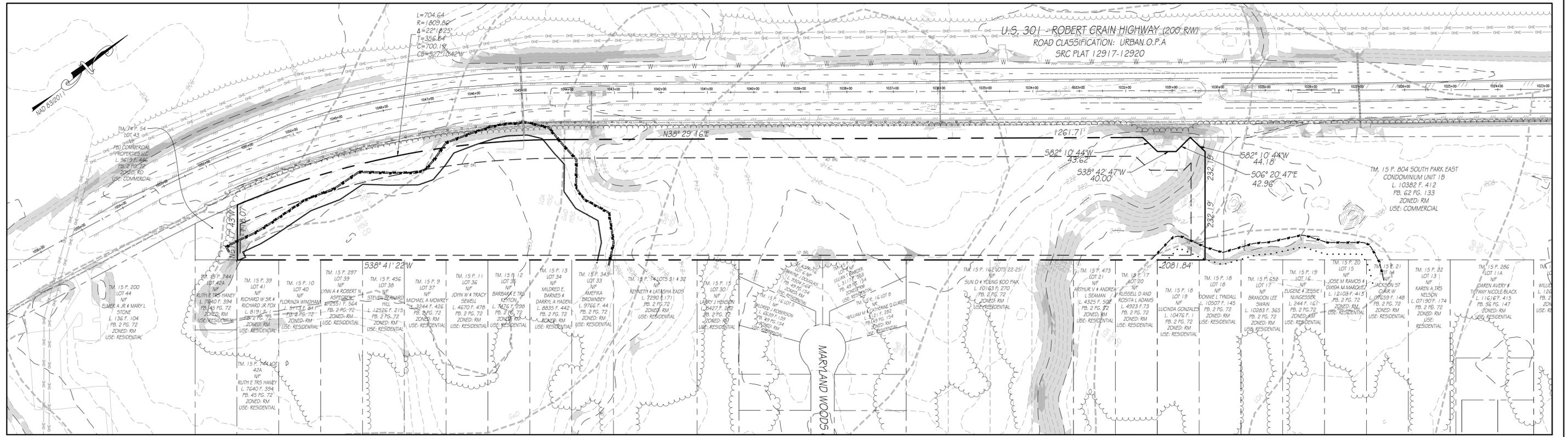
OWNER:
WANAMAKER PROPERTIES VII, LLC
415 RIVERSIDE DRIVE
PRINCE FREDERICK, MARYLAND 20678

APPLICANT:
BAYSIDE AUTO GROUP
1200 SOLOMOMONS ISLAND ROAD
PRINCE FREDERICK, MARYLAND 20678
ATTN: GEOFF WANAMAKER

ENGINEER:
LORENZI, DODDS, & GUNNILL, INC.
3475 LEONARDTOWN ROAD, SUITE 100
WALDORF, MARYLAND 20601
ATTN: TIM LESSNER



SDP-240001



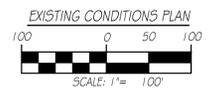
SOILS INFORMATION

SOIL MAP UNIT	SOIL DESCRIPTION	ERODIBILITY	HYDRIC	HSG
B ₁ C	BELTSVILLE SILT LOAM (5 TO 10 PERCENT SLOPES)	0.43	NO	C
B ₂ B	BELTSVILLE-GROSSSTOWN- WOODSTOWN COMPLEX (0 TO 5 PERCENT SLOPES)	0.37	NO	C
G ₁ D	GROSSSTOWN- WOODSTOWN-BELTSVILLE COMPLEX (5 TO 15 PERCENT SLOPES)	0.37	NO	C
LOA	LENNI AND QUINDOCQUA SOILS (0 TO 2 PERCENT SLOPES)	0.43	YES	CD

BAYSIDE KIA OF WALDORF

SITE DEVELOPMENT PLAN

6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND



PROFESSIONAL CERTIFICATE
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13310, Expiration Date: 1-30-25.

James C. Lorenzi, P.E. 9/3/24
Date

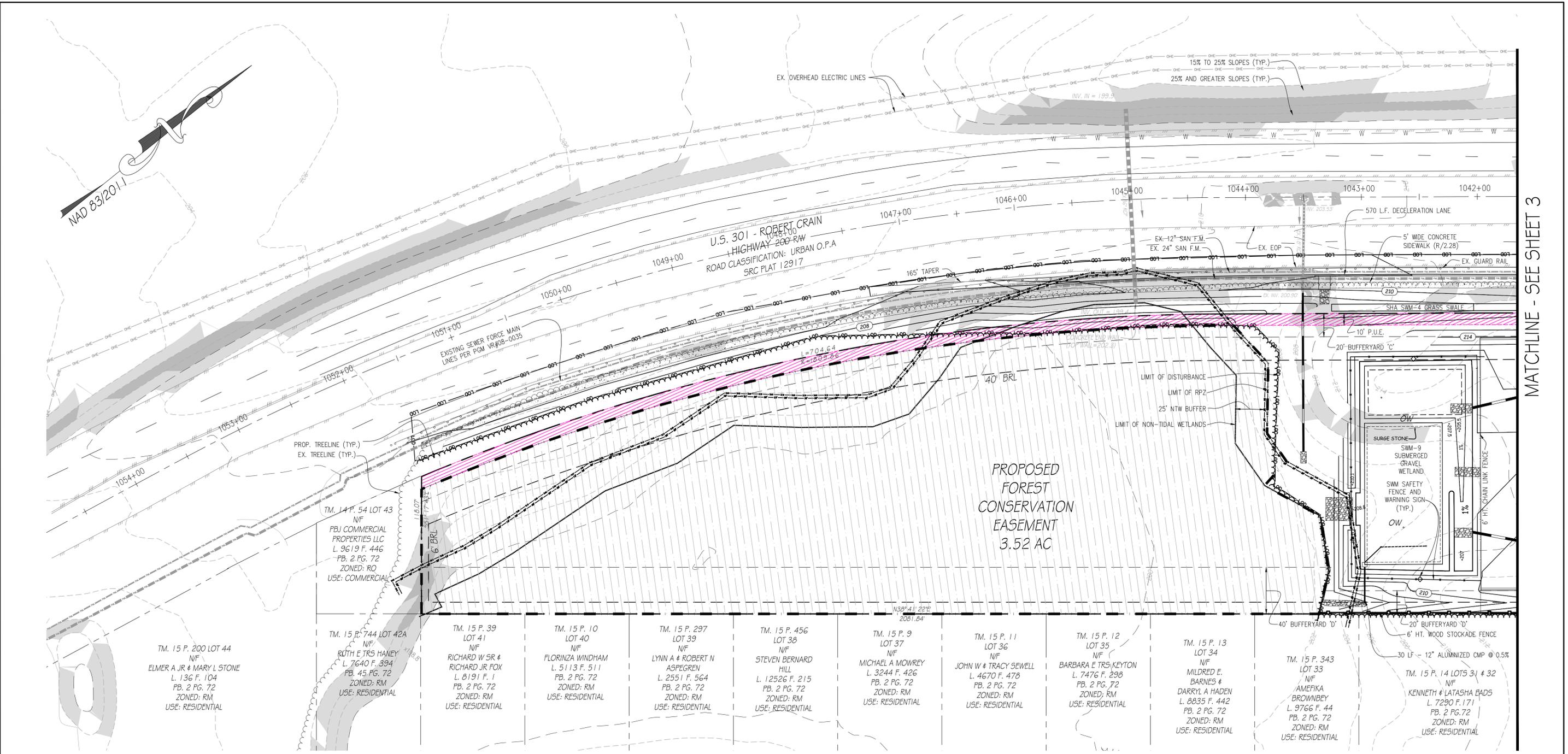


REVISIONS	
1.	PER PGM 6-26-24
2.	PER PGM 9-3-24

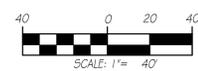
BAYSIDE KIA OF WALDORF	
TAX MAP 15, GRID 13, P/O PARCEL 776	
SOUTH PARK EAST CONDOMINIUM UNIT 1A	
6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND	
SITE DEVELOPMENT PLAN	
TITLE SHEET AND EXISTING CONDITIONS PLAN	
DRAWN	DECEMBER 2023
CHECKED	CONTRACT REFERENCE
219190	
LORENZI, DODDS, AND GUNNILL, INC.	
ENGINEERS - PLANNERS - SURVEYORS	
WWW.LDGENGINERS.COM	
SCALE	AS NOTED
1	
OF 12	

SDP-240001

MATCHLINE - SEE SHEET 3



MAD 8/20/11



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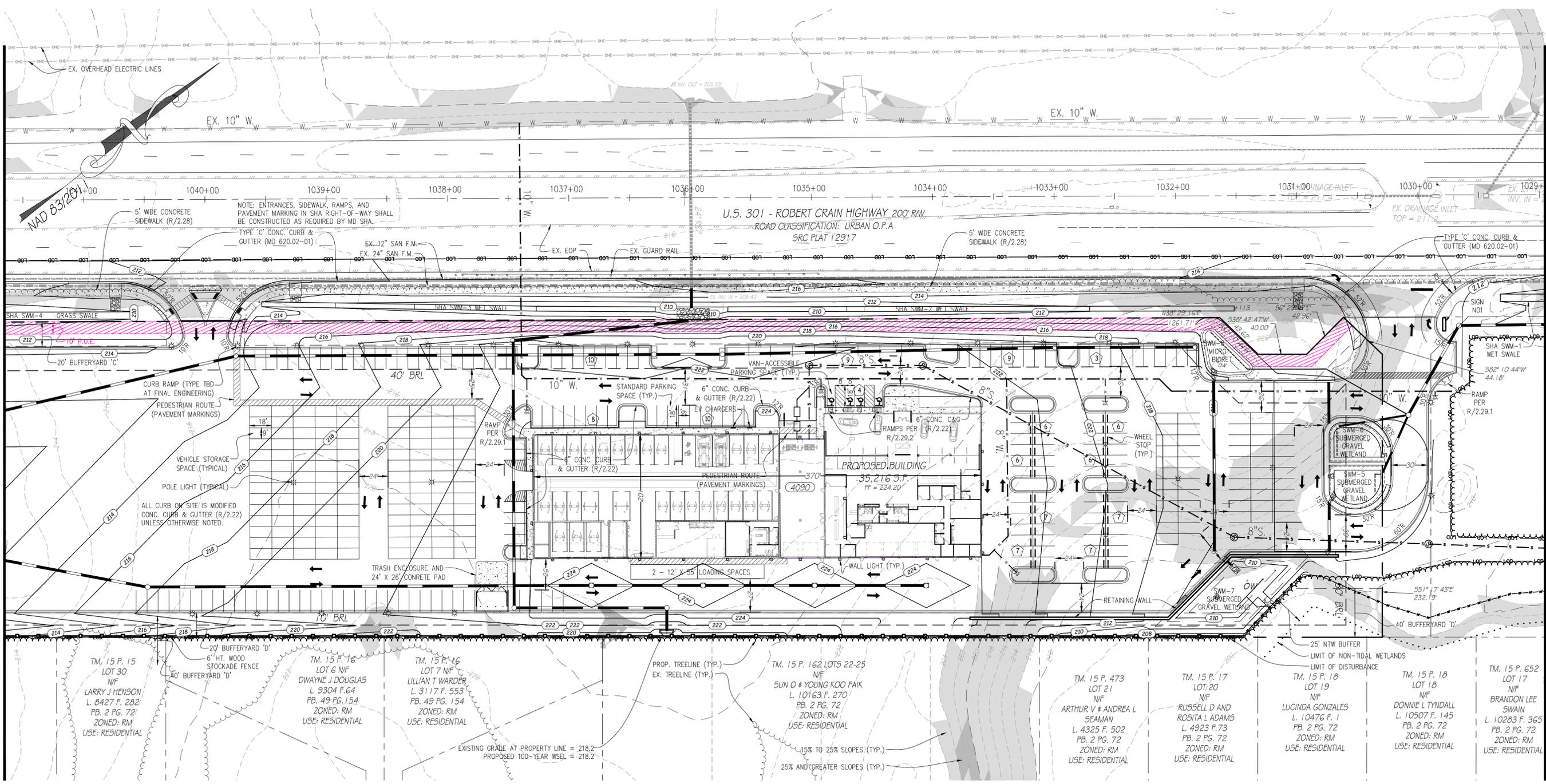
James C. Lorenzi
James C. Lorenzi, P.E. Date 9/3/24



REVISIONS 1. PER PGM 6-26-24 2. PER PGM 9-3-24	BAYSIDE KIA OF WALDORF TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND	SCALE 1" = 40'
DRAWN CHECKED DECEMBER 2023	CONTRACT REFERENCE 219190	2 OF 12
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM		

MATCHLINE - SEE SHEET 2

MATCHLINE - SEE SHEET 4



- PARKING LOT DESIGN NOTES**
- PARKING SPACES REQUIRED: 105
 - PARKING SPACES PROVIDED: 105 (101 STANDARD + 4 VAN-ACCESSIBLE)
 - ALL STANDARD PARKING SPACES ARE 9' X 18'
 - ALL VAN-ACCESSIBLE PARKING SPACES ARE 8' X 18' WITH 8' WIDE ACCESS AISLE.
 - NUMBER OF PARKING SPACES PER BAY IS NOTED THUS: (8)
 - ALL DRIVE AISLES ARE MIN. 24' WIDE. SEE PLAN FOR DIMENSIONS.
 - ALL PARKING BUMP-OUTS AND ISLANDS HAVE A 5' RADIUS UNLESS OTHERWISE NOTED.
 - WHERE CURB IS PROPOSED, RADIUS REFERS TO FLOWLINE.

STREET SIGN SCHEDULE

QTY.	SYM.	SIGN TEXT	SIZE	MUTCD NO.
2	NP	"NO PARKING IN ACCESS AISLE"	12" X 18"	N/A
4	RP	"RESERVED PARKING"	12" X 18"	R7-8
4	*	"VAN-ACCESSIBLE"	12" X 6"	R7-8a
4	*	"MAXIMUM FINE \$500"	12" X 9"	N/A

* TO BE PROVIDED AT EACH LOCATION MARKED [RP].



