

TOWN GRAY
GRAY TOWN COUNCIL
AGENDA • DECEMBER 1, 2020

**Gray Town
Council Workshop**

**Henry Pennell Municipal Complex
24 Main Street
Gray, ME 04039**

6:00 PM

I. OPENING STATEMENT:

Due to the Governor's latest Executive Order, the Town Council meeting will NOT take place in person. The meeting will be held online with the link provided the day of the meeting.

II. CALL TO ORDER

Roll Call

III. WORKSHOP 6:00 PM - 6:55 PM

Introductions

Update on Outstanding Project Items:

- Outflow Pipe Relocation/Easements
- Tree/Other Impacts
- Utility Pole Relocations/Eliminations
- Location of Additional Crosswalk

Update on Project Plans:

- Estimate on Hybrid Construction Inspection
- Water District Communication
- Other?

Update on Timeline

IV. ADJOURNMENT

Motion to Adjourn

** The Town of Gray is an equal opportunity employer and complies with all applicable equal access to public accommodations law. If you are planning to attend a Town Council or Town committee or board meeting and need assistance with a physical disability, please contact the Town Manager's office at least 48 hours in advance of the meeting to have the Town assist you. 657-3339. TTY 657-3931.*

Shaker Rd Sidewalk Conflicts

| House # | Name | Construction conflict | Notes |
|--------------------------------|--------------------------------------|---|---|
| 55 | Shirley Wilkinson | Fence | Adam will remove fence before construction. |
| 50 | Sally/Heath Emery | (2) 30" Tree Removal | Has two driveways, would like to maintain one open for business during construction. Regrade lawn suitable for mowing. |
| 42 | Clifton Foster | Tree cluster, remove some? | Contacted/no concerns |
| 39 | Thomas Ringle | (2) 24",30" Tree Removal | Contacted/no concerns |
| 32 | Steven Dunn | (2) 28" Tree Removal | May like to keep firewood when tree is cut. |
| 30 | Johnathan Libby | (3) 16",24",40" Tree Removal | Contacted/no concerns |
| 26 | Nicholas Tworog | 24" Tree Removal | Would like communication for scheduling removal of irrigation. 3 affected trees, would like one replaced. Would like disturbed driveway area paved to meet roadway. |
| 21 | Nathan Stone | Mailbox | |
| 15 | 15 Shaker Road LLC Kevin McCarthy | Mailbox @ #15/outfall pipe at #13 | Contacted/no concerns |
| 5 | Hal Tranter | Mailbox | |
| 59 | Shevchenko | New sidewalk extension | Has not returned call |
| 61 | Craig Barton | New sidewalk extension | Has not returned call |
| Outfall pipe locations: | | | |
| 45 | Roxanne Martin | Drainage Path under building Additional easment for pipe relocation? | Pave whole driveway? Flowable fill existing pipe under house? Additional easment for pipe relocation? |
| 37 | Christopher Branch | Drainage Path through trees | Additional easment for pipe relocation? |
| 13 | OTT Comm. | Drainage Path through yard | OTT owns #11. OTT has easement for #13 from owner of #15. Both OK |
| | Jim Moran | Sunset common area borders 37 Shaker outfall area | Has not returned call |

Shaker Rd Sidewalk Obstructions - Utility Poles

| CMP # | OTT/PTN # | Notes |
|--------|-----------|---|
| 253 | 19.1 | possible guidewire conflict |
| 241 | 6.1 & 6.1 | Remove unlabelled pole next to #241 |
| 233? | | In walkway at monument |
| 237 | | Broken duplicate/remove |
| | R4A/5 | In walkway near DD/corner |
| 233.01 | R4A/4 | In walkway at Stinson Hall. Duplicate pole/remove (has conduit) |
| | R4A/3 | New pole/no conflict |
| | R4A/2 | OTT will Relocate |
| | R4A/1 | OTT will Relocate |
| | R1/1 | OTT will Relocate |
| | R1/2 | OTT will Relocate |
| | R1/3 | OTT will Relocate |
| | R1/4 | New esplanade area/no conflict |
| | R1/5 | New esplanade area/no conflict |
| | R3/1/R1> | Duplicate pole, necessary for fiber to reach OTT building |

Suzanna Gallant

From: Will Haskell <whaskell@gorrillpalmer.com>
Sent: Thursday, November 19, 2020 9:28 AM
To: Deb Cabana; Sandy Carder; Alec Dodd; Suzanna Gallant
Cc: Anne Gass
Subject: RE: Shaker Road Project
Attachments: 2020-11-18 Proposal Shaker Rd Sidewalks Construction.pdf; 2344.861 plan set.pdf

Good Morning,

I have attached the current set of plans and a proposal for construction phase services. Also see below for a brief status summary on the agenda items

- Introductions
- Update on outstanding project items:
 - **Outflow pipe relocation/easements** We are working on preparing a legal description for the revised outfall easement at 45 Shaker Road. Once we complete the legal description, we anticipate that the Town Attorney would assist with preparing a new easement. To my knowledge Alec has had difficulty connecting with the right person at Sunset View Drive to discuss the possible relocation of the storm drain outfall/easement into the dedicated open space so we can avoid impacting the existing mature tree buffer along the property line between 37 Shaker Road and the Sunset View open space. I don't think we need to have the revised easements finalized before the project goes out to bid.
 - **Tree/other impacts** Alec has been coordinating this effort with homeowners and we may need to make adjustments to the plans based on his discussions with homeowners. Will we be replanting new trees where we are cutting existing trees?
 - **Utility pole relocations/eliminations** My understanding is that Alec met with Otelco and there was preliminary agreement to relocate the Otelco poles on the east side of Shaker Road between Gray Corner and the first leg of Gray Park (except for first two poles closest to Gray Corner). Between the two legs of Gray Park, we have shifted the sidewalk away from the curb to create an esplanade so the Otelco poles in that section will not be in the sidewalk.
 - **Location of addt'l crosswalk** The attached plans show the new crosswalk location near the end of the existing sidewalk on the east side of Shaker Road.
- Update on project plans:
 - **Estimate on hybrid construction inspection** See attached proposal. We have estimated the construction duration to be 18 weeks and propose to provide construction inspection services for 25 hours per week. We have no control over the contractor's schedule, so the duration of the project could be shorter or longer than what we have estimated.
 - **Water District communication** We will be providing the current set of plans to the Water District for review and will meet with them if requested. There are two existing catch basins on the east side of the road where an old water main runs directly through the basins (CB 1 at 16+25 and CB 8 at 25+15). We understand that this main is abandoned so when we replace the catch basins we understand that the old water main can be cut and capped to eliminate the conflict. We still need to add notes on the plans regarding this work.
 - Other?