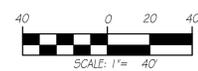
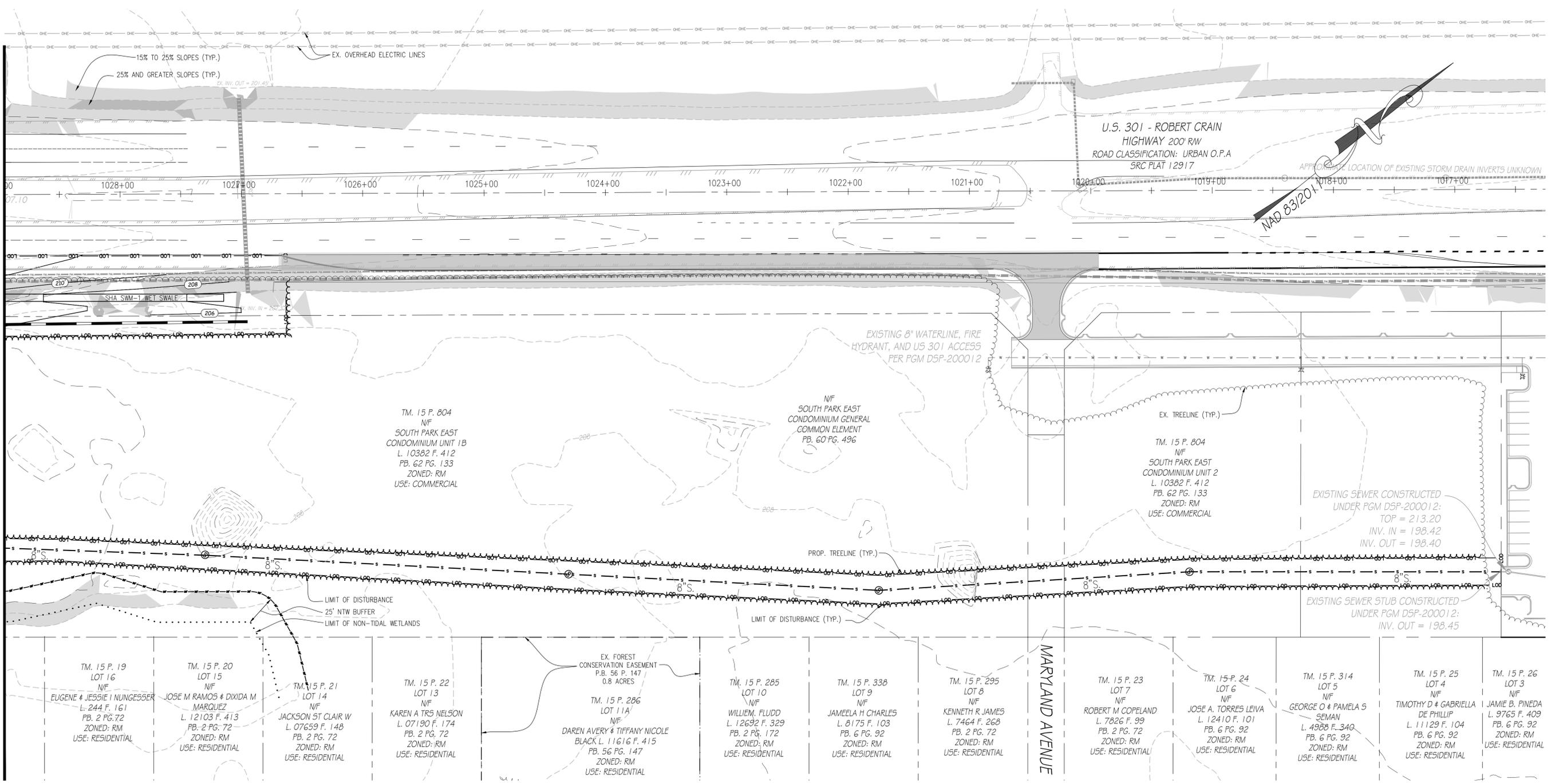
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James C. Lorenzi
James C. Lorenzi, P.E. 9/3/24
Date



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DRAWN CHECKED DECEMBER 2023		CONTRACT REFERENCE 219190		3 OF 12
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS - PLANNERS - SURVEYORS WWW.LDGENGINEERS.COM				

MATCHLINE - SEE SHEET 3



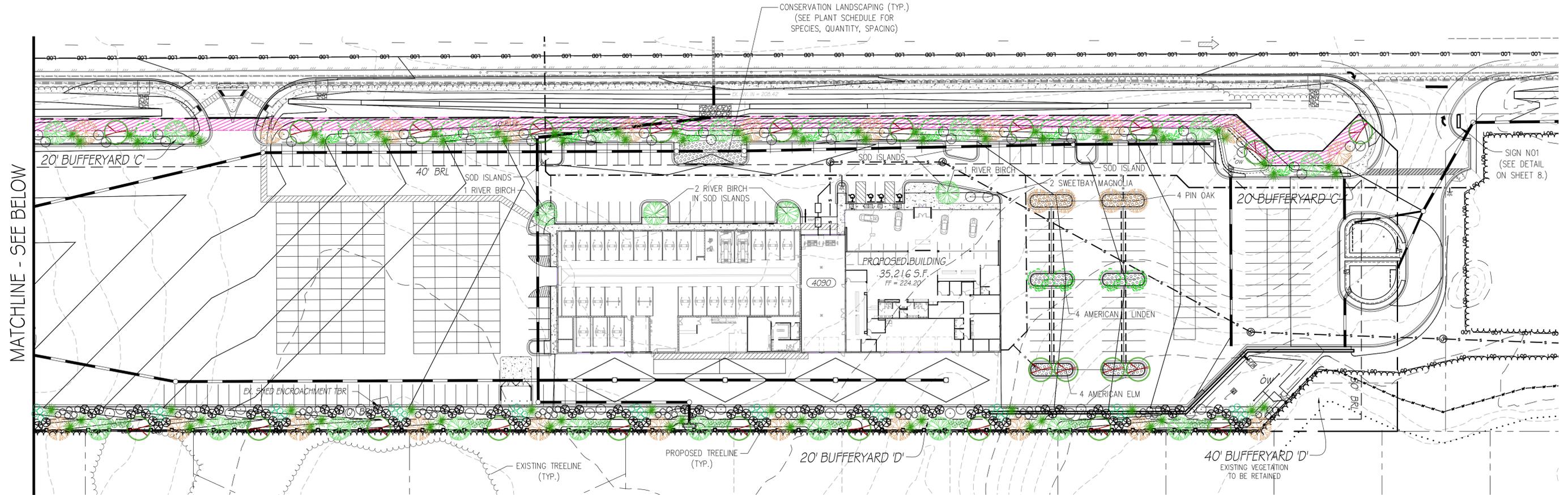
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James C. Lorenzi
James C. Lorenzi, P.E. 9/3/24 Date



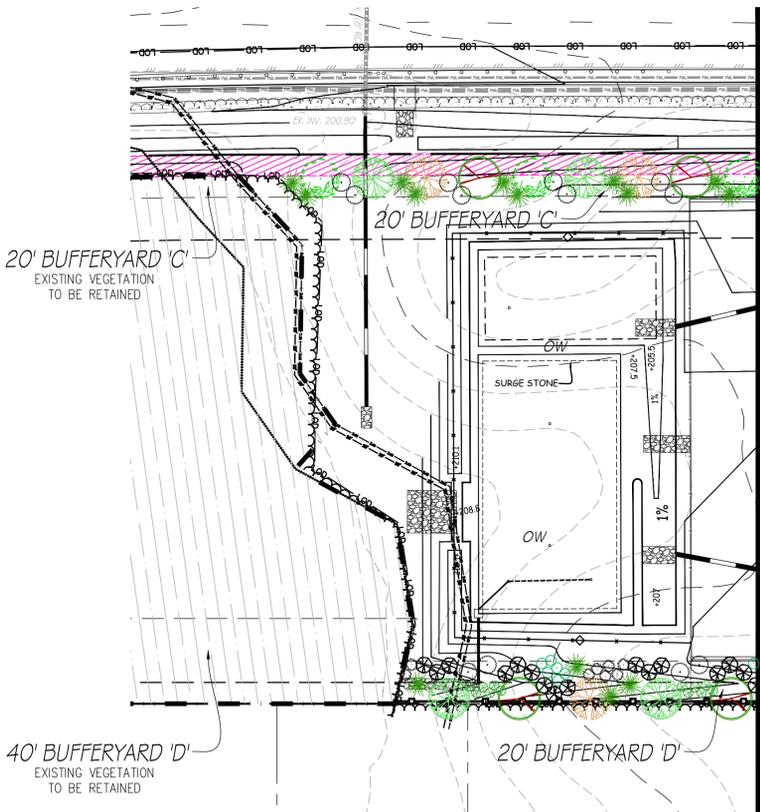
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DRAWN DECEMBER 2023 CHECKED		CONTRACT REFERENCE 219190		4 OF 12
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM				

SDP-240001

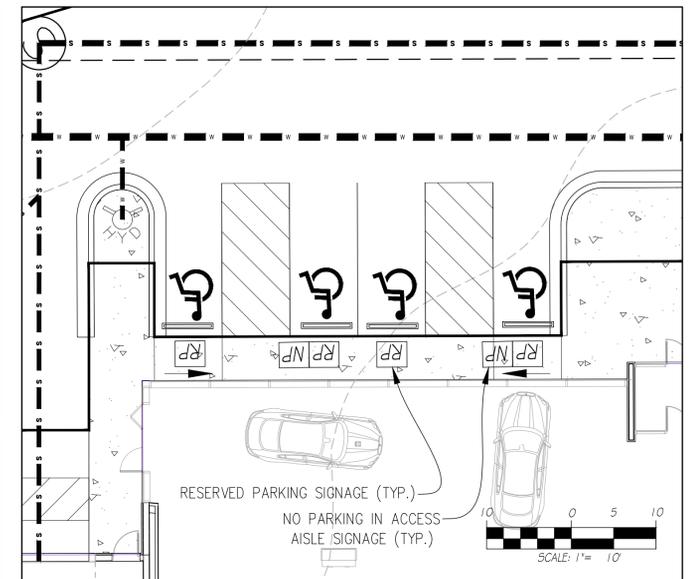
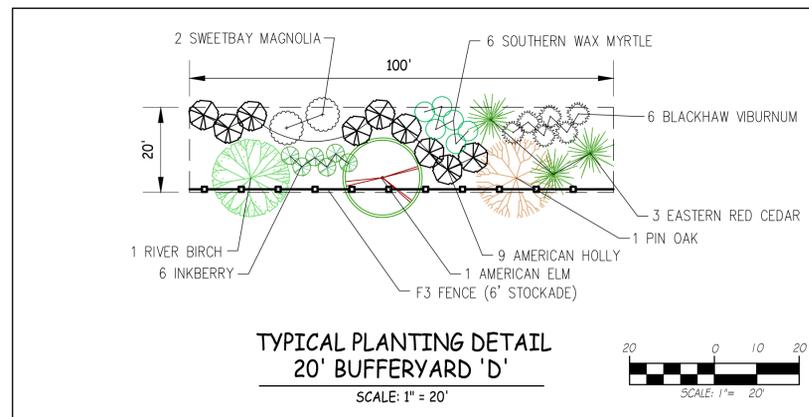
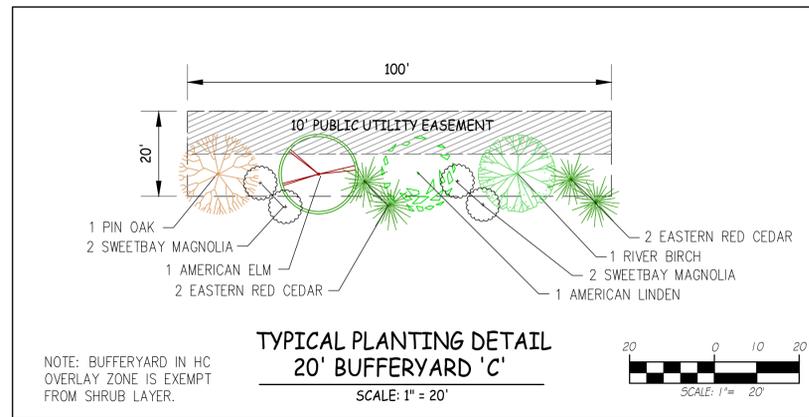
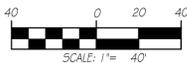
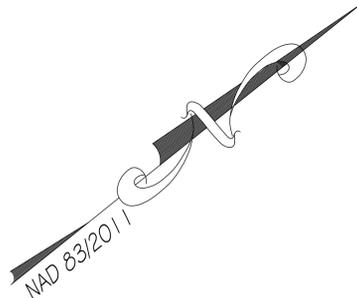


LANDSCAPING AND SIGNAGE PLAN

SCALE: 1" = 40'



MATCHLINE - SEE ABOVE



SIGNAGE PLAN INSET

<p>REVISIONS</p> <p>1. PER PGM 6-26-24</p> <p>2. PER PGM 9-3-24</p>	<p>BAYSIDE KIA OF WALDORF</p> <p>TAX MAP 15, GRID 13, P/O PARCEL 776</p> <p>SOUTH PARK EAST CONDOMINIUM UNIT 1A</p> <p>6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND</p>	<p>SCALE AS NOTED</p>
<p>DRAWN DECEMBER 2023</p> <p>CHECKED</p>	<p>CONTRACT REFERENCE 219190</p>	<p>5</p>
<p>LORENZI, DODDS, AND GUNNILL, INC.</p> <p>ENGINEERS ~ PLANNERS ~ SURVEYORS</p> <p>WWW.LDGENGINEERS.COM</p>		<p>OF 12</p>



Permit Number	SDP-240001	Project Name	BAYSIDE KIA
Parking and Landscaping Worksheet: (This worksheet is for the purpose of implementing Articles XX and XXI)			
A. Number of total parking spaces provided.	A	105	
B. Minimum number of parking spaces required.	B	105	
C. Number of parking spaces above minimum to be pervious.	C	0	
D. Are the correct number of landscape islands provided? < 100 spaces: 1 island/ 7 spaces >=100 spaces: 1 island/ 10 spaces bioretention: 1 island/ 15 spaces (detail and location provided)	D	N/A	Yes No
E. Are landscape islands sized correctly? < 30,000 sq. ft.: max. size of 1,500 sq. ft., unless retaining trees or in excess > 30,000 sq. ft.: max. size of 3,500 sq. ft., unless retaining trees or in excess	E	N/A	Yes No
F. Do all islands have a minimum soil area of 153 sq. ft. (8 1/2' x 18')?	F	N/A	Yes No
G. Is one walkway across center row island provided per 10 parking spaces?	G	N/A	Yes No
H. Area of proposed parking area (Parking area includes parking spaces, drive aisles, and driveways. Actual measured paved area or 400 sq ft/space, whichever is greater.)	H	47898	Sq ft
I. Impervious surface mitigation proposed for parking area: a. Pervious surface b. Existing on-site forest retained in addition to Forest Conservation requirements(>10,000 sq. ft.) c. Pocket parks integrated into parking areas (>2,500 sq. ft. each) d. Stormwater filtered through bioretention e. Structural green roof f. Structured parking	I	47898	Sq ft
J. Is item I subtotal > or = item H? If so, this code has been met.	J	Yes	
Stop here if all proposed parking is located on private residential lots.			
K. Multiply item H by 0.10 to determine interior landscaping required:	K	4790	Sq ft

L. Measure interior landscaping provided:	L	5378	Sq ft
M. Is item L > or = item K? If so, you have met this code.	M	Yes	Sq ft
N. Multiply (Item H - structured parking) by 0.35 for area required to be shaded	N	16764	Sq ft
O. Calculate shade provided by trees using the following: Canopy trees 2 1/2" dbh within vehicle accommodation area: Canopy trees 2 1/2" dbh within 15' of paved area: Understory trees 1 1/2" dbh within vehicle accommodation area: Understory trees 1 1/2" dbh within 15' of paved area:	O	25614	Sq ft
P. Is item O > or = item N? If so, this code has been met.	P	Yes	
Q. Multiply item K by 0.5 to determine conservation landscaping required:	Q	2395.00	Sq ft
R. Measure the areas where conservation landscaping is provided:	R	2968	Sq ft
S. Is item R > or = item Q? If so, this code has been met.	S	Code Met	
T. Is existing vegetation retained to meet landscaping and bufferyard requirements wherever possible and a min. of 12 1/2' from edge of paving?	T	Yes	No
U. Are 90% of the parking trees a native species?	U	N/A	Yes No
V. Are all of the bufferyard trees a native species?	V	Yes	No
W. Are all of the conservation landscaping plants native species?	W	Yes	No
X. Are all non-invasive species used for required plantings?	X	Yes	No
Y. Is a maximum of 1/3 trees a single species?	Y	Yes	No
Z. Is a detail provided to show soil amended in interior landscape areas to 2 1/2" depth	Z	Yes	No
AA. Are trees planted a minimum of 4' from the edge of paving and surrounded by a minimum of 200 sq. ft. of soil area?	AA	Yes	No

TABULATION OF REQUIRED BUFFERYARD PLANTS

REQUIRED BUFFERYARD	REQUIRED BY	LOCATION	LENGTH	P.U. PER 100 L.F.	CANOPY	UNDERSTORY	SHRUBS	EVERGREENS
20' BUFFERYARD 'C'	FIGURE X-1 (ROAD BUFFER)	US 301 FRONTAGE (EXCLUDES PROPOSED ENTRANCE AND AREA OUTSIDE L.O.D.)	1300 L.F.	4 CANOPY TREES 8 UNDERSTORY TREES 12 SHRUBS (HC BUFFER EXEMPT FROM SHRUB LAYER)	52	-	-	-
20' BUFFERYARD 'C'	FIGURE X-1 (ROAD BUFFER)	US 301 FRONTAGE (AREA OUTSIDE L.O.D.)	738 L.F.	4 CANOPY TREES 8 UNDERSTORY TREES 12 SHRUBS (EX. VEGETATION TO BE RETAINED)	N/A	-	-	-
20' BUFFERYARD 'D'	297-222 B (1)	ADJACENT TO SOUTHEASTERN PROPERTY LINE (EXCLUDES AREA OUTSIDE L.O.D.)	1199 L.F.	3 CANOPY TREES 5 UNDERSTORY TREES 18 SHRUBS 9 EVERGREENS F3 FENCE (6' STOCKADE)	36	60	216	108
40' BUFFERYARD 'D'	297-222 B (1)	ADJACENT TO SOUTHEASTERN PROPERTY LINE (AREA OUTSIDE L.O.D.)	876 L.F.	2 CANOPY TREES 4 UNDERSTORY TREES 14 SHRUBS 9 EVERGREENS (EX. VEGETATION TO BE RETAINED)	N/A	-	-	-
TOTAL					88	164	216	108

NOTE: IF POSSIBLE, EX. VEGETATION MAY BE RETAINED IN ANY BUFFERYARD AREA TO FULFILL BUFFERYARD PLANT UNIT REQUIREMENTS.

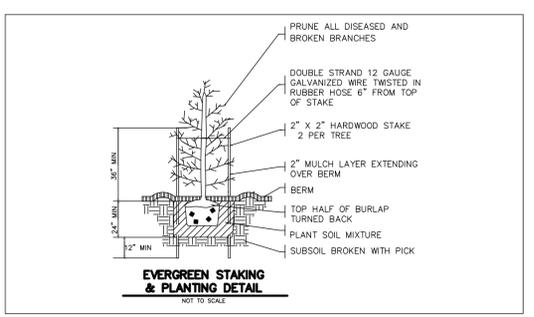
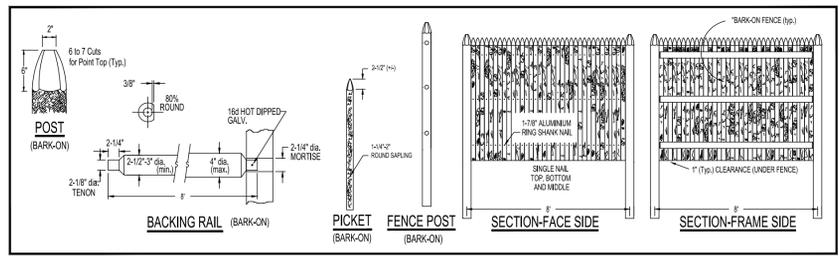
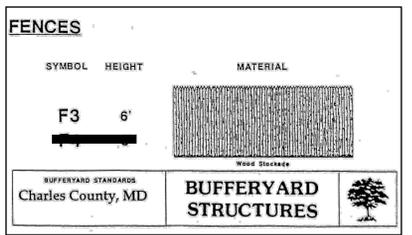
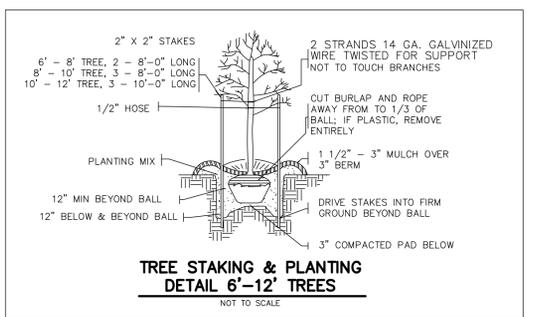
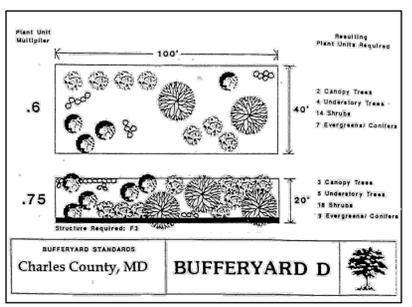
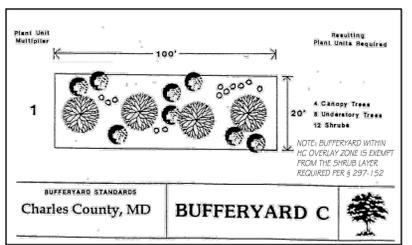
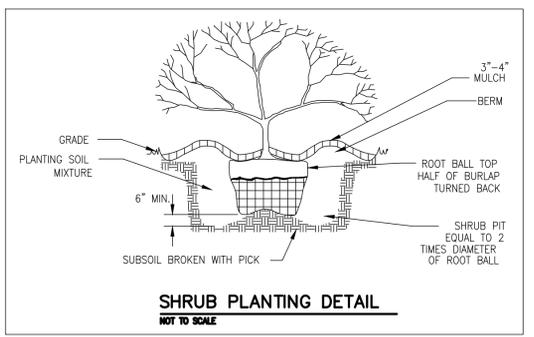
PLANT SCHEDULE FOR BUFFERYARDS AND PARKING LOT

SYMBOL	SPECIES	SIZE AND CONDITION	QUANTITY	LOCATION (TYPE)
	<i>Echinacea purpurea</i> PURPLE CONEFLOWER	1 GAL. CONT.	125	PARKING LOT (CONSERVATION LANDSCAPING)
	<i>Chamaecrista fasciculata</i> PARTRIDGE PEA	1 GAL. CONT.	125	NOTE: EQUAL NUMBER OF EACH SPECIES TO BE PLANTED IN A RANDOM PATTERN IN EACH PARKING LOT ISLAND, SPACED AT APPROXIMATELY 3 FT. O.C.
	<i>Sorghastrum nutans</i> INDIAN GRASS	1 GAL. CONT.	125	
	<i>Betula nigra</i> RIVER BIRCH	4-5 FT. HT., B&B MULTI-STEMMED	29	BUFFERYARD, PARKING LOT (CANOPY TREE)
	<i>Ilex opaca</i> AMERICAN HOLLY	4-5 FT. HT., CONT.	108	BUFFERYARD (EVERGREEN)
	<i>Ilex glabra</i> INKBERRY	5 GAL. CONTAINER	72	BUFFERYARD (SHRUB)
	<i>Juniperus virginiana</i> EASTERN RED CEDAR	3-4 FT. HT., CONT.	88	BUFFERYARD (UNDERSTORY TREE)
	<i>Magnolia virginiana</i> SWEETBAY MAGNOLIA	4-5 FT. HT., CONT.	78	BUFFERYARD, PARKING LOT (UNDERSTORY TREE)
	<i>Myrica cerifera</i> SOUTHERN WAX MYRTLE	5 GAL. CONTAINER	72	BUFFERYARD (SHRUB)
	<i>Quercus palustris</i> PIN OAK	2-1/2 IN. CAL., B&B	29	BUFFERYARD, PARKING LOT (CANOPY TREE)
	<i>Tilia americana</i> AMERICAN LINDEN	2-1/2 IN. CAL., B&B	17	BUFFERYARD, PARKING LOT (CANOPY TREE)
	<i>Ulmus americana</i> AMERICAN ELM	2-1/2 IN. CAL., B&B	29	BUFFERYARD, PARKING LOT (CANOPY TREE)
	<i>Viburnum prunifolium</i> BLACKHAW VIBURNUM	3 GAL. CONTAINER	72	BUFFERYARD (SHRUB)

NOTE: SUBSTITUTIONS ARE PERMITTED WITH CHARLES COUNTY PLANNING DEPARTMENT APPROVAL. SPECIES OF ALL PROPOSED PLANTS SHALL BE NATIVE TO MARYLAND.

PLANTING NOTES

- THE LANDSCAPE CONTRACTOR SHALL REVIEW ARCHITECTURE/ENGINEERING PLANS TO BECOME FAMILIAR WITH ALL EXISTING AND PROPOSED UTILITIES. LANDSCAPE CONTRACTOR SHALL LOCATE ALL UTILITIES, STRUCTURES, PAVEMENT, AND VEGETATION TO REMAIN AND TAKE THE NECESSARY PRECAUTIONS FOR THEIR PROTECTION DURING LANDSCAPE INSTALLATION. UTILITIES SHOWN ON PLANS ARE APPROXIMATE. CONTRACTOR IS RESPONSIBLE FOR LOCATING UTILITIES. ANY FIELD ADJUSTMENT REQUIRED SHALL BE REVIEWED BY THE LANDSCAPE ARCHITECT PRIOR TO IMPLEMENTATION.
- ALL PLANTING MATERIALS, SIZE, AND PLANTING METHODS ARE TO CONFORM TO AMERICAN STANDARD FOR NURSERY STOCK BY THE AMERICAN ASSOCIATION OF NURSEYMEN. ALL PLANTING SPECIFICATIONS SHALL CONFORM TO THE LANDSCAPE SPECIFICATIONS GUIDELINES FOR BALTIMORE-WASHINGTON AREAS PUBLISHED BY THE LANDSCAPE CONTRACTORS ASSOCIATION OF METROPOLITAN WASHINGTON.
- ALL PLANT MATERIALS ARE REQUIRED TO BE INSPECTED FOR THE USE AND OCCUPANCY PERMIT, AND SHALL BE LABELED WITH THE BOTANICAL (LATIN) NAME AND BONDED FOR TWO (2) YEARS.
- A TOPSOIL MIX OF 1 PART SAND, 3 PARTS LOAM, AND 1 PART PEAT SHALL BE USED FOR BACKFILL IN PLANT INSTALLATION.
- A MINIMUM 3" DEPTH OF PINE BARK MULCH SHALL BE INSTALLED IN ALL PLANTING BEDS AND AROUND ALL TREES (TO A MINIMUM 4-FOOT DIAMETER RING). IN THE ISLANDS DESIGNATED AS CONSERVATION ISLANDS, GROUND COVER PLANTINGS, IN PLACE OF THE 3" MULCH, SHALL BE USED. TOPSOILING, SEEDING, AND SODDING SHALL CONFORM TO SCD SPECIFICATIONS.
- ALL SOILS IN INTERIOR LANDSCAPE AREAS SHALL BE LOOSENED AND AMENDED WITH 1/3 ORGANIC MATTER TO A DEPTH OF 2.5'.
- PLANT MATERIAL AND LOCATION SUBSTITUTIONS/CHANGES NEED TO BE APPROVED BY THE CHARLES COUNTY PGM PLANNING DIVISION.
- SEE CHARLES COUNTY SPECIFICATIONS FOR ADDITIONAL LANDSCAPE RELATED REQUIREMENTS.
- FINAL GRADING AND TOPSOILING SHALL BE COMPLETED PRIOR TO PLANTING OR PLANTING LAYOUT.
- UNLESS NOTED, TREE AND PLANTING BED LOCATIONS AS SHOWN ON THESE PLANS ARE FOR GUIDANCE ONLY. INDIVIDUAL PLANT LOCATIONS MAY BE ADJUSTED AS NECESSARY IN FIELD BY LANDSCAPE CONTRACTOR. QUANTITIES MUST BE MAINTAINED.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ADVISE THE LANDSCAPE ARCHITECT OF ANY CONDITION FOUND ON THE SITE WHICH PROHIBITS INSTALLATION AS SHOWN ON THESE DRAWINGS.
- REFER TO LANDSCAPE PLAN AND PLANT SCHEDULES.
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR FINE GRADING, REMOVAL OF MISCELLANEOUS DEBRIS, AND ANY ADDITIONAL FILL REQUIRED TO CREATE A SMOOTH CONDITION PRIOR TO PLANTING IN ALL LANDSCAPED AREAS.
- MINIMUM DISTANCE BETWEEN CENTER OF SHRUB, GRASS, OR PERENNIAL PLANT AND CURB OR SIDEWALK SHALL BE 18 INCHES.
- AT STORMWATER MANAGEMENT FACILITIES, MINIMUM DISTANCE BETWEEN CENTER OF SHRUB, GRASS, OR PERENNIAL PLANT AND ROOF DRAIN OUTLET OR STORM DRAIN INLET SHALL BE 3 FEET.
- FOLIAGE OF ALL WOODY TREES AND SHRUBS TO BE MINIMUM 3 FEET FROM BUILDING AT MATURITY. COORDINATE FINAL LOCATIONS OF ALL PLANTS IN CLOSE PROXIMITY TO BUILDING WITH OWNER, LANDSCAPE ARCHITECT.



NOTE: WHERE BUFFERYARD REQUIREMENTS ARE SATISFIED BY EXISTING VEGETATION LOCATED WITHIN THE BUFFER, IF AT TIME OF INSPECTION EXISTING VEGETATION IS DEEMED INSUFFICIENT, THE DEVELOPER WILL PROVIDE PLANTINGS CONSISTENT WITH THE BUFFERYARD DETAILS ILLUSTRATED ABOVE.



REVISIONS	BAYSIDE KIA OF WALDORF TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND		SCALE N/A
1. PER PGM 6-26-24	SITE DEVELOPMENT PLAN LANDSCAPE DETAILS AND NOTES		
DRAWN CHECKED	DECEMBER 2023	CONTRACT REFERENCE	219190
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM			6 OF 12

6" COMBINATION CURB & GUTTER

6" COMBINATION CURB & GUTTER

MODIFIED COMBINATION CURB & GUTTER

NOTES:

- MAX JOINT SPACING FOR CONCRETE CURB AND COMBINATION CURB & GUTTER = 10'
- SEE SPECIFICATIONS FOR LOCATION AND DESCRIPTION OF TREATMENT FOR THE USE OF MODIFIED COMBINATION CURB AND GUTTER TO BE USED ONLY ON LOCAL ROADS OR MINOR COLLECTOR ROADS.
- 6" AND MODIFIED COMBINATION CURB AND GUTTER TO BE USED ONLY ON LOCAL ROADS OR MINOR COLLECTOR ROADS.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
COMBINATION CURB & GUTTER
REVISIONS:
R 2.22

CONCRETE SIDEWALK

DETAIL A (SEE NOTE 5)

PLAN: SIDEWALK PASSING DETAIL

NOTES:

- SIDEWALK TO BE SCRIBED IN 5' MAXIMUM SQUARES.
- EXPANSION JOINTS ACROSS THE SIDEWALK NOT TO BE MORE THAN 15' APART.
- 1/2" PREFORMED BITUMINOUS EXPANSION MATERIAL IN EXPANSION JOINTS TO BE KEPT 1/4" BELOW SURFACE OF SIDEWALK.
- CONCRETE TO BE MIX NO. 2.
- SIDEWALK MAY ABUT CURB WITH PERMISSION OF THE HIGHWAY ENGINEER. WHEN SIDEWALK ABUTS CURB, WALK SHALL BE A MINIMUM WIDTH OF 5' FROM BACK OF CURB, AND 1/4" ABOVE CURB WITH 1/2" PREFORMED BITUMINOUS EXPANSION MATERIAL BETWEEN SIDEWALK AND CURB, AND RESTING ON A COMPACTED CRUSHED STONE BASE. SEE DETAIL "A" THIS SHEET.
- ON LONGITUDINAL SIDEWALK GRADES OF 3% OR GREATER, A CONCRETE HEADER, 6" THICK AND 6" DEEP BELOW THE NORMAL 4" SIDEWALK THICKNESS SHALL BE CONSTRUCTED FOR THE FULL WIDTH OF THE SIDEWALK AT INTERVALS OF 48 FEET. THE HEADERS SHALL BE PLACED AT EXPANSION JOINT LOCATIONS AND SHALL BE MONOLITHIC WITH THE SIDEWALK.
- WHERE 5' SIDEWALK WIDTH IS NOT PROVIDED, PASSING ZONES SHALL BE CONSTRUCTED EVERY 200'. SEE SIDEWALK PASSING DETAIL THIS SHEET. DRIVEWAY APPROXS WITH 2% SLOPE CAN MEET THIS REQUIREMENT.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
CONCRETE SIDEWALK
REVISIONS:
R 2.28