- I have a response to Will Boyle's question regarding Maine DEP permitting and why it is not needed for this project. I was planning on discussing this verbally at the meeting, but can prepare a written response if you want.
- We need to discuss restoration of the storm drain trench where we are replacing the storm drain outfall pipes across private property. For example, at 45 Shaker Road the pipe runs down the middle of the driveway then we redirect it around the existing building. How will the driveway be restored? Will the Town pay for repaving the trench only, or will they pay for a pavement overlay so the finished product does not have a trench patch. There are other questions, such as at 55 Shaker – there is a split rail fence in the right-of-way that will be impacted by the widened sidewalk. Who pays for removing and relocating that fence? In reality it is probably not a remove and relocate it is probably a purchase new and relocate as the existing posts may be partially rotted below grade. At 13/15 Shaker Road the storm drain outfall trench crosses grass, pavement, landscaped areas, and gravel. We can require restoration to match existing, but still have the question of leaving a trench patch in the paved area.
- Will we be including new trees where existing trees need to be removed to accommodate the widened sidewalk?
- I'm still a bit unclear on what we are doing at 22 Shaker Road (Old Grange Hall) relative to the parking spaces off of Gray Park. I think Kathy Tombarelli had some comments here and I need to touch base with her.
- **Update on timeline** My recommendation is that we get the 37 Shaker Road stormdrain outfall location addressed before we advertise for bids, but if this is going to take significant time, then we can bid the project now and make adjustments later. December can be a difficult month for bidding because of the holidays. If we do advertise for bids in early to mid-December, my recommendation would be to lengthen the bidding period so bids are due no earlier than the week of January 11. A proposed preliminary bidding schedule would be
 - Advertise 12/11
 - Prebid Meeting (Virtual) 12/16 or 17
 - Bid Opening (Virtual) 1/14/2021

Thank you,

William C. Haskell | Principal



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November 18, 2020

Mr. Alec Dodd, Public Works Director
Town of Gray
24 Main Street
Gray, ME 04039

Re: Shaker Road Sidewalks & Drainage – Construction Phase Services

Dear Alec:

Gorrill Palmer is pleased to submit this proposal for construction phase services on the Shaker Road Sidewalk and Drainage improvement project. This work includes reconstruction of The Shaker Road sidewalks from Gray Corner to their ends. It also includes construction of a new sidewalk along the westerly side of Shaker Road from the current end to Chipman Farm Stand and the reconstruction of three storm drain outfall pipes on the west side of the road. Additionally, the catch basins in Shaker Road will be replaced. We anticipate bidding the project this winter with construction starting in the spring/summer of 2021. The project will be partially funded by a Maine DOT MPI grant.

Scope of Services

Gorrill Palmer would complete the following services as part of the construction phase. We are proposing part time construction inspection services throughout the construction phase. It is difficult to determine the exact duration of construction for this project. For this proposal, we have assumed 18 weeks of construction. We propose the following construction phase scope:

Construction Administration

1. Attend pre-construction meeting.
2. Review and respond to contractor's submittals/shop drawings.
3. Review and respond to contractor's requests for clarification.
4. Review and process contractor payment requisitions (assume 5).
5. Provide additional oversight and support with day-to-day questions that come up on the construction site, and prepare and issue design sketches and clarifications, if needed. We have assumed 6 hours per week.

Construction Inspection Services

6. Provide part-time construction observation services, including an estimated 25 hours per week for 18 weeks. These hours include construction observations, travel time, field reports, and tracking quantities and budget. We have no way of determining the exact duration of construction, therefore, if the contractor's schedule varies from this assumption, we will initiate discussions with the Town to determine how to proceed. In connection with observations of the work while it is in progress, conduct daily site observations to observe, as an experienced and qualified design professional, the progress and quality of the work. Based on information obtained during our observations, GP will determine if the Work is proceeding in general accordance with the Contract Documents, and GP shall keep the Town informed of the progress of the work.
7. Maintain a daily log of work completed based on the contract items and units. Review quantities daily with the Contractor's Superintendent.
8. Submit weekly field reports to the Town of the work completed during the previous week. Weekly reports will also include photographs of the construction work in progress.



9. Consult with and advise the Town concerning, and determining the acceptability of, substitute materials, and equipment proposed by Contractor.
10. Based on on-site observations, review Applications for Payment and supporting documentation.
11. Assist the Town in conducting a final inspection to determine if the completed Work is acceptable so that GP may recommend, in writing, that final payment be made to the Contractor.

Fee

We propose to complete this work on an hourly rate plus expenses basis for an estimated fee of **\$58,860, including estimated expenses.**

Closure

If you agree with this Proposal and associated fee, please sign and date below and return a copy for our records.

Please contact us with any questions or if you need additional information.

Sincerely,
Gorrill Palmer

A handwritten signature in black ink that reads "William C. Haskell".

William C. Haskell, PE
Principal
Enc.