PEDESTRIAN RAMP

SECTION A-A

PLAN

NOTES:

- RIGHT-OF-WAY TRUNCATION TO BE SET 20' FROM P.I. ALONG EACH OF THE INTERSECTING LINES.
- MINIMUM DISTANCE BETWEEN BACK OF SIDEWALK AND RIGHT-OF-WAY LINE SHALL BE ONE FOOT.
- SURFACE TEXTURE OF CONCRETE RAMP & FLARES SHALL BE COARSE BROOKING OR OTHER NON-SLIP TYPE SURFACE.
- STANDARD COMBINATION CURB AND GUTTER IS SHOWN. DETAILS TO BE SIMILAR FOR MODIFIED CURB & GUTTER EXCEPT THAT FLOW LINE UP IS TO BE OMITTED.
- THIS DETAIL CANNOT BE USED WHERE SIDEWALK IS ADJACENT TO CURB.
- AT LEAST ONE PEDESTRIAN RAMP SHALL BE PROVIDED ACROSS FROM T-INTERSECTIONS WHERE THERE IS A REVERSING SIDEWALK.
- MINIMUM LENGTH OF RAMP: L = 5' FOR MODIFIED CURB, L = 6' FOR 6" CURB, AND L = 11' FOR 8" CURB.
- SIDEWALK TRANSITIONS TO PROVIDE 5' x 5' MINIMUM LANDING AREA.
- TRANSITION PANELS TO BE INTO EXISTING SIDEWALK MUST BE A MINIMUM OF 5' IN LENGTH.
- RAMP AND SIDEWALK SHALL MEET ADA REQUIREMENTS.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
PEDESTRIAN RAMP
REVISIONS:
R 2.29.1

PEDESTRIAN RAMP SIDEWALK ABUTMENT CURB

SECTION A-A

PLAN

NOTES:

- TO BE USED WHERE SIDEWALK ABUTS THE CURB. THIS STANDARD MAY BE MODIFIED TO SUIT A PARTICULAR LOCATION.
- NO TRAVERSABLE SLOPE ON THE RAMP OR SIDEWALK SHALL EXCEED 1:1 IN THE DIRECTION OF PEDESTRIAN TRAVEL OR 4:1 PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL. THE CROSS-SLOPE OF THE LANDING AREA CANNOT EXCEED GRADE OF ROADWAY.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
PEDESTRIAN RAMP SIDEWALK ABUTMENT CURB
REVISIONS:
R 2.29.2

TYPICAL CLOSED SECTION STREET CONNECTION ENGINEERING ACCESS PERMITS

NOTES:

- MINIMUM CHANNELIZATION LIMITS IS SHOWN, ASSUMING AN OPEN SECTION. RURAL HIGHWAY, REFER TO 13-2.
- TURNING LANES AND SHOULDERS ARE NOT SHOWN.
- REFER TO 12-0 FOR STREET CONNECTION STANDARDS.

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TYPE 'C' CURB

TYPE 'D' CURB

TYPE 'E' CURB

NOTES:

- SLOPE GUTTER PAN 1/4" PER FOOT TOWARD FLOW LINE ON ALL ROADWAYS INCLUDING SUPERELEVATED SECTIONS, EXCEPT INTERCHANGE RAMPS.
- ROADWAY PAVEMENT SLOPE.
- ROADWAY PAVEMENT CONSTRUCTION JOINT.
- PROVIDE LONGITUDINAL THE DEVICE "J" BAR MODIFIED. REFER TO STANDARD NO. MD 572-61.
- FLOW LINE.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
COMBINATION CURB & GUTTER
REVISIONS:
R 2.22

PRECAST CONCRETE WHEEL STOPS

ELEVATION TYPE I (AUTOMOBILE)

SECTION A-A (TYPICAL STOP) - 6" DIA.

ELEVATION TYPE II (TRUCK)

SECTION B-B

ELEVATION TYPE III-A (TRUCK)

SECTION C-C

ELEVATION TYPE III-B (TRUCK)

NOTES:

- PRECAST CONCRETE WHEEL STOPS SHALL BE LOCATED AS SHOWN ON THE PLANS, THEN SECURED IN PLACE WITH TWO (2) NO. 7 REINFORCEMENT BARS PER WHEEL STOP.
- COST OF THE REINFORCEMENT BARS WILL BE INCIDENTAL TO THE CONTRACT UNIT PRICE BID PER EACH FOR THE WHEEL STOPS.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
PRECAST CONCRETE WHEEL STOPS
REVISIONS:
R 2.28

DETECTABLE WARNING SURFACES

MAT DETAILS

DOME SPACING

DOME SECTION

PLACEMENT GUIDELINES

NOTES:

- THE DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 TO 8 INCHES FROM THE FACE OF CURB.
- FOR SLOPED APPLICATIONS DETECTABLE WARNING SHALL BE PLACED SUCH THAT THE DOMES CLOSEST TO THE BACK OF CURB ARE NO LESS THAN 0.5' AND NO MORE THAN 3.0' FROM THE BACK OF CURB. TRUNCATED DOME SURFACES SHALL BE FABRICATED TO PROVIDE FULL DOMES ONLY.
- DETECTABLE WARNING SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH SECTION 611 OF THE SPECIFICATIONS.
- DETECTABLE WARNING SURFACES ARE REQUIRED AT STREET CROSSING & SIGNALIZED INTERSECTIONS.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
DETECTABLE WARNING SURFACES
REVISIONS:
R 2.29.1

CHAIN LINK FENCE

ELEVATION

SECTION A-A

NOTES:

- ALL ROUND LINE POSTS TO BE CAPED WITH COPPER TERMINAL POSTS TO BE INSTALLED AT ALL TERMINAL LOCATIONS.
- WITH TENSION BANDS TO BE INSTALLED AT ALL TERMINAL LOCATIONS.
- ALL ROUND LINE POSTS TO BE 1 1/2" DIA. WITH 1/2" DIA. TENSION BANDS TO BE 1/2" DIA. WITH 1/2" DIA. TENSION BANDS TO BE 1/2" DIA.
- MATERIALS TO MEET REQUIREMENTS OF ASHRAE 21-1.
- REFER TO SPECIFICATION 14-01 WHEN WIND IS IN THE CONTRACT DOCUMENTS.

CHARLES COUNTY GOVERNMENT
DEPARTMENT OF PLANNING & GROWTH MANAGEMENT
APPROVED: [Signature] DATE: 8/24/15
DATE: 8/24/15

STANDARD DETAIL
CHAIN LINK FENCE
REVISIONS:
R 2.29.1

P-2 PAVING DETAIL

NOT TO SCALE

NOTES:

- ISLAND SHALL BE A MINIMUM OF 75 SQUARE FEET.
- CONCRETE ISLAND SHALL BE A TYPE A OR B MONOLITHIC CONCRETE (NO. MD-645-02 OR MD-645-03) AND INCLUDE NOSE DOWN CURBS ON APPROX. ENDS.
- MINIMUM CHANNELIZATION DESIGN, TURNING LANES NOT SHOWN.
- REFER TO 11-0 FOR ENTRANCE DESIGN STANDARDS.

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602 CATEGORY CODE ITEMS

APPROVED: [Signature] DATE: 8/24/15

Maryland Department of Transportation
STATE HIGHWAY ADMINISTRATION
STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES
STANDARD TYPES C AND D
CONCRETE CURB AND COMBINATION
CONCRETE CURB & GUTTER
STANDARD NO. MD 620.02-01

608 CATEGORY CODE ITEMS

APPROVED: [Signature] DATE: 8/24/15

Maryland Department of Transportation
STATE HIGHWAY ADMINISTRATION
STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES
PRECAST CONCRETE WHEEL STOPS
STANDARD NO. MD 634.04

611 CATEGORY CODE ITEMS

APPROVED: [Signature] DATE: 8/24/15

Maryland Department of Transportation
STATE HIGHWAY ADMINISTRATION
STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES
DETECTABLE WARNING SURFACES
STANDARD NO. MD 655.40

607 CATEGORY CODE ITEMS

APPROVED: [Signature] DATE: 8/24/15

Maryland Department of Transportation
STATE HIGHWAY ADMINISTRATION
STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES
CHAIN LINK FENCE
TYPICAL 5 FT. RURAL & 6 FT. & 8 FT
STANDARD NO. MD 690.01

REVISIONS
1. PER PGM 6-26-24

BAYSIDE KIA OF WALDORF
TAX MAP 15, GRID 13, P/O PARCEL 776
SOUTH PARK EAST CONDOMINIUM UNIT 14
6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND

SITE DEVELOPMENT PLAN
SITE DETAILS

SCALE N/A

DRAWN DECEMBER 2023 CONTRACT REFERENCE 219190

LORENZI, DODDS, AND GUNNILL, INC.
ENGINEERS - PLANNERS - SURVEYORS
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7 OF 12

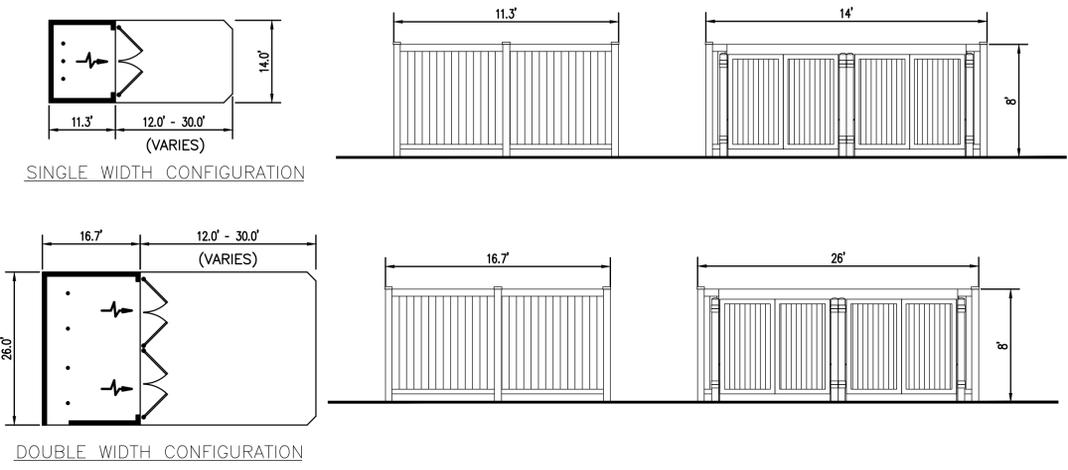
SDP-240001

PROFESSIONAL CERTIFICATE

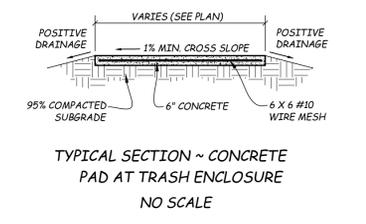
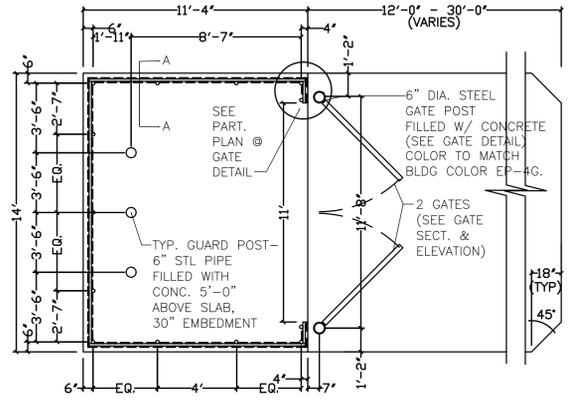
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13310, Expiration Date: 1-30-25.

[Signature]
James C. Lorenzi, P.E.
Date 6/26/24



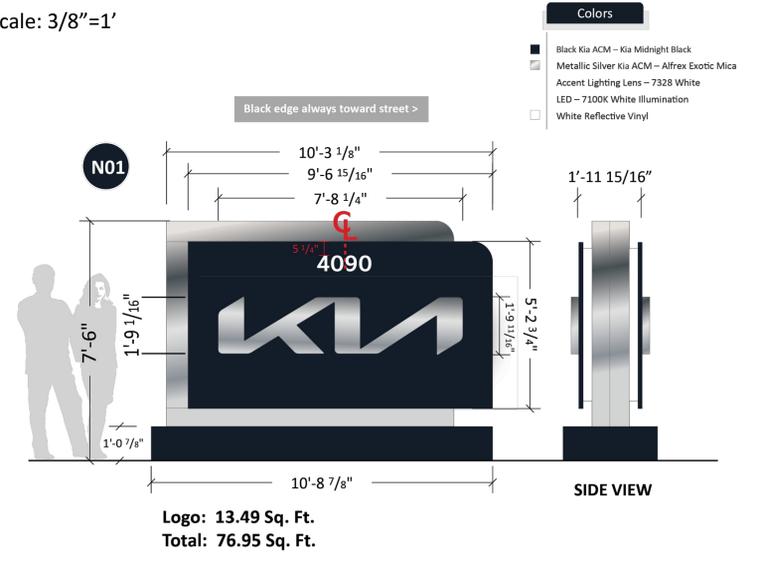


- NOTES:**
1. REFER TO ARCHITECTURAL PLAN DETAILS.
 2. TRASH ENCLOSURE MUST CONFORM TO ALL BUILDING SETBACKS. (CHECK LOCAL CODES.)
 3. FLOOR DRAIN MAY BE REQUIRED (CHECK LOCAL CODES.)
 4. SLAB TO SLOPE 0.1' FROM BACK TO FRONT FOR DRAINAGE. IF FLOOR DRAIN IS REQUIRED, SLOPE TO GRATE MIN. 0.1' BELOW PERIMETER OF SLAB.
 5. LOCATE PERSONNEL ACCESS (WHEN USED) SO THAT IT IS MOST ACCESSIBLE FROM REAR DOOR OF BUILDING.
 6. REVERSES OF THE ABOVE CONFIGURATIONS ARE ALSO ACCEPTABLE.
 7. IT IS RECOMMENDED THAT TRASH ENCLOSURE APRONS BE BLACK.



1. DEPTH OF T/E APRON VARIES DEPENDING ON SITE CONDITIONS.
2. A 3' WIDE PERSONNEL ACCESS MAY BE ADDED ON THE SIDE OR REAR WALLS IF DESIRED.

Scale: 3/8"=1'



- Colors**
- Black Kia ACM - Kia Midnight Black
 - Metallic Silver Kia ACM - Alfrex Exotic Mica
 - Accent Lighting Lens - 7328 White
 - LED - 7100K White Illumination
 - White Reflective Vinyl
- Specifications**
- Cladding - 4MM Kia Black and Metallic Silver ACM
 - Accent Lighting - 7100K LED Illumination with 7328 White Polycarbonate Lens
 - Kia Wordmark - Milled Aluminum and Plastic
 - "Sandwich" Design with 7100K LED Illumination thru edge of filler
 - Black edge always toward street >
 - Address #'s "4090" in 6" tall, white reflective vinyl.

RESERVED PARKING
R7-8 NO PARKING
LEGEND - GREEN (RETROREFL), WHITE SYMBOL ON BLUE (RETROREFL) BACKGROUND - WHITE (RETROREFL)

NO PARKING IN ACCESS AISLE

VAN ACCESSIBLE

MAXIMUM FINE \$500

2 1/2" ARROWHEAD

SIGN SIZE	DIMENSIONS (INCHES)										
	A	B	C	D	E	F	G	H	J	K	L
STANDARD	13/4	2 1/2 D	1	2 C	1 1/2 D	12	18	6	3/8	1/4	1 1/2

REFERENCES
MUTCD SECTION - 26 - 35
MUTCD SUPPLEMENT - 28 - 31

LEGEND - RED
BACKGROUND - WHITE

ACCESSIBLE PARKING NOTE: IN ACCORDANCE WITH MARYLAND ACCESSIBILITY CODE REGULATIONS,
 • INSTALL "RESERVED PARKING" SIGN AND "MAX. FINE \$500" PLAQUE @ EACH ACCESSIBLE PARKING SPACE.
 • INSTALL "VAN ACCESSIBLE" PLAQUE @ VAN ACCESSIBLE SPACE.
 • INSTALL "NO PARKING IN ACCESS AISLE" SIGN @ ACCESS AISLE.
 • SIGNAGE STACKING TO BE 7 FT. IN HEIGHT MEASURED FROM BOTTOM OF RESERVED PARKING SIGNAGE TO GRADE, IF POLE MOUNTED.

SIGN STACKING DETAIL
NO SCALE

SWM SAFETY FENCE DETAIL
NOT TO SCALE

SWM WARNING SIGN DETAIL
NOT TO SCALE

FIRE DEPARTMENT CONNECTION SIGNAGE DETAIL
NOT TO SCALE

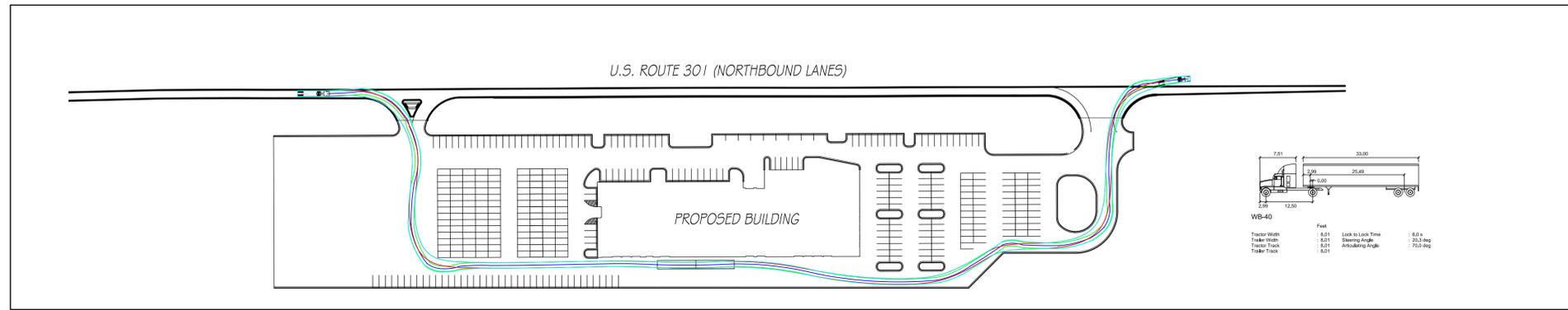
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James C. Lorenzi, P.E.
Date: 6/26/24

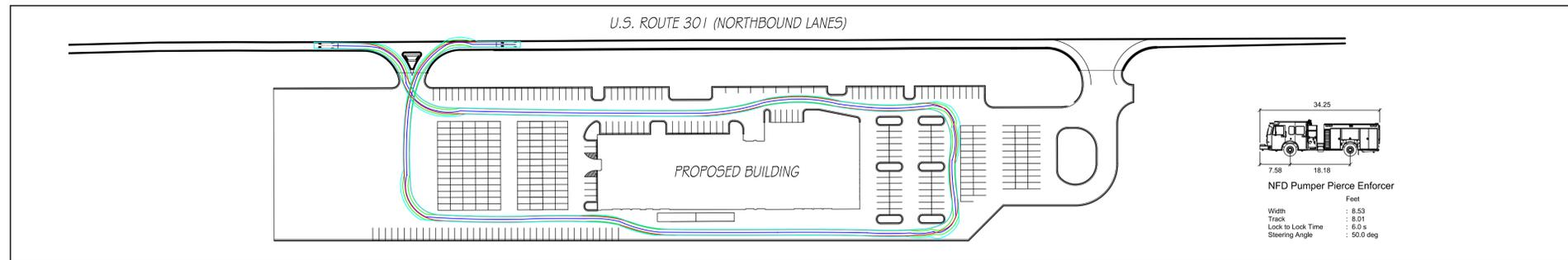


REVISIONS		BAYSIDE KIA OF WALDORF	
1.	PER PGM 6-26-24	TAX MAP 15, GRID 13, P/O PARCEL 776	SCALE N/A
DRAWN DECEMBER 2023		SITE DEVELOPMENT PLAN	
CHECKED DECEMBER 2023		SITE DETAILS	
DRAWN DECEMBER 2023		219190	
CHECKED DECEMBER 2023		219190	
LORENZI, DODDS, AND GUNNILL, INC.		ENGINEERS - PLANNERS - SURVEYORS	
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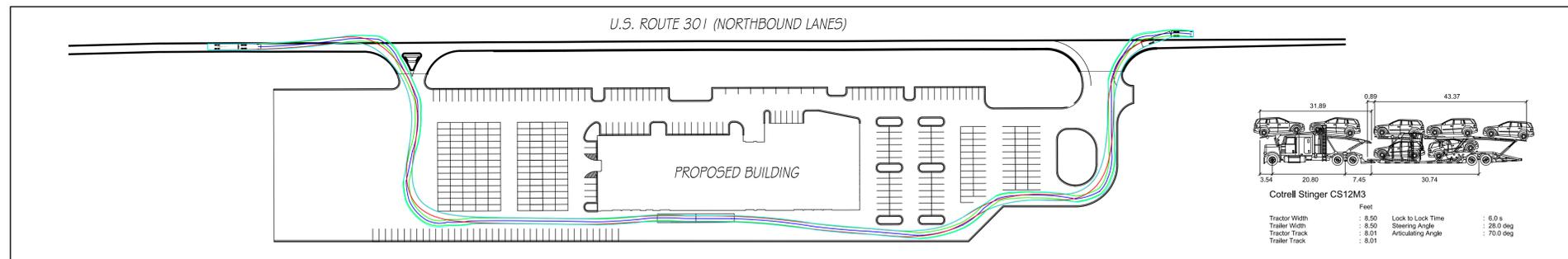
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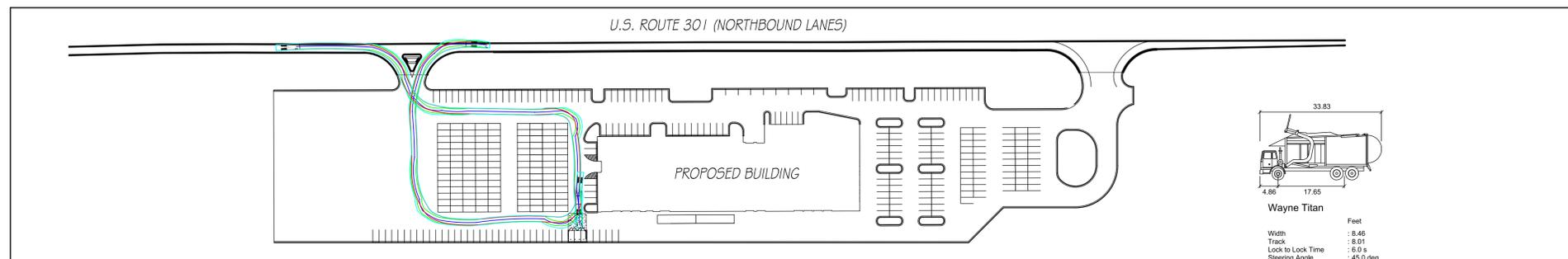
WB-40 VEHICLE TURNING RADIUS EXHIBIT



FIRE TRUCK TURNING RADIUS EXHIBIT



CAR CARRIER TURNING RADIUS EXHIBIT



REFUSE TRUCK TURNING RADIUS EXHIBIT



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James C. Lorenzi
James C. Lorenzi, P.E.

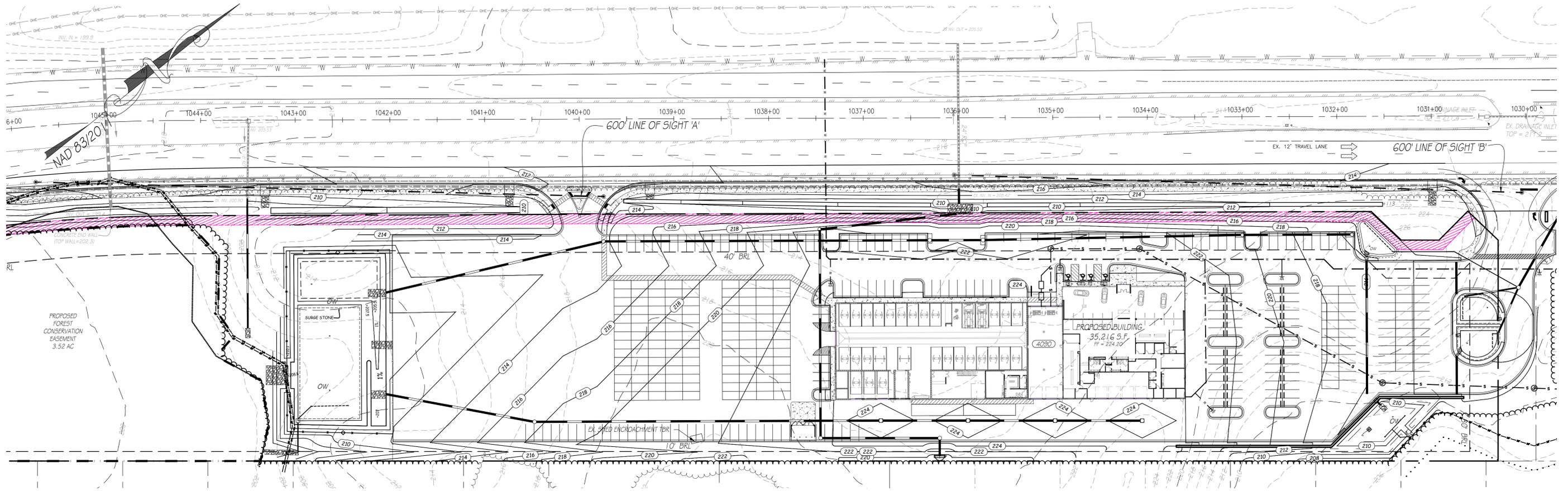
6/26/24
Date



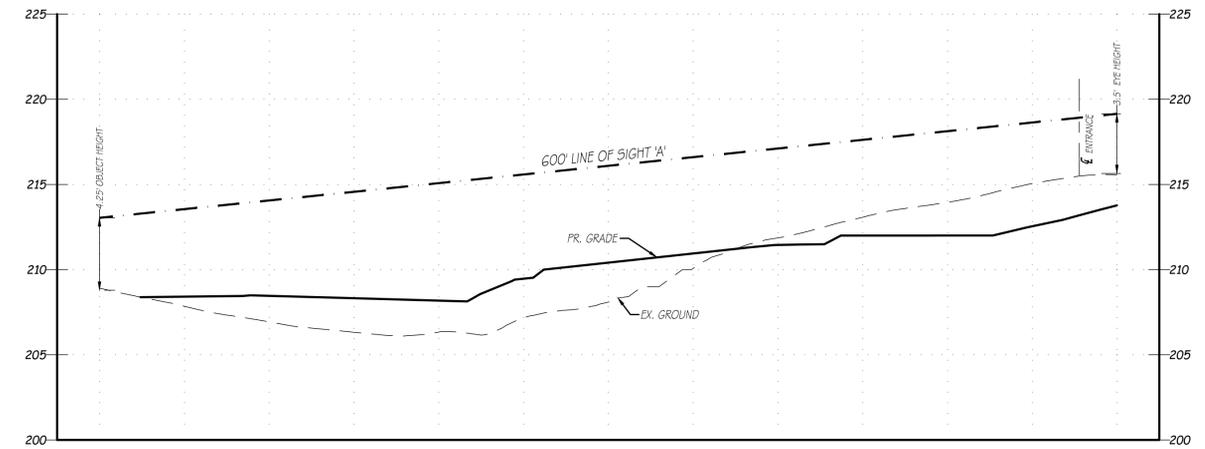
REVISIONS 1. PER PGM 6-26-24	BAYSIDE KIA OF WALDORF TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND		SCALE 1" = 100'
	SITE DEVELOPMENT PLAN VEHICLE CIRCULATION PLAN		9 OF 12
DRAWN CHECKED	DECEMBER 2023	CONTRACT REFERENCE 219190	
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM			

SDP-240001

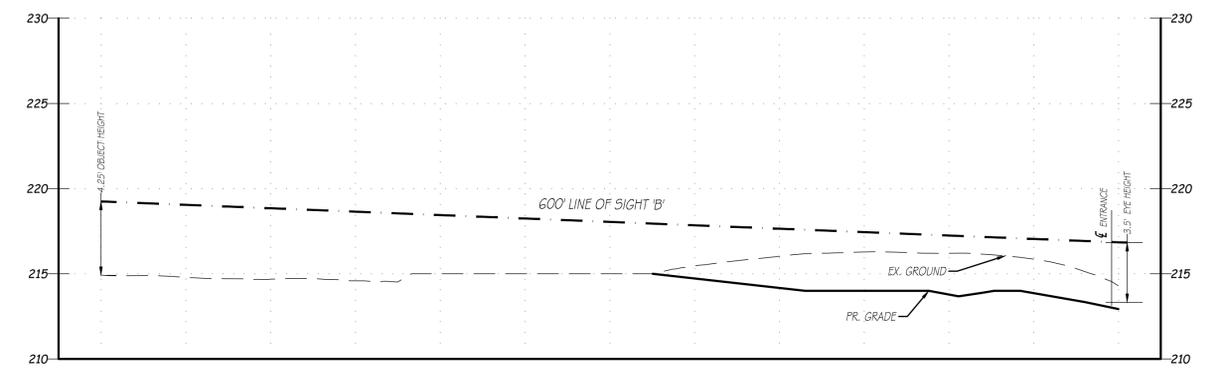
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SIGHT DISTANCE PLAN



SIGHT DISTANCE PROFILE 'A': US ROUTE 301 ~ CRAIN HIGHWAY
 200' RIGHT-OF-WAY
 ROAD CLASSIFICATION: URBAN O.P.A.
 DESIGN SPEED: 60 M.P.H. ~ OPERATING SPEED: 60 M.P.H. ~ POSTED SPEED: 45 M.P.H.
 SCALE: 1" = 50' H. 1" = 5' V.



SIGHT DISTANCE PROFILE 'B': US ROUTE 301 ~ CRAIN HIGHWAY
 200' RIGHT-OF-WAY
 ROAD CLASSIFICATION: URBAN O.P.A.
 DESIGN SPEED: 60 M.P.H. ~ OPERATING SPEED: 60 M.P.H. ~ POSTED SPEED: 45 M.P.H.
 SCALE: 1" = 50' H. 1" = 5' V.