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Town of Gray

Printed Name & Title

Signature

Date

GENERAL NOTES

1. THE MAINE DOT AND THE TOWN OF GRAY SHALL HAVE THE RIGHT AND AUTHORITY TO DETERMINE THE ACCEPTABILITY OF WORK AND MATERIALS IN PROGRESS OR COMPLETED. THE MAINE DOT AND THE TOWN OF GRAY SHALL HAVE THE RIGHT TO REJECT ANY WORK OR MATERIALS WHICH DO NOT CONFORM, IN ITS SOLE OPINION, TO THE PLANS OR SPECIFICATIONS.
2. ALL SIGNING, SIGNAL AND STRIPING MATERIALS AND PLACEMENT SHALL CONFORM TO THE MAINE DOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS AND WITH THE FEDERAL HIGHWAY ADMINISTRATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", 2009 EDITION.
3. ALL DISTURBED AREAS SHALL BE LOAMED AND SEEDED, UNLESS OTHERWISE NOTED, SEEDING METHOD NO. 1 SHALL BE UTILIZED ON ALL LAWNS AND DEVELOPED AREAS, AND SEEDING METHOD NO. 2 SHALL BE USED IN ALL OTHER LOCATIONS. LOAM SHALL BE PLACED TO A MINIMUM DEPTH OF 4" IN METHOD NO. 1 AREAS, AND 2" IN ALL OTHER AREAS UNLESS OTHERWISE NOTED OR DIRECTED.
4. DISPOSITION OF SURPLUS MATERIAL SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. SURPLUS MATERIAL SHALL NOT BE DISPOSED OF ON THE PROJECT SITE. DISPOSITION SHALL BE MADE ONLY AT WASTE AREAS WHICH ARE LICENSED TO ACCEPT SUCH MATERIALS, UNLESS THE MATERIALS CAN BE INCORPORATED IN FILLS IN OTHER PROJECTS OF THE CONTRACTOR.
5. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH SUBPART P OF 29 CRF PART 1926.650-.652 (CONSTRUCTION STANDARD FOR EXCAVATIONS).
6. THE CONTRACTOR SHALL CONTACT DIG-SAFE AND APPROPRIATE AUTHORITIES PRIOR TO ANY SUBSURFACE ACTIVITIES.
7. LOCATIONS OF RIGHT-OF-WAY SHOWN ON PLANS ARE APPROXIMATE ONLY.
8. BUTT JOINTS SHALL BE USED AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENT, IN ACCORDANCE WITH THE MAINE DOT STANDARD DETAILS AND SPECIFICATIONS.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY NECESSARY OPENING PERMITS AND FOR ANY COSTS ASSOCIATED WITH THEM.
10. DRIVEWAY ACCESSES SHALL BE MAINTAINED AT ALL TIMES.
11. THE CONTRACTOR SHALL SUBMIT A PLAN TO CONTROL TRAFFIC DURING CONSTRUCTION OF THE IMPROVEMENTS TO THE MAINE DOT, ENGINEER AND THE TOWN OF GRAY FOR APPROVAL, CONFORMING TO THE FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", 2009 EDITION.
12. THE CONTRACTOR SHALL COMPLY WITH THE INSURANCE REQUIREMENTS OUTLINED UNDER SECTION 110 IN THE DEPARTMENT'S "STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES" (HEREAFTER STANDARD SPECIFICATIONS). MINIMUM INSURANCE REQUIREMENTS SHALL INCLUDE AT LEAST WORKERS' COMPENSATION INSURANCE, COMMERCIAL GENERAL LIABILITY AND AUTOMOBILE LIABILITY INSURANCE AS DEFINED THEREIN. THE CONTRACTOR SHALL PROVIDE THE DEPARTMENT WITH SATISFACTORY PROOF OF SUCH INSURANCE COVERAGE. IN THE EVENT THAT SUCH INSURANCE IS TERMINATED OR CANCELED WITHOUT BEING REPLACED WITH COMPARABLE INSURANCE, THE DEPARTMENT MAY SUSPEND OR TERMINATE THE CONSTRUCTION OF ALL TRAFFIC IMPROVEMENTS IN PROGRESS AT THE TIME OF SUCH TERMINATION OR CANCELLATION.
13. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH A PERFORMANCE BOND, CERTIFIED CHECK OR OTHER NEGOTIABLE SECURITY ACCEPTABLE TO THE OWNER IN THE FULL AMOUNT OF THE COST TO CONSTRUCT SUCH IMPROVEMENTS WHICH CONFORMS TO THE GENERAL REQUIREMENTS FOR SUCH SURETY AS OUTLINED UNDER SECTION 110.2.1 IN THE STANDARD SPECIFICATIONS.
14. THE CONTRACTOR SHALL PROVIDE THE DEPARTMENT, GORRILL-PALMER AND TOWN OF GRAY WITH A SCHEDULE OF WORK FOR CONSTRUCTING THE IMPROVEMENTS AND AN EMERGENCY CONTACT LIST.
15. THE CONTRACTOR SHALL ALLOW OR ARRANGE FOR THE MAINE DOT, ITS INSPECTORS, AGENTS, EMPLOYEES, CONTRACTORS OR INVITED GUESTS, TO ENTER UPON ANY LAND OWNED OR CONTROLLED BY THE DEVELOPER OUTSIDE OF AND ADJOINING THE RIGHT-OF-WAY OF ANY HIGHWAY OR PUBLIC WAY, WHICH MAYBE USED FOR CONSTRUCTION OF THE TRAFFIC IMPROVEMENTS, AT ANY AND ALL TIMES AND FOR ANY AND ALL PURPOSES NECESSARY OR INCIDENTAL TO SUCH INSPECTION OR TESTING.
16. THE PLACEMENT OF BITUMINOUS PAVING MATERIALS SHALL BE SUBJECT TO ALL OF THE WEATHER AND SEASONAL LIMITATIONS OUTLINED UNDER SECTION 401 IN THE MAINE DOT STANDARD SPECIFICATIONS, NOVEMBER 2014 EDITION.
17. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR THE ELEVATION OF THE EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND, WHERE POSSIBLE MEASUREMENTS TAKEN IN THE FIELD. THIS INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AND DIG SAFE AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE HIS WORK AND SCHEDULE AND THE UTILITY RELOCATION WORK WITH THE PROPER UTILITY COMPANY.
18. MAINTENANCE OF EROSION CONTROL MEASURES IS OF PARAMOUNT IMPORTANCE TO THE OWNER. THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS. ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON-SITE INSPECTIONS OF THE OWNER OR THEIR REPRESENTATIVES AT NO ADDITIONAL COST TO THE OWNER. AT A MINIMUM, THE BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL SHALL BE USED.
19. ALL MATERIAL SCHEDULES SHOWN ON THE PLANS ARE FOR GENERAL INFORMATION ONLY. THE CONTRACTOR SHALL PREPARE HIS OWN MATERIAL SCHEDULES BASED UPON HIS PLAN REVIEW. ALL SCHEDULES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR PERFORMING WORK.
20. PROPERTY LINE AND R.O.W. MONUMENTS SHALL NOT BE DISTURBED BY CONSTRUCTION. IF DISTURBED, THEY SHALL BE RESET TO THEIR ORIGINAL LOCATIONS AT THE CONTRACTOR'S EXPENSE, BY A MAINE LICENSED PROFESSIONAL LAND SURVEYOR.