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James C. Lorenzi
 James C. Lorenzi, P.E. Date: 9/3/24



REVISIONS 1. PER PGM 6-26-24 2. PER PGM 9-3-24		BAYSIDE KIA OF WALDORF TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND	
DRAWN CHECKED: DECEMBER 2023 CONTRACT REFERENCE: 219190		SITE DEVELOPMENT PLAN SIGHT DISTANCE PLAN AND PROFILES	
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM		SCALE 1" = 50' HOR. 1" = 5' VERT. 10 OF 12	



Plan View
Scale - 1" = 40ft

MAD 8/3/2011

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
PARKING	+	10.5 fc	43.8 fc	0.0 fc	N/A	N/A
Property Line - North	+	0.8 fc	2.0 fc	0.0 fc	N/A	N/A
Property Line - South	+	0.3 fc	0.5 fc	0.0 fc	N/A	N/A

Symbol	Label	Image	Quantity	Manufacturer	Catalog Number	Description	Number of Lenses	Lumens	Light Level (Foot-candles)	Wattage	Plot
SP1			8	BEACON PRODUCTS	SP-2-720-180-9K7-4R	VISER	1	21300	1	179.4	
SP2			13	BEACON PRODUCTS	SP-2-720-240-9K7-1R	VISER	1	28850	1	463.1	
SP3			3	BEACON PRODUCTS	SP-2-720-240-9K7-1R	VISER	1	28850	1	241.7	
				BEACON PRODUCTS	SP-2-720-210-9K7-4R	VISER	1	24104	1	216.2	
				BEACON PRODUCTS	SP-2-720-240-9K7-1R	VISER	1	28850	1	241.7	
				BEACON PRODUCTS	SP-2-720-240-9K7-50R	VISER	1	21330	1	166.8	
SP4			5	BEACON PRODUCTS	SP-2-720-240-9K7-50R	VISER	1	21330	1	166.8	
W1			24	Beacon Products	TDW-600-130-4K7-4R	TRAWARISE	1	15533	1	136.433	



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t. 443.787.4264
<http://www.idcmep.net/>
MECHANICAL ■ ELECTRICAL ■ PLUMBING
ENGINEERING SERVICES

REVISIONS 1. PER PGM 9-3-24	BAYSIDE KIA OF WALDORF TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND		SCALE 1" = 40'
	SITE DEVELOPMENT PLAN PHOTOMETRIC SITE LIGHTING PLAN		
DRAWN CHECKED	DECEMBER 2023	CONTRACT REFERENCE	219190
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM			11 OF 12



VIPER Area/Site

VIPER LUMINAIRE

FEATURES

- Low profile LED area/site luminaire with a variety of IES distributions for lighting applications such as auto dealership, retail, commercial, and campus parking lots
- Featuring two different optical technologies, Strike and Micro Strike Optics, which provide the best distribution patterns for retrofit or new construction
- Rated for high vibration applications including bridges and overpasses. All sizes are rated for 15G
- Control options including photo control, occupancy sensing, NX Distributed Intelligence™, wiSCAPE and 7-Pin with networked controls
- New customizable lumen output feature allows for the wattage and lumen output to be customized in the factory to meet whatever specification requirements may entail
- Field interchangeable mounting provides additional flexibility after the fixture has shipped



CONTROL TECHNOLOGY



SPECIFICATIONS

CONSTRUCTION

- Die-cast housing with hidden vertical heat fins are optimal for heat dissipation while keeping a clean smooth outer surface
- Corrosion resistant, die-cast aluminum housing with 1000 hour powder coat paint finish
- External hardware is corrosion resistant

OPTICS

- Micro Strike Optics (50, 320, 480, or 720 LED counts) maximize uniformity in applications and come standard with mid-power LEDs which evenly illuminate the entire luminous surface area to provide a low glare appearance. Catalog logic found on page 2
- Strike Optics (36, 72, 108, or 162 LED counts) provide best in class distributions and maximum pole spacing in new applications with high powered LEDs. Strike optics are held in place with a polycarbonate bezel to mimic the appearance of the Micro Strike Optics so both solutions can be combined on the same application. Catalog logic found on page 3
- Both optics maximize target zone illumination with minimal losses at the house-side, reducing light trespass issues. Additional backlight control shields and house side shields can be added for further reduction of illumination behind the pole
- One-piece silicone gasket ensures a weatherproof seal
- Zero up-light at 0 degrees of tilt
- Field rotatable optics

INSTALLATION

- Mounting patterns for each arm can be found on page 11
- Optional universal mounting block for ease of installation during retrofit applications. Available as an option (ASOU) or accessory for square and round poles.
- All mounting hardware included

INSTALLATION (CONTINUED)

- Knuckle arm filter option available for 2-3/8" OD tenon
- For products with EPA less than 1 mounted to a pole greater than 20ft, a vibration damper is recommended

ELECTRICAL

- Universal 120-277 VAC or 347-480 VAC input voltage, 50/60 Hz
- Ambient operating temperature -40°C to 40°C
- Drivers have greater than 90% power factor and less than 20% THD
- LED drivers have output power over-voltage, over-current protection and short circuit protection with auto recovery
- Field replaceable surge protection device provides 20kA protection meeting ANSI/IEEE C62.41.2 Category C High and Surge Location Category C3. Automatically takes fixture off-line for protection when device is compromised

CONTROLS

- Photo control, occupancy sensor programmable controls, and Zigbee wireless controls available for complete on/off and dimming control
- Please consult brand or sales representative when combining control and electrical options as some combinations may not operate as anticipated depending on your application
- 7-pin ANSI C136.41-2013 protocol receptacle option available for twist lock photocontrols or wireless control modules (control accessories sold separately)
- 0-10V Dimming Drivers are standard and dimming leads are extended out of the luminaire unless control options require connection to the dimming leads. Must specify if wiring leads are to be greater than the 6" standard

CONTROLS (CONTINUED)

- NX Distributed Intelligence™ available with fixture wireless control module, features dimming and occupancy sensor
- wiSCAPE® available with in fixture wireless control module, features dimming and occupancy sensor. Also available in 7-pin configuration

CERTIFICATIONS

- Meets the qualifications for DLC Premium
- Listed to UL1598 and CSA C22.2#250-0-24 for wet locations and 40°C ambient temperatures
- 1.5 G rated for ANSI C136.31 high vibration applications
- Fixture is IP65 rated
- Meets DA recommendations using 3K CCT configuration at 0 degrees of tilt
- This product qualifies as a "designated country construction material" per FAR 52.225-11 Buy American Construction Materials under Trade Agreements effective 04/23/2020. See Buy American Solutions.

WARRANTY

- 5 year warranty
- See HLI Commercial and Industrial Outdoor Lighting Warranty for additional information

KEY DATA	
Lumen Range	5,000-80,000
Wattage Range	36-600
Efficacy Range (LPW)	92-155
Weight lbs. (kg)	13.7-30.9 (6.2-13.9)



TRAVERSE

SURFACE/CEILING/GARAGE

FEATURES

- Wall surface mounted luminaire with a field replaceable LED light engine & optical bezel system
- Optional passive infrared (PIR) motion sensor
- Horizontal opening motion allows for minimal effort during installation, maintenance, or upgrading
- LED wattages from 27 Watt to 136 Watt. Easy and fast mounting
- Mount the backplate to wall and attach front on hinges to close



SPECIFICATIONS

CONSTRUCTION

- Traverse luminaire consists of a die cast aluminum two-piece housing
- Die cast main (thermal) housing provides direct heat exchange between the LED light engine and the cool outdoor
- LED drivers are thermally isolated from the main housing, mechanically attached and heat sinked to the rear housing
- Shape of the main housing is designed to prevent debris accumulation and as a bird nesting deterrent. The back and main housings are designed to hinge open for easy mounting and easy access
- IFS polyester powder-coat electrostatically applied and thermocured
- IFS finish consists of a five stage iron phosphate chemical pretreatment regimen with a polymer primer sealer, oven dry off and top coated with a thermoset super TGIC polyester powder coat finish
- The finish meets the AAMA 2604 performance specification which includes passing a 3000 hour salt spray test for corrosion resistance and resists cracking or loss of adhesion per ASTM D522 and resists surface impacts of up to 160 inch-pounds

OPTICS

- Optical one piece cartridge system consisting of an LED engine, optics, gasket and stainless steel bezel
- Cartridge is held together with internal brass standoffs soldered to the board so that it can be field replaced as a one piece optical system
- Optics are held in place without the use of adhesives
- Cartridge assembly is available in various lighting distributions using TR designed acrylic optical lenses over each LED

DATE: LOCATION:

TYPE: PROJECT:

CATALOG #:

ELECTRICAL

- 100V through 277V, 347V and 480V, 50 Hz to 60 Hz
- Dimming Drivers are standard and dimming leads are extended out of the luminaire unless control options require connection to the dimming leads. Must specify if wiring leads are to be greater than the 6" standard.
- Component-to-component wiring within the luminaire may carry no more than 80% of rated load and is certified by UL for use at 600 VAC at 50°C or higher
- Plug disconnects are certified by UL for use at 600 VAC, 15A or higher, 15A rating applies to primary (AC) side only
- Surge protection – 20kA

OPTIONS/CONTROLS

- Available with an optional passive infrared (PIR) motion sensor capable of detecting motion 360° around the luminaire. When no motion is detected for the specified time, the Motion Response system reduces the wattage down to a factory preset level, reducing light level accordingly. When motion is detected, the luminaire returns to full wattage and full light output. Please contact Beacon Products if project requirements vary from the standard configurations
- Please consult brand or sales representative when combining control and electrical options as some combinations may not operate as anticipated depending on your application.

INSTALLATION

- Rear housing (back plate) is designed with various bolt patterns for direct wall mounting or mounting to a recessed junction box
- Rear housing has three integral 3/4" NPT power feed locations (bottom and each side) for surface mounted conduit applications
- After mounting the rear housing to the wall or junction box, the main housing is designed to attach and hinge closed after connecting the male and female quick connectors
- Mounting design permits a simple retrofit to existing wall luminaires that utilize or recessed junction boxes

CERTIFICATIONS

- The luminaire shall bear a CSA label and be marked suitable for wet locations (standard)
- This product qualifies as a "designated country construction material" per FAR 52.225-11 Buy American Construction Materials under Trade Agreements effective 06/03/2020.

WARRANTY

- 5 year warranty

KEY DATA	
Lumen Range	3,300-17,000
Wattage Range	27-136
Efficacy Range (LPW)	106-155



MATT BAYER
Electrical Designer

MattB@IDCmep.net

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Bel Air, MD 21014
t. 443.787.4264
<http://www.idcmep.net/>

MECHANICAL ■ ELECTRICAL ■ PLUMBING
ENGINEERING SERVICES

REVISIONS	BAYSIDE KIA OF WALDORF		SCALE
1. PER PGM 9-3-24	TAX MAP 15, GRID 13, P/O PARCEL 776 SOUTH PARK EAST CONDOMINIUM UNIT 1A 6TH ELECTION DISTRICT ~ CHARLES COUNTY, MARYLAND		N/A
	SITE DEVELOPMENT PLAN SITE LIGHTING DETAILS		
DRAWN CHECKED:	DECEMBER 2023	CONTRACT REFERENCE:	219190
LORENZI, DODDS, AND GUNNILL, INC. ENGINEERS ~ PLANNERS ~ SURVEYORS WWW.LDGENGINEERS.COM			<div style="border: 2px solid black; border-radius: 50%; width: 30px; height: 30px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">12</div> <p>OF 12</p>

TRAFFIC CONCEPTS, INC.

Traffic Impact Studies • Feasibility • Traffic Signal Design • Traffic Counts • Expert Testimony

November 19, 2024

Mr. Jason R. Groth, AICP
Acting Director
Department of Planning and Growth Management
Charles County Government
200 Baltimore Street
La Plata, MD 20646

RE: South Park East
APF Traffic Mitigation
TC #3883

Dear Mr. Groth:

The South Park East project will create a mix of commercial uses. Phase I of the project will create a 38,150 gross square foot automobile dealership. The remaining uses on the property are unknown at this time, but the traffic impact study assumed Phase II of the project as 110,000 gross square feet of self-storage. Phase III of the project was assumed as two (2) 3,300 gross square foot fast-food restaurants with drive-thrus, and a 5,000 gross square foot high-turnover sit-down restaurant.

The traffic impact study dated July 2024 has been reviewed and technically approved by your office and by the Maryland State Highway Administration. The study indicates all key intersections and road links will operate at acceptable levels of service under future traffic conditions, with the following exceptions:

1. The unsignalized intersection of US 301 @ the median break south of Smallwood Drive will operate at acceptable "A" through "C" levels of service. However, the intersection is projected to operate without reserve capacity during the PM and Saturday peak hours during Phases I through III.
2. The unsignalized intersection of US 301 @ the median break north of Billingsley Road will operate at acceptable "A" through "D" levels of service. However, the intersection is projected to operate without reserve capacity during the AM, PM, and Saturday peak periods during Phase III.

3. The signalized intersection of US 301 @ Billingsley Road is projected to operate at “E” levels of service under Phase III future traffic conditions during the AM, PM, and Saturday peak periods. The projected full build out of the development (Phase III) will have an impact greater than 1% on this intersection, and therefore, mitigation is required.

The developer of the South Park East project proposes the following traffic mitigation at the above-noted intersections:

1. US 301 @ the median break south of Smallwood Drive

Traffic analyses under a *future unsignalized condition* show that in the future Phase III condition, the intersection is projected to function with reserve capacity on the stop-controlled movements during the AM peak hour. However, the intersection is projected to function without reserve capacity for the Westbound Left/Right and Southbound Left/U-turn movement during the PM and Saturday peak hours under future Phase I through future Phase III conditions. The overall intersection Level of Service was calculated to be “A” during the AM, “C” during the PM, and “B” during the Saturday peak periods under future Phase III conditions utilizing the Unsignalized Highway Capacity Manual methodology. The overall intersection Level of Service was calculated to be “B” during the AM peak, and “C” during the PM and Saturday peak hours utilizing the critical lane analysis methodology.

The Adequate Public Facilities Manual indicates when reserve capacity is not available, additional studies should be conducted to determine potential means to correct the deficiency. The lack of reserve capacity is due to the heavy volume of traffic along the mainline of US 301. This traffic will experience gaps due to up and downstream traffic signals that will allow for the southbound and westbound left turns to occur. The Highway Capacity Manual includes exceedingly high headway default values that can skew results of the analyses.

The applicant proposes to conduct a traffic signal warrant analysis at a future date (at the beginning of the Phase III portion of the project) to determine if traffic signalization is necessary as the project builds out to determine actual traffic volumes (remember that high traffic generators have been assumed for the pad sites in this area to create a worst-case traffic condition). MDOT-SHA has agreed to this as noted in their approval letter (copy attached). Therefore, this intersection will meet the standards of the Zoning Ordinance, Section 257.

2. US 301 @ the median break north of Billingsley Road

Traffic analyses under a *future unsignalized condition* show that in the future Phase III condition, the intersection is projected to function with reserve capacity on the stop-controlled movements during the AM peak hour. However, the intersection is projected to function without reserve capacity for the Westbound Left movement during the AM, PM, and Saturday peak hours under future Phase III conditions. The overall intersection Level of Service was calculated to be “A” during the AM and PM, and “D” during the Saturday peak periods under future Phase III conditions utilizing the Unsignalized Highway Capacity Manual methodology. The overall intersection Level of Service was calculated to be “B” during the AM and PM peak, and “C” during the Saturday peak hours utilizing the critical lane analysis methodology.

The Adequate Public Facilities Manual indicates when reserve capacity is not available, additional studies should be conducted to determine potential means to correct the deficiency. The lack of reserve capacity is due to the heavy volume of traffic along the mainline of US 301. This traffic will experience gaps due to up and downstream traffic signals that will allow for the southbound and westbound left turns to occur. The Highway Capacity Manual includes exceedingly high headway default values that can skew results of the analyses.

The applicant proposes to conduct a traffic signal warrant analysis at a future date (at the beginning of the Phase III portion of the project) to determine if traffic signalization is necessary as the project builds out to determine actual traffic volumes (remember that high traffic generators have been assumed for the pad sites in this area to create a worst-case traffic condition). MDOT-SHA has agreed to this as noted in their approval letter (copy attached). Therefore, this intersection will meet the standards of the Zoning Ordinance, Section 257.

3. US 301 @ Billingsley Road

Since this intersection operates under a coordinated signal system, the Synchro/HCM program was used to determine the adequacy of this intersection. This intersection is projected to function at a Level of Service “E” during the AM, PM, and Saturday peak hours with the full build out of the development utilizing the Synchro/HCM methodology. As noted in the Adequate Public Facilities Manual, if the volume to capacity is degraded by less than 0.01 (1 percent), mitigation is not required. In this case, the impact on the volume to capacity ratio during the AM peak hour does not exceed 1% until Phase III of the project. However, the impact on the volume to capacity ratio during the PM peak hour (1.1%) and during the Saturday peak hour (1.9%) exceeds 1% under Phase I of the project. Therefore, mitigation is required.

Due to the minimal impact of Phase I of the project, it is requested that the proposed improvement discussed below be required at the start of Phase II of the overall project.

In addition to the level of service requirements noted above, a queuing analysis was conducted utilizing the SimTraffic methodology. The results of the queuing analyses indicate the following for the US 301 @ Billingsley Road intersection:

- The eastbound left turn bay from Billingsley Road onto US 301 is approximately 430'. The projected queue length will reach 370' under background conditions and as long as 456' during future phase II condition. This issue will be addressed with the proposed improvement as discussed below.
- The westbound double left turn bays from Billingsley Road onto US 301 are approximately 400' long. The projected queue length will reach 667' through future phase III conditions. Since the proposed development will not add traffic to these turn bays, the developer should not be required to provide improvements at this location.
- The northbound left turn bay from US 301 to Billingsley Road is approximately 990' long. The projected queue length will reach 1359' through future phase III conditions. Since the proposed development will not add traffic to this turn bay, the developer should not be required to provide improvements at this location.

The developer of the South Park East project proposes to address the adequacy and queuing issue by constructing the following improvement in conjunction with Phase II of the project:

The developer is proposing to construct an additional left turn bay to provide double-left turn bays on the eastbound approach of Billingsley Road at US 301. These turn bays will mitigate the impact of the South Park East project on the intersection and will also accommodate the future queuing requirements of South Park East, South Park West, and Kaiser Permanente (all projects included in this study and the queuing analyses). The additional turn lane will be constructed to the extent practicable within the existing median area of Billingsley Road.



With this improvement in place, the Synchro/HCM analyses indicate that the v/c ratio will be improved to a level better than background conditions. This improvement will also provide adequate storage area to accommodate total future traffic volumes, including South Park East, South Park West, and Kaiser Permanente. This improvement will over-mitigate the impact of the South Park East project on this intersection. The results of the analyses are shown in the charts below.

HCM 2000 - AM PEAK HOUR					
KEY INTERSECTIONS	BACKGROUND Delay / LOS (V/C RATIO)	FUTURE PHASE I Delay / LOS (V/C RATIO)	FUTURE PHASE II Delay / LOS (V/C RATIO)	FUTURE PHASE III Delay / LOS (V/C RATIO)	FUTURE PHASE III WITH IMPR Delay / LOS (V/C RATIO)
US 301 @ Billingsley Road	49.2 / D (0.683)	52.2 / D (0.686)	52.6 / D (0.686)	59.5 / E (0.726)	57.4 / E (0.680)

HCM 2000 - PM PEAK HOUR					
KEY INTERSECTIONS	BACKGROUND Delay / LOS (V/C RATIO)	FUTURE PHASE I Delay / LOS (V/C RATIO)	FUTURE PHASE II Delay / LOS (V/C RATIO)	FUTURE PHASE III Delay / LOS (V/C RATIO)	FUTURE PHASE III WITH IMPR Delay / LOS (V/C RATIO)
US 301 @ Billingsley Road	56.0 / E (0.717)	56.7 / E (0.728)	56.8 / E (0.729)	58.2 / E (0.744)	57.4 / E (0.698)

HCM 2000 - SAT PEAK HOUR					
KEY INTERSECTIONS	BACKGROUND Delay / LOS (V/C RATIO)	FUTURE PHASE I Delay / LOS (V/C RATIO)	FUTURE PHASE II Delay / LOS (V/C RATIO)	FUTURE PHASE III Delay / LOS (V/C RATIO)	FUTURE PHASE III WITH IMPR Delay / LOS (V/C RATIO)
US 301 @ Billingsley Road	53.9 / D (0.641)	58.2 / E (0.660)	59.0 / E (0.662)	67.2 / E (0.691)	61.3 / E (0.640)

Delay is reported in seconds per vehicle.

LOS – Level of Service

v/c – volume to capacity

As shown in the charts above, the proposed improvement at the intersection of US 301 at Billingsley Road will improve the v/c ratio beyond the background conditions, thereby over-mitigating the impact of the development on this intersection. In addition, the queue issue will be resolved with the additional left turn bay. Therefore, this intersection will meet the standards of the Zoning Ordinance, Section 257.

Mr. Jason R. Groth, AICP

November 19, 2024

Page 7 of 7

The improvements and future traffic signal warrant analyses discussed above will mitigate the impact of the development on the road network. This mitigation proposal will meet the requirements of the Charles County Zoning Ordinance §297-257 and 297-264.

If you have any questions or require additional information, please do not hesitate to contact our office.

Sincerely,
TRAFFIC CONCEPTS, INC.



By: Jackie L. Chandler
JChandler@traffic-concepts.com

November 7, 2024

Ms. Jackie Chandler, P. E.
Traffic Concepts, Inc.
7525 Connelley Drive
Suite B
Hanover MD 21076

Dear Ms. Chandler:

Thank you for the opportunity to review the Traffic Impact Study report (TIS) for the South Park East Development in Charles County. The State Highway Administration (SHA) has reviewed the TIS report. The SHA concurs with the report findings for this project as currently proposed and will not require the submission of any additional traffic analysis. An access permit is required for all construction within the MDOT SHA right-of-way. Detailed comments are attached.

South Park East Development
SHA Tracking No. # 24APCH004XX
US 301
Mile Point: 21.62
Charles County

Please reference the SHA tracking number on future submissions. If you have any questions or concerns, then please contact the District Five Access Management Regional Engineer, Mr. Jonathan Makhlof at 410-841-1084, toll-free 1-800-331-5603 (x1084) or via email at Jmakhlof2@mdot.maryland.gov. Mr. Makhlof will be happy to assist you.

Sincerely,



For Kimberly M. Tran, P. E.
District Engineer

Ms. Jackie Chandler
Page Two

Attachments: TIS Review comments

cc: Mr. Kirby Blass, Planner III, Department of Planning & Growth Management, Charles County
Mr. Jason Groth, Planning Director, Department of Planning and Growth Management, Charles County
Ms. Heather Kelley, Planner, Department of Planning & Growth Management, Charles County
Mr. Keven Zinn, Owner/Developer, Bayside Auto Group.
Ms. Mellisa Hively, Traffic/Transportation Planner, Department of Planning & Growth Management, Charles County
Mr. Kayode Adenaiya, Transportation Engineer, Traffic Development and Support Division, MDOT SHA
Ms. Rola Daher, Consultant for the Maryland Department of Transportation, Travel Forecasting and Analysis Division, SHA
Mr. Scott Holcomb, Consultant for the Maryland Department of Transportation, Travel Forecasting and Analysis Division, SHA
Mr. Jonathan Makhoulf, Regional Engineer, District Five Access Management, SHA
Ms. Amy Morris, Consultant for the Maryland Department of Transportation, District Five Traffic, SHA
Ms. Julie Wright, Consultant for the Maryland Department of Transportation, Traffic Development and Support Division, SHA
Mr. Simon Chacha, Transportation Manager, District Five Traffic, SHA
Ms. Karen Fiasco, Deputy District Engineer, District Five, SHA

Traffic Impact Study

The proposed South Park East Development site is located along the east side of US 301 between Smallwood Drive and Billingsley Road, Waldorf in Charles County. The proposed development will create 38,150 sft automobile sales in phase I. Phase II will include a 110,000 sft mini warehouse. Phase III will create two 3,300 sft fast-food restaurants with drive-thru and a 5,000 sft high-turnover sit-down restaurant.

- The following road sections were analyzed under existing, background and future conditions:
 - US 301 at Smallwood Drive
 - US 301 at Median Break south of Smallwood Drive
 - US 301 at Median Break north of Billingsley Road
 - US 301 at Billingsley Road

Based on the information provided, please address the following comments in a point-by-point response:

Travel Development and Support Division Comments: - *Ms. Julie Wright*

1. No further comments regarding Phase I of this TIS.
2. Further study of site access intersections will be revisited as this development, South Park West and Kaiser continue to move through the planning phases and the future land uses and traffic patterns are determined.

Travel Forecasting and Analysis Division Comments: - *Mr. Scott Holcomb*

1. TFAD does not have any further comments on this TIS.

Please submit the plans and supporting documentation in PDF format using the Access Management Electronic Plans Submittal system. The system can be accessed at www.roads.maryland.gov by selecting the Business Center drop down menu and Permits and Miss Utility Information, and click the link stating, "Click here for Electronic plans Submittal Link".

Item Cover Page

PLANNING COMMISSION AGENDA ITEM REPORT

DATE: February 3, 2025

SUBMITTED BY: Amy Brackett

ITEM TYPE: Briefing

AGENDA SECTION: NEW BUSINESS: No Public Comments

SUBJECT: **Economic Development Briefing and Update**

Staff: Kelly Robertson-Shlagel

SUGGESTED ACTION:

ATTACHMENTS:

[Planning Commission Update Economic Development.pdf](#)



PLANNING COMMISSION UPDATE

Economic Development Overview



February 3, 2025

ECONOMIC DEVELOPMENT DEPARTMENT



Kelly Robertson-Slagle
Director



Michelle Frye
Deputy Director



Mark Thompson
Chief of Commercial
Development



Anthony Adesina
Business Retention and
Expansion Manager



Chudi Obi
Business Development
Coordinator



**Lucretia
Freeman-Buster**
Chief of Business Services



Sydney Garner
Agriculture and Rural
Development Manager



Lacey Herbert
Economic Development
Administrative Coordinator



Anthony Twyman
MWBD/DBE Compliance
Manager



**Chastity
Durham-Miller**
Small and Minority Business
Development Specialist

Cheryl Anderson
Part Time - Business
Development Assistant

Dr. Margaret Dureke
Consultant - BGAP Program Manager



Jack Street
Research and Special
Projects Manager



Jennifer Reginald
Marketing Coordinator

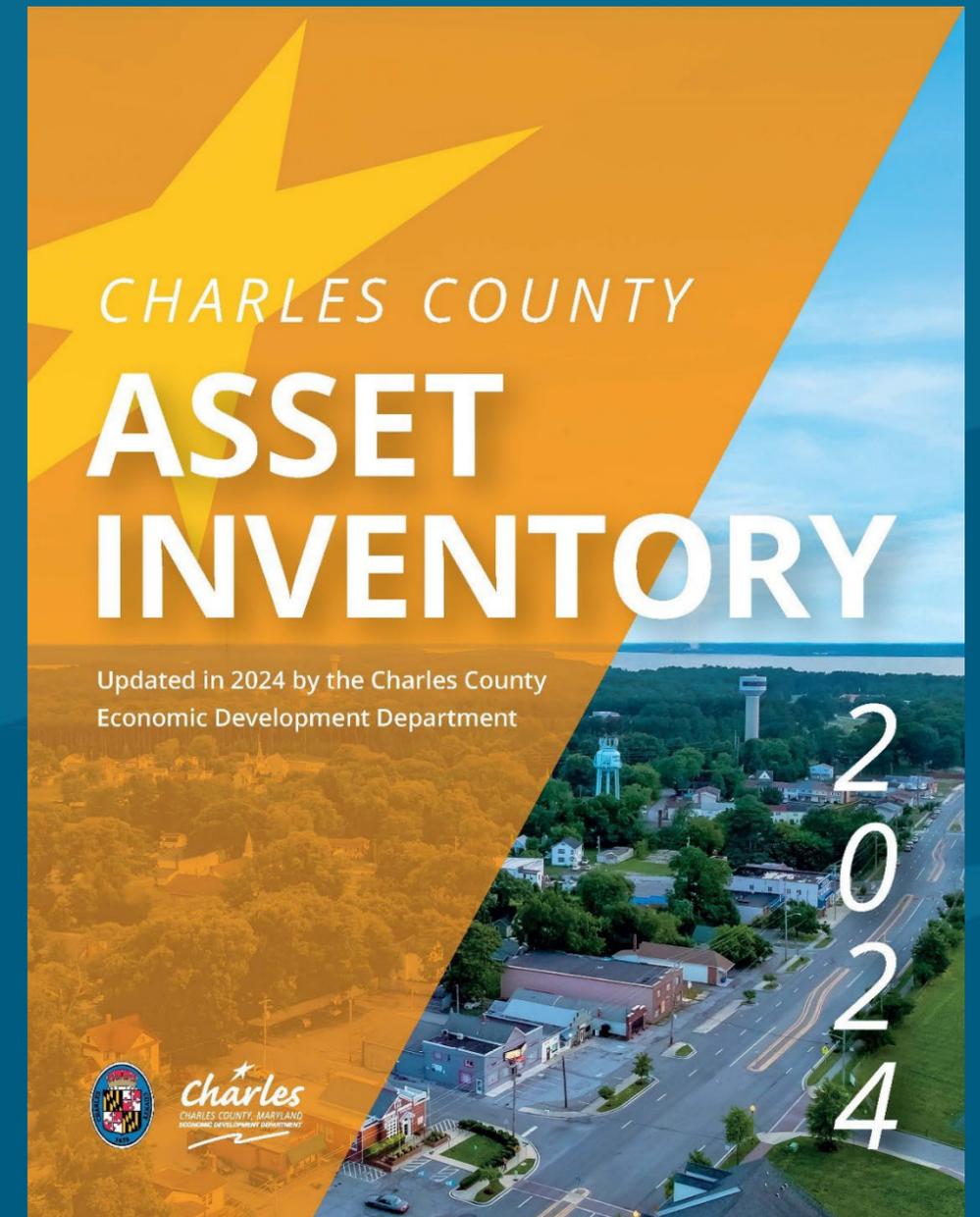


2024 Annual Report Highlights



Asset Inventory Report

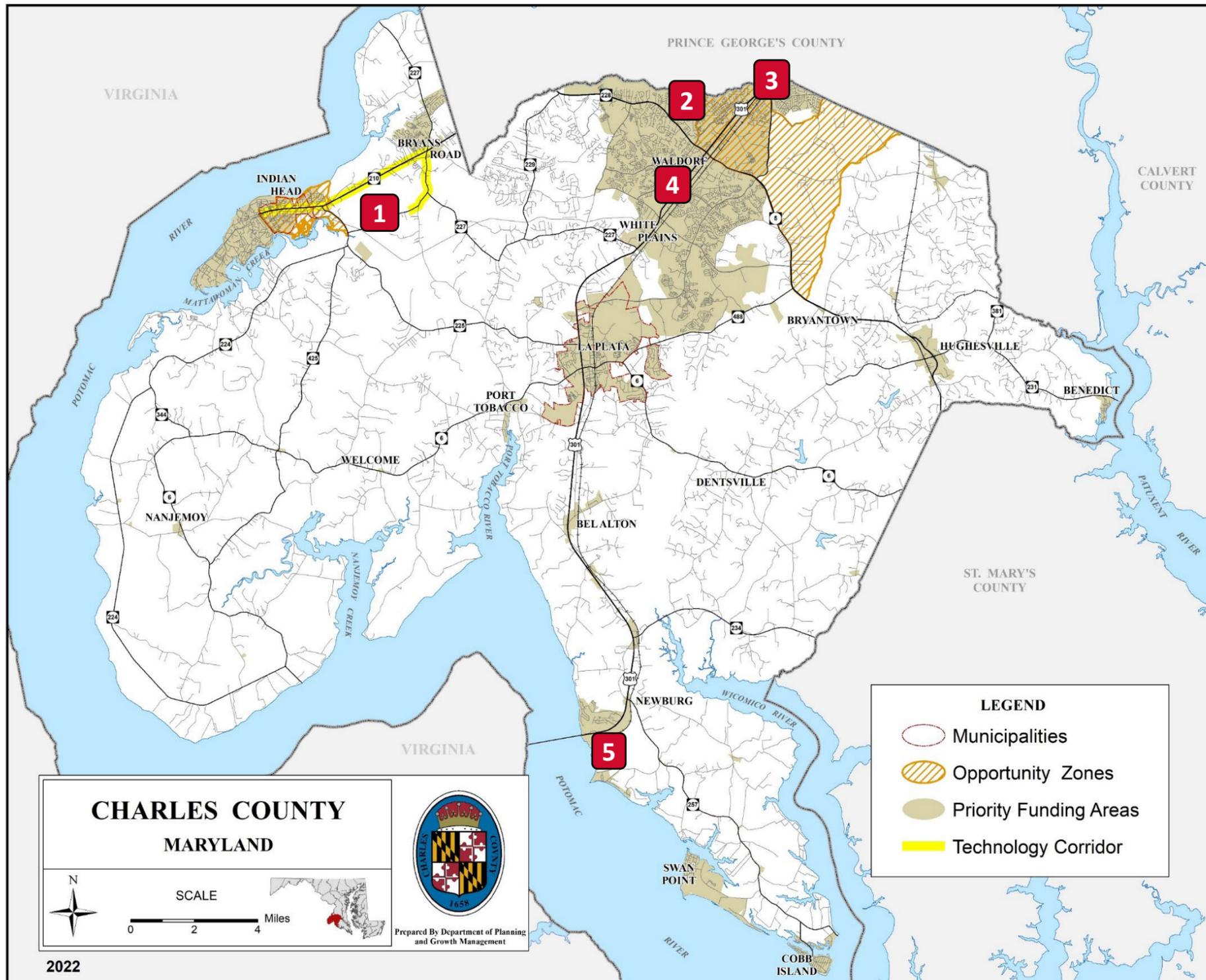
- The Economic Development Department (EDD) debuted the first edition of the Asset Inventory in November 2023 at the Annual Fall Meeting.
- The 2024 version of the Asset Inventory has been updated and is available now electronically on the EDD website, www.MeetCharlesCounty.com, under "Data & Resources."
- We continue are assessing strengths/weaknesses, identifying gaps, as well as possible opportunities between assets.