TEMPORARY EROSION CONTROL MEASURES

21. CONSTRUCTION SHALL NOT COMMENCE UNTIL AUTHORIZED BY THE MAINE DOT AND THE ENGINEER.
22. CURB TIP DOWNS IN SIDEWALK AREAS SHALL BE 7' AND 4' IN NON-SIDEWALK AREAS.
23. ALL NEW SIGNS SHALL HAVE HIGH INTENSITY REFLECTIVE SHEETING. WHEN WOOD POSTS ARE USED THEY SHALL BE PRESSURE TREATED.
24. THE CONTRACTOR SHALL COMPLETE THE WORK WITHIN THE RIGHT-OF-WAY, AND WILL BE RESPONSIBLE IF THEY TRESPASS ON PRIVATE PROPERTY.
25. ALL PAVEMENT MARKINGS THAT CONFLICT WITH THE PROPOSED SHALL BE REMOVED IN ACCORDANCE WITH MAINE DOT SPECIFICATIONS.
26. A TACK COAT OF EMULSIFIED ASPHALT, RS-1 OR HFMS-1 SHALL BE APPLIED TO ANY EXISTING PAVEMENT AT A RATE OF APPROXIMATELY 0.025 GALLON/SY, AND ON MILLED PAVEMENT APPROXIMATELY 0.050 GALLONS/SY. A FOG COAT OF EMULSIFIED ASPHALT SHALL BE BETWEEN SHIM/INTERMEDIATE COURSE AND THE SURFACE COURSE, AT A RATE NOT TO EXCEED 0.025 GALLONS/SY.
27. THE CONTRACTOR SHALL SUBMIT A QC PLAN AS OUTLINED UNDER MAINE DOT STANDARD SPECIFICATIONS, NOVEMBER 2014 EDITION DIVISION 400, PAVEMENTS, SECTION 401, PARAGRAPH 401.19 FOR APPROVAL BY THE MAINE DOT AND ENGINEER. THE ACCEPTANCE METHOD SHALL BE METHOD D. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE OF ANY PAVING. THE DENSITY REQUIREMENTS AND DISINCENTIVE SHALL APPLY AS OUTLINED IN SECTION 401.204 OF THE MAINE DOT, SECTION 401. CORES WILL NOT BE REQUIRED. THE TOWN MAY TAKE SAMPLES FOR TESTING AT THEIR DISCRETION TO DETERMINE IF THE MIX IS WITHIN THE TOLERANCES LISTED IN TABLE B OF SECTION 401.204.
28. THE CONTRACTOR SHALL ANTICIPATE THAT GROUNDWATER WILL BE ENCOUNTERED DURING CONSTRUCTION AND SHALL INCLUDE SUFFICIENT COSTS WITHIN THEIR BID TO PROVIDE DEWATERING AS NECESSARY. NO SEPARATE PAYMENT SHALL BE MADE TO THE CONTRACTOR FOR DEWATERING.
29. REMOVING AND RESETTING SIGNS SHALL BE INCIDENTAL TO THE PROJECT
30. ALL WORK COMPLETED UNDER THIS CONTRACT SHALL BE GOVERNED BY AND BE IN CONFORMITY WITH THE STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES (DECEMBER 2014 EDITION), AND THE STANDARD DETAILS (DECEMBER 2014) EXCEPT AS MODIFIED BY THE PLANS OR SPECIFICATION SPECIAL PROVISIONS AND ANY AND ALL CORRECTIONS, REVISIONS OR ADDITIONS ISSUED BY MAINE DOT.
31. MAINTENANCE OF TRAFFIC PER MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 2009 EDITION.
32. CONTRACTOR SHALL USE THE B.M.P. (BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL, 2014) AS A MINIMUM STANDARD.
33. CATCH BASINS SHALL BE 4' DIAMETER A1-C PER MDOT STANDARD DETAILS. HEADSTONES SHALL BE PLACED IN LOCATIONS NOTED ON PLAN VIEWS.
34. ALL EXISTING DRAINAGE CATCH BASIN AND OUTLET INFORMATION SHALL BE FIELD VERIFIED PRIOR TO ORDERING NEW STRUCTURES.
35. ALL CLEARING AND TRIMMING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT WILL BE MADE. THE ACTUAL LINES FOR CLEARING AND TRIMMING SHALL BE ESTABLISHED BY THE CONTRACTOR AND APPROVED IN THE FIELD BY THE ENGINEER.
36. EXISTING CONDITIONS ARE BASED ON SURVEY COMPLETED BY TITCOMB ASSOCIATES.
37. ALL EXISTING WATER VALVE COVERS AND ANY OTHER EXISTING UTILITIES SHALL BE ADJUSTED TO GRADE BY THE APPROPRIATE UTILITY COMPANY.
38. ANY NECESSARY CLEANING OF EXISTING PAVEMENT PRIOR TO PAVING SHALL BE INCIDENTAL TO THE RELATED PAVING ITEMS.
39. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING ALL EXISTING MAILBOXES TO ENSURE THAT THE MAIL WILL BE DELIVERABLE. MAILBOXES SHALL BE RELOCATED ACCORDING TO THE MAILBOX DETAIL OR AS DIRECTED BY THE ENGINEER. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK; IT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
40. COORDINATE WITH APPROPRIATE UTILITY COMPANY FOR SUPPORT OF UTILITY POLES AS NECESSARY.
41. ANY DAMAGE CAUSED TO THE EXISTING UTILITIES BY THE CONTRACTORS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO SEPARATE PAYMENT SHALL BE MADE.

1. SILTATION FENCE SHALL BE INSTALLED DOWNGRADIENT OF ANY DISTURBED AREAS TO TRAP RUNOFF-BORNE SEDIMENTS UNTIL GRASS AREAS ARE REVEGETATED. THE SILT FENCE SHALL BE INSTALLED PER THE DETAILS PROVIDED ON THIS PLAN AND INSPECTED BEFORE AND IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. REPAIRS SHALL BE MADE IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THE FENCE LINE. IF THERE ARE SIGNS OF UNDERCUTTING AT THE CENTER OR THE EDGES, OR IMPOUNDING OF LARGE VOLUMES OF WATER BEHIND THE FENCE, THE BARRIER SHALL BE REPLACED WITH A STONE CHECK DAM.
2. SHAKER ROAD AND GRAY PARK SHALL BE SWEEPED TO CONTROL MUD, DUST, AND THE TRACKING OF MATERIAL OFF THE SITE AND ONTO THE SURROUNDING ROADWAYS, AS NECESSARY.
3. SILT FENCING WITH A MINIMUM STAKE SPACING OF 6 FEET SHALL BE USED, UNLESS THE FENCE IS SUPPORTED BY WIRE FENCE REINFORCEMENT OF MINIMUM 14 GAUGE AND WITH A MAXIMUM MESH SPACING OF 6 INCHES, IN WHICH CASE STAKES MAY BE SPACED A MAXIMUM OF 10 FEET APART. THE BOTTOM OF THE FENCE SHALL BE ANCHORED.
4. WATER AND/OR CALCIUM CHLORIDE SHALL BE FURNISHED AND APPLIED IN ACCORDANCE WITH MDOT SPECIFICATIONS - SECTION 637 - DUST CONTROL.
5. LOAM AND SEED IS INTENDED TO SERVE AS THE PRIMARY PERMANENT REVEGETATIVE MEASURE FOR ALL DENUDEED AREAS NOT PROVIDED WITH OTHER EROSION CONTROL MEASURES, SUCH AS RIPRAP.

STANDARDS FOR TIMELY STABILIZATION OF CONSTRUCTION SITES

1. THE CONTRACTOR SHALL STABILIZE THE DISTURBED SLOPES WITH LOAM AND SEED AND EROSION CONTROL BLANKET (NORTH AMERICAN GREEN SC150 OR APPROVED EQUIVALENT,) INSTALLED AND ANCHORED IN ACCORDANCE WITH MANUFACTURER'S REQUIREMENTS.
2. DITCH FLOW LINE SHALL BE STABILIZED WITH NORTH AMERICAN GREEN SC150 OR EQUIVALENT INSTALLED AND POSITIONED IN ACCORDANCE WITH MANUFACTURER'S REQUIREMENTS.

U:\2344.861_Gray_Shaker Road Sidewalk_V - CAD\DWG\2344.861-NOTES.dwg 11/19/2020 8:44 AM

| Rev. | Date | Revision |
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| Issued For | Date | By |
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|---|------------|-------------------|
| Design: CJD | Draft: LAN | Date: NOV 2020 |
| Checked: WCH | Scale: | Job No.: 2344.861 |
| File Name: 2344.861-NOTES.dwg | | |
| This plan shall not be modified without written permission from Gorrill Palmer. Any alterations, authorized or otherwise, shall be at the user's sole risk and without liability to Gorrill Palmer. | | |



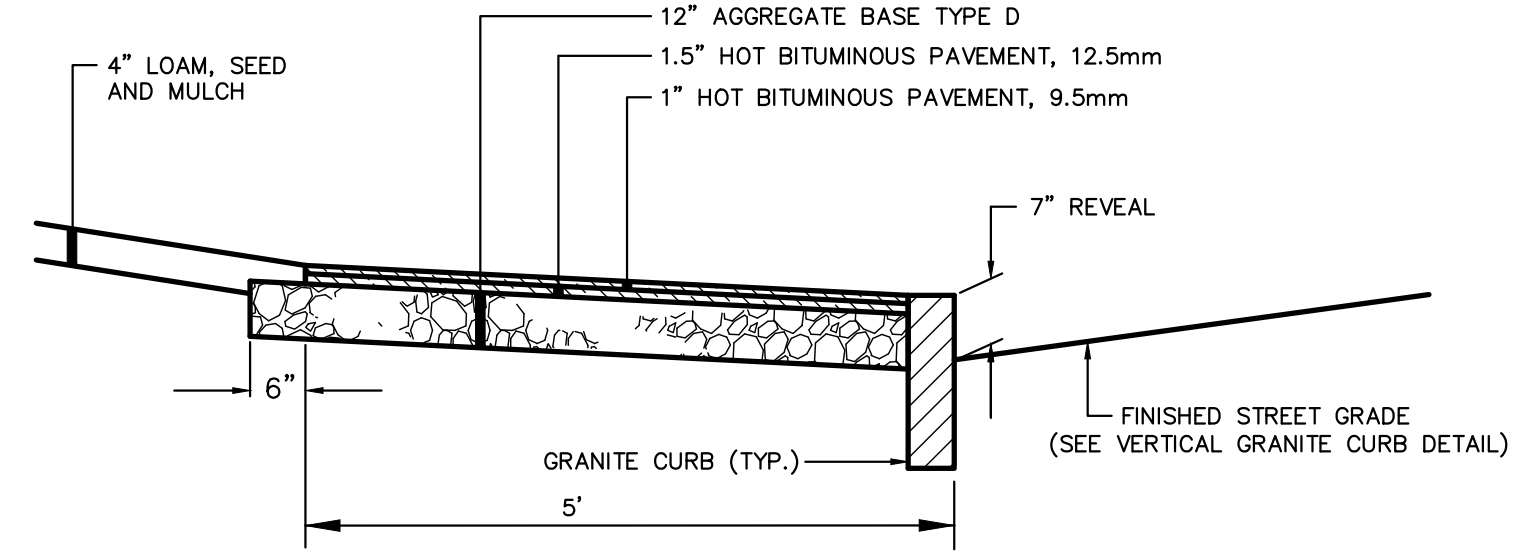
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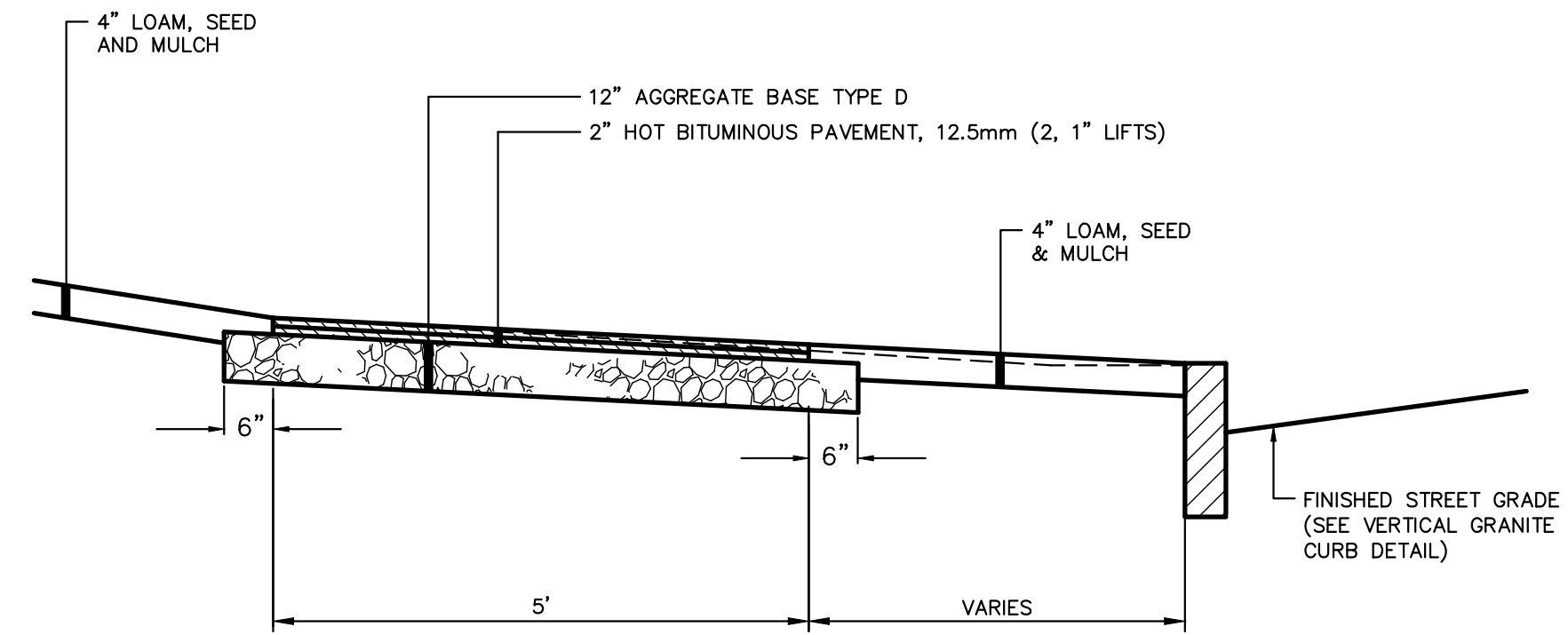
NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

| | |
|---------------|--|
| Drawing Name: | General Notes |
| Project: | Shaker Road Sidewalk and Drainage Improvements Gray, Maine |
| Client: | Town of Gray 24 Main Street, Gray, Maine 04039 |

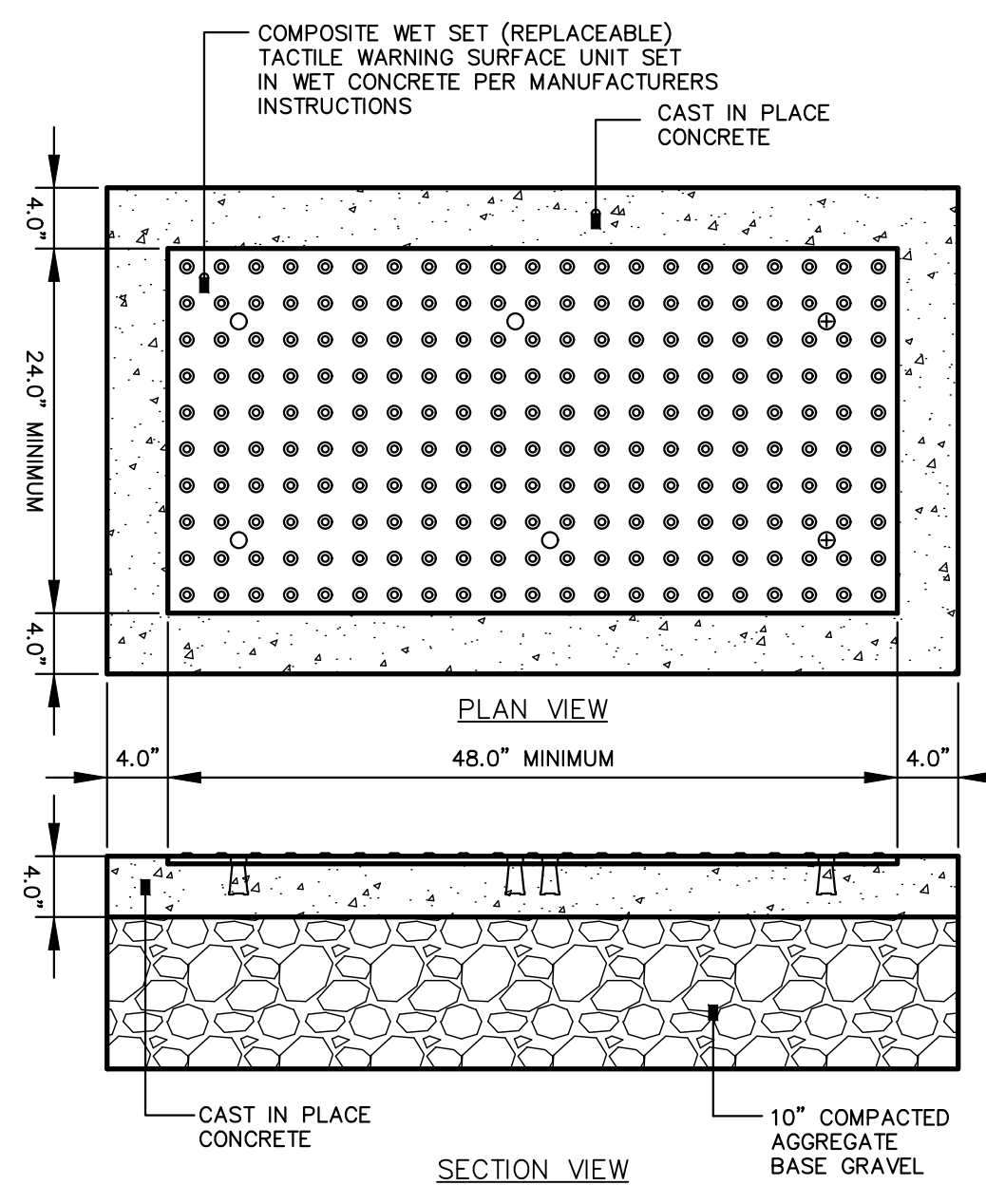
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| Drawing No. | 2 |
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BITUMINOUS SIDEWALK
NOT TO SCALE

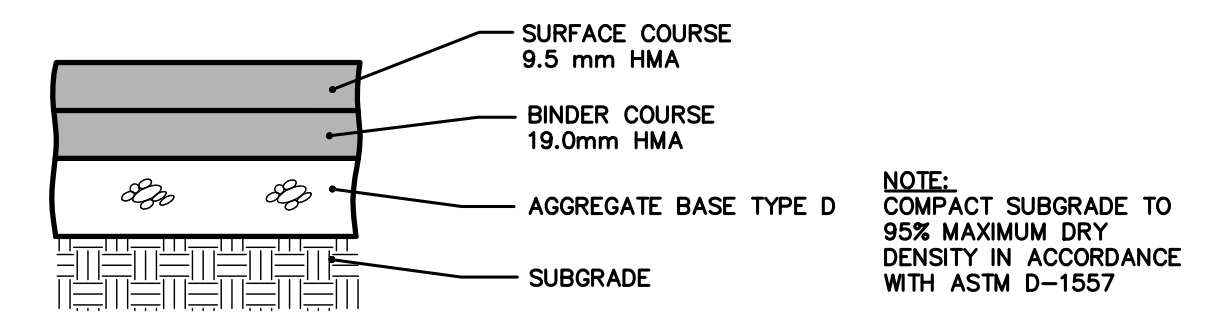


BITUMINOUS SIDEWALK WITH ESPLANADE
NOT TO SCALE



- NOTES:
1. ALL DETECTABLE WARNING PLATES SHALL BE UNCOATED CAST IRON. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
 2. CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MDOT CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4,000 PSI. THE CONCRETE SHALL BE SEALED PRIOR TO SETTING PANELS. THE EXPOSED CONCRETE BORDER SHALL RECEIVE A GROOVED EDGE BETWEEN THE TILE AND CONCRETE, ALONG WITH A UNIFORM BROOM FINISH PERPENDICULAR TO THE FLOW OF PEDESTRIAN TRAFFIC.
 3. TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. TRUNCATED DOME BRICKS AND GRANITE PAVERS ARE NOT ALLOWED.
 4. SIZE: THE DETECTABLE WARNING PLATES SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE PLATE WIDTH PLUS CONCRETE SHALL EXTEND THE WIDTH OF THE CURB RAMP, LANDING OR BLENDED TRANSITION TO THE STREET.
 5. ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.

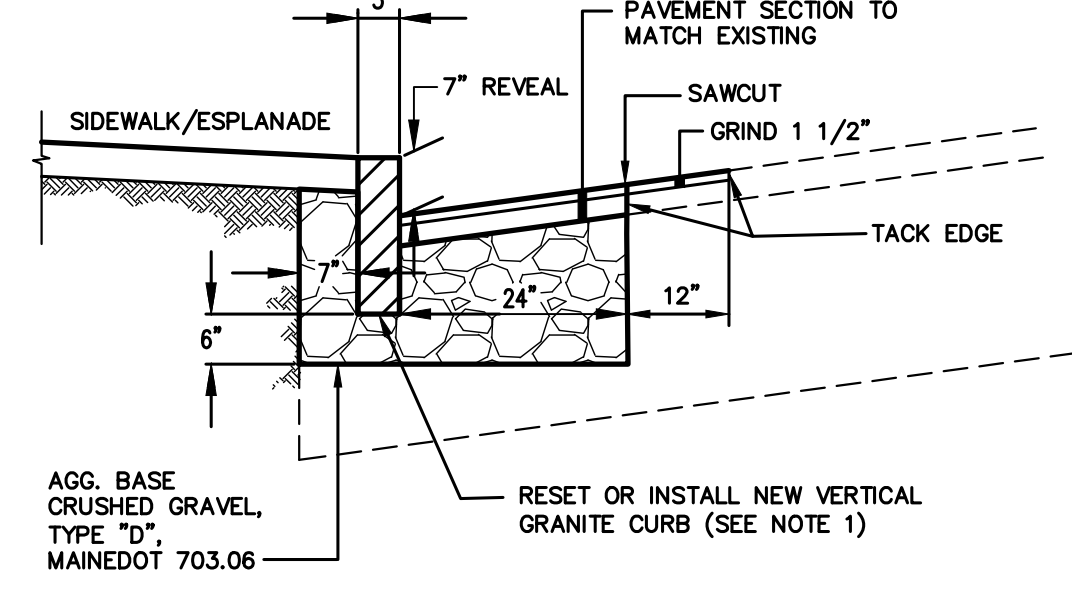
SIDEWALK RAMP DETECTABLE WARNING TILE
NOT TO SCALE



| LAYERS | SHAKER ROAD | GRAY PARK |
|----------------------------|-------------|-----------|
| SURFACE COURSE 12.5 mm HMA | 1.25" | 1.25" |
| BINDER COURSE 19.0mm HMA | 2" | 2" |
| AGGREGATE BASE TYPE D | 15" | 15" |

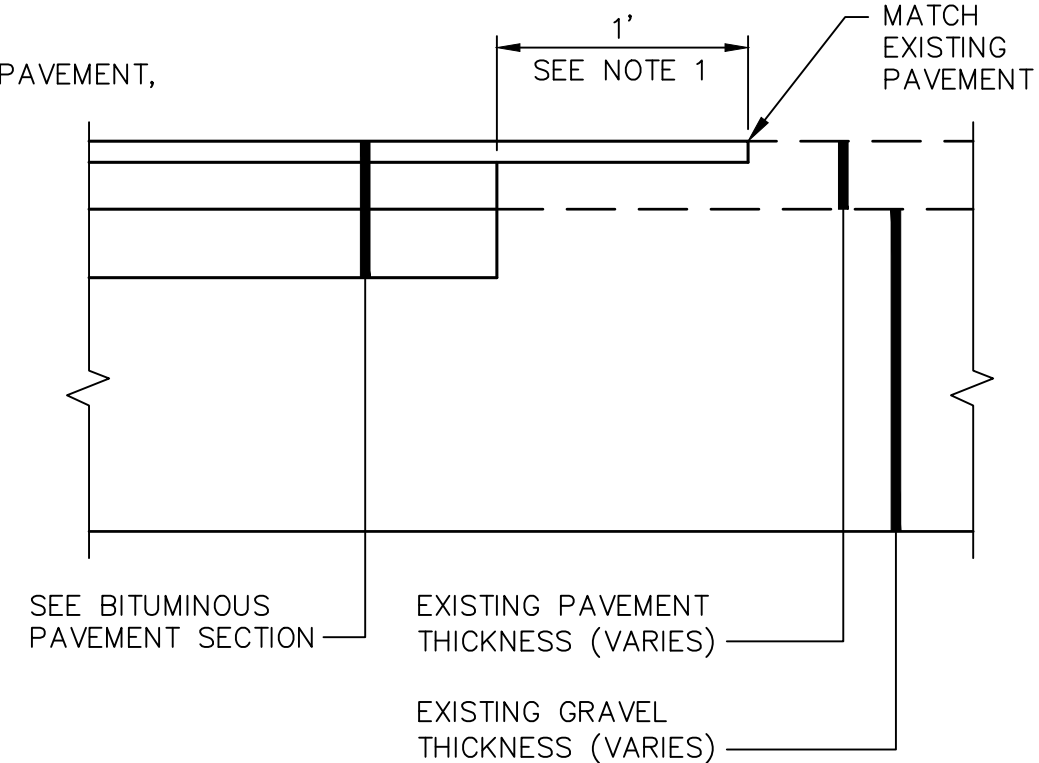
BITUMINOUS PAVEMENT SECTION
NOT TO SCALE

- NOTES:
1. WHERE NEW CURB IS CALLED OUT FOR ON THE PLANS, VERTICAL GRANITE CURB SHALL MEET THE REQUIREMENTS OF SECTION 609 OF THE MAINE DEPARTMENT OF TRANSPORTATION (MDOT) STANDARD SPECIFICATIONS, LATEST REVISION, AND THE DIMENSIONS SHOWN ON THE DRAWINGS.
 2. PAVEMENT CROSS SLOPE SHALL MATCH EXISTING ROADWAY CROSS SLOPE, UNLESS NOTED OTHERWISE.

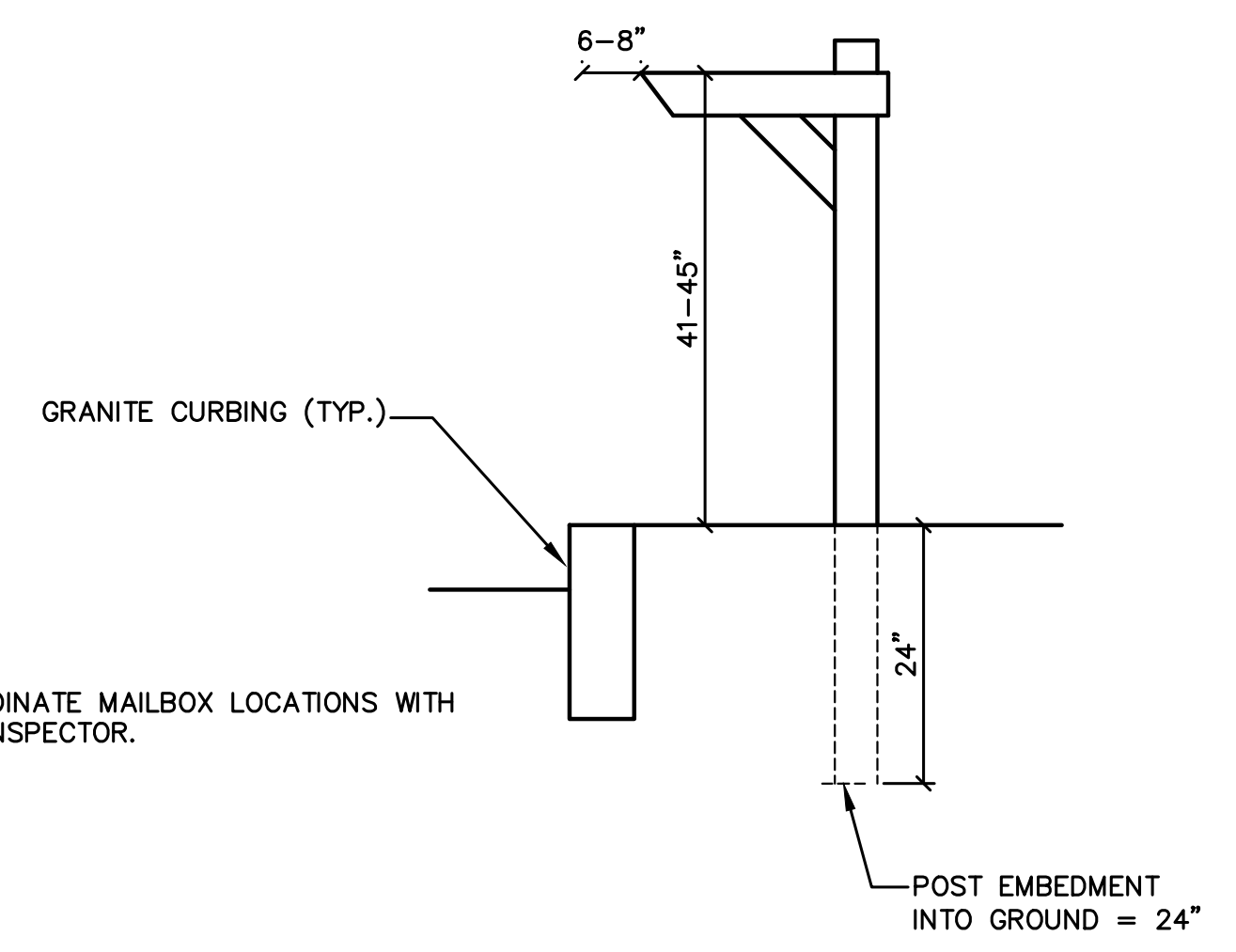


VERTICAL GRANITE CURB & SAWCUT
NOT TO SCALE

- NOTE:
1. GRIND EXISTING PAVEMENT, TO 1 1/2" DEPTH.

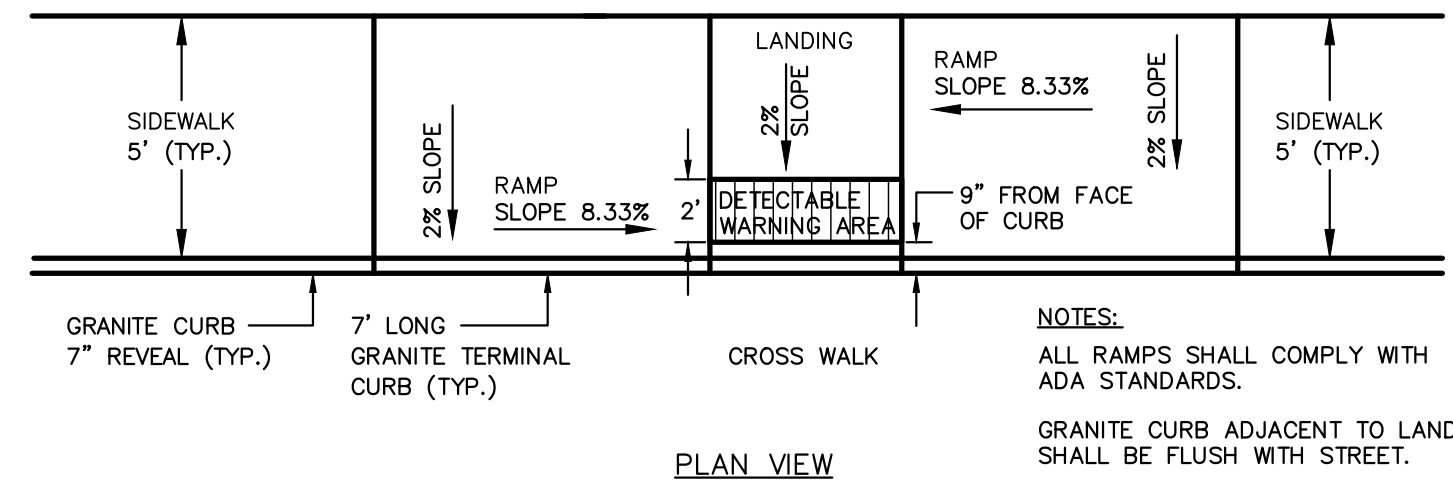


PAVEMENT SHIPLAP JOINT
NOT TO SCALE

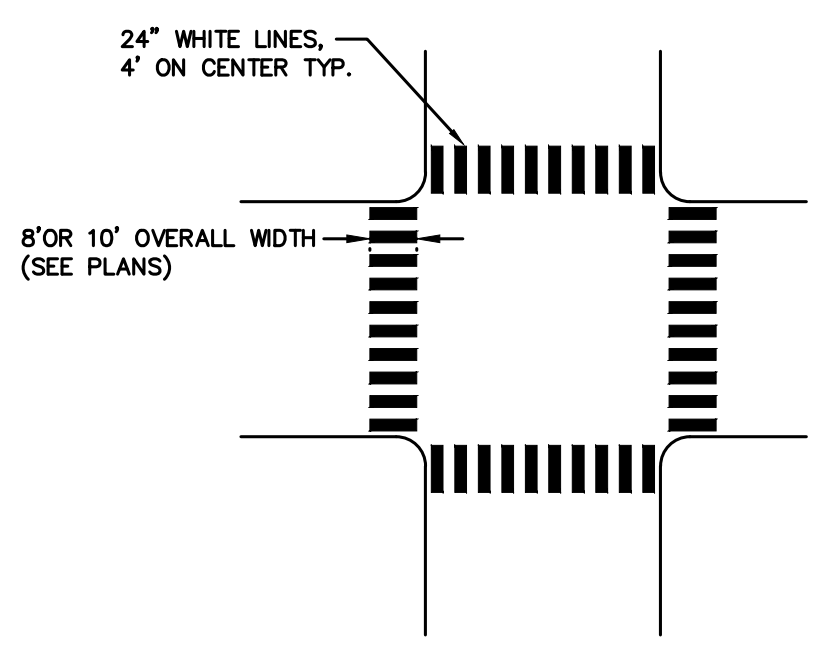


- NOTES:
1. COORDINATE MAILBOX LOCATIONS WITH THE INSPECTOR.

MAILBOX DETAIL
NOT TO SCALE



PARALLEL SIDEWALK RAMP
NOT TO SCALE



CROSSWALK MARKINGS
NOT TO SCALE

95% PLANS

NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

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| Design: CJD | Draft: LAN | Date: NOV 2020 |
| Checked: WCH | Scale: AS SHOWN | Job No.: 2344.861 |
| File Name: 2344.861-DET.dwg | | |