Aligning Zoning Policies for Economic Growth

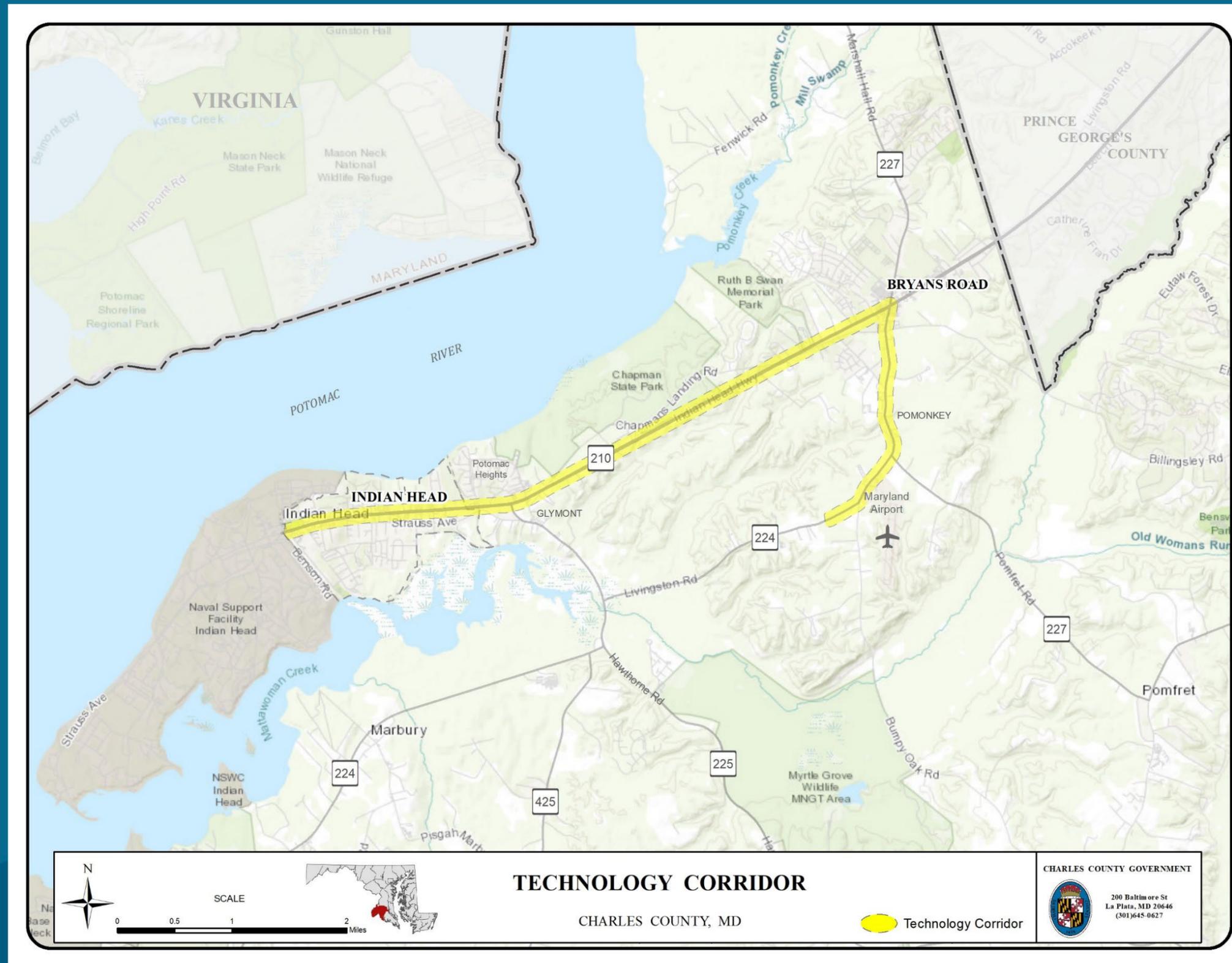
- The Economic Development Department is working closely with the Department of Planning and Growth Management to review and recommend appropriate updates to the Comprehensive Plan and Zoning Code Update.
- This effort ensures that our regulatory framework remains responsive to the needs of existing businesses, accommodates new and emerging industries.
- By aligning zoning policies with economic growth opportunities, we aim to foster a business-friendly environment that attracts investment, enhances job creation, and sustains long-term economic vitality in Charles County.

Major Projects



1. Western Charles County Technology Corridor
2. Berry Pointe
3. Waldorf Station
4. Charles County Sports and Wellness Center
5. Morgantown Power Plant

Western Charles County Technology Corridor

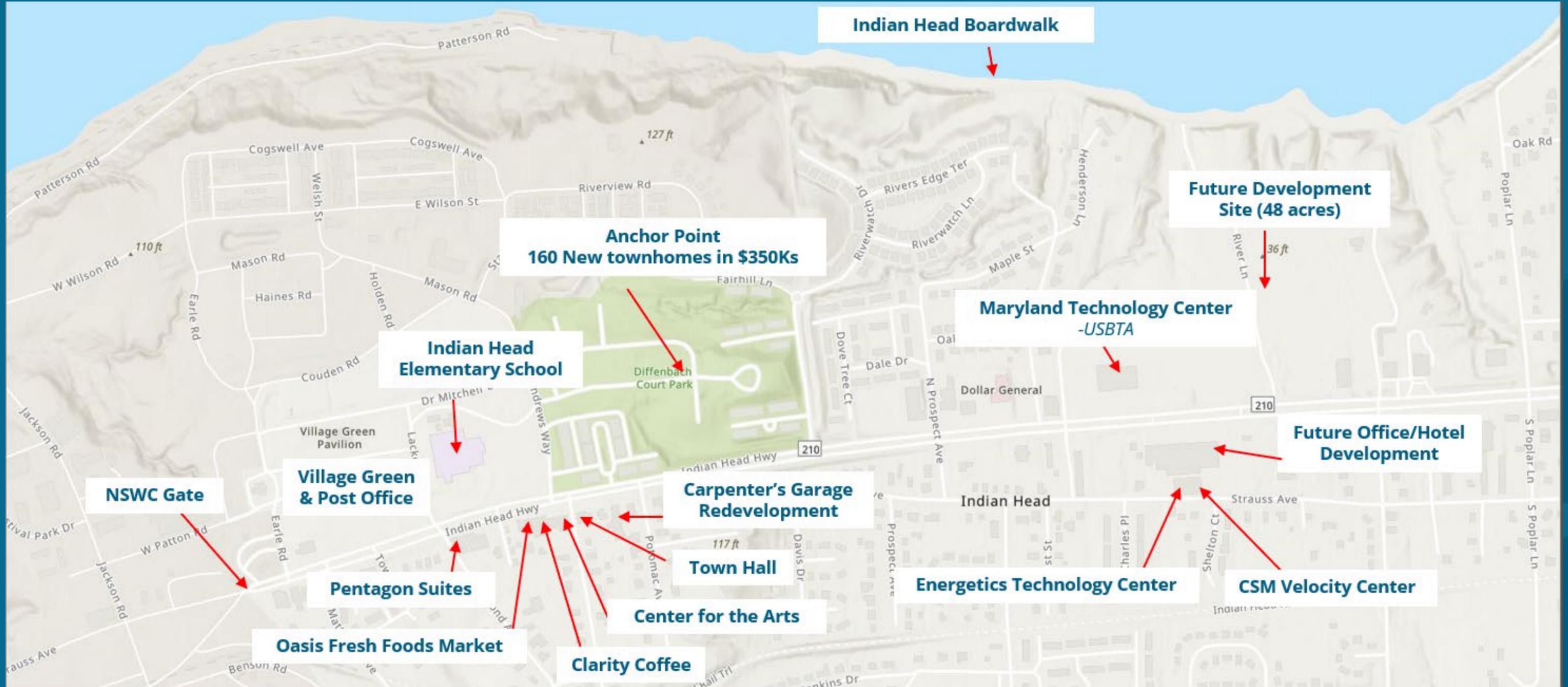


Western Charles County Technology Corridor



- EDD supported the creation of a new Aviation Business Park zone to directly support expected growth from NSWC IHD National Energetics and Smart Arsenal initiatives.
- EDD coordinated with the Resilience Authority of Charles County and the Town of Indian Head to apply for Clean Water Commerce Act funding to complete shoreline restoration of the Henderson Property.
- EDD coordinated with the Charles County Military Alliance Council, The Patuxent Partnership, NSWC IHD, College of Southern Maryland, and CCPS to apply for a \$575K Federal Workforce Development grant to expand the Southern Maryland 2030 Workforce Development program into Charles County.

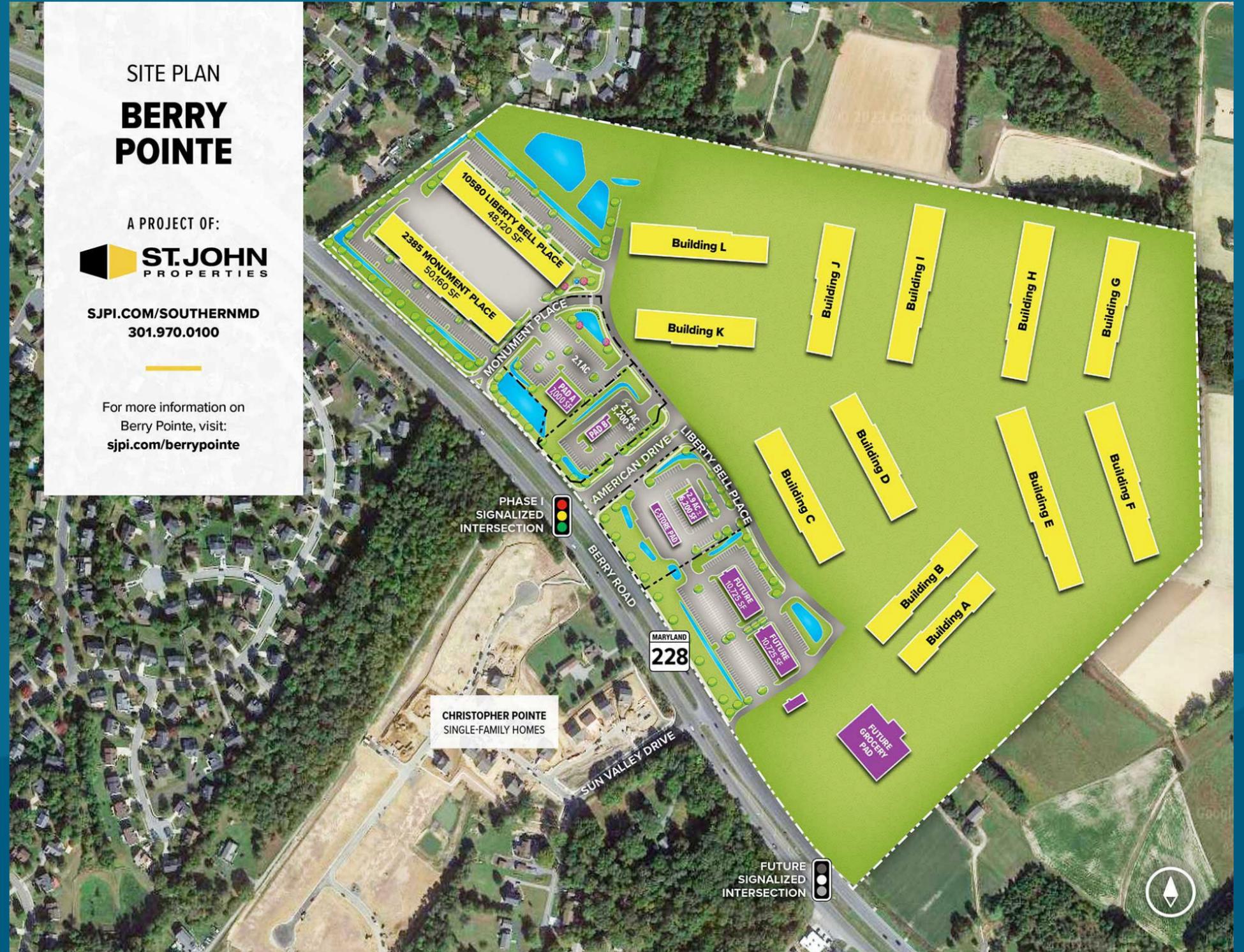
Indian Head Redevelopment



Berry Pointe



The project will include 518,400 sq ft of flex/R&D space, 93,000 sq ft of office, and 22,500 sq ft of supporting retail developed by St. John Properties.



Waldorf Station



The gateway to Charles County is a 145-acre mixed use project developed by Greenberg Gibbons.

Charles County Sports and Wellness Complex



Redevelopment of the former Sears Building into a Sports and Wellness Complex.

Morgantown Generating Station



The 355-acre intermodal (rail, highway, water access) property is a rare industrial redevelopment opportunity.

PRESENTED BY:

Kelly Robertson-Slagle, Director of Economic Development
Mark Thompson, Chief of Commercial Development



Economic Development Department Charles County Government

10665 Stanhaven Place, Ste. 206 | White Plains, MD 20695
301-885-1340 | info@MeetCharlesCounty.com

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www.MeetCharlesCounty.com
www.CharlesCountyMD.gov

Charles CHARLES COUNTY, MARYLAND
CLOSE TO THE CAPITAL. FAR FROM ORDINARY

Item Cover Page

PLANNING COMMISSION AGENDA ITEM REPORT

DATE: February 3, 2025

SUBMITTED BY: Amy Brackett

ITEM TYPE: Update

AGENDA SECTION: NEW BUSINESS: No Public Comments

SUBJECT: **Affordable Housing Update**

Staff: Joel Binkley

SUGGESTED ACTION:

ATTACHMENTS:

Item Cover Page

PLANNING COMMISSION AGENDA ITEM REPORT

DATE: February 3, 2025

SUBMITTED BY: Amy Brackett

ITEM TYPE: Administrative

AGENDA SECTION: NEW BUSINESS: No Public Comments

SUBJECT: **Poll of the Planning Commission Members for New Business**

SUGGESTED ACTION:

ATTACHMENTS: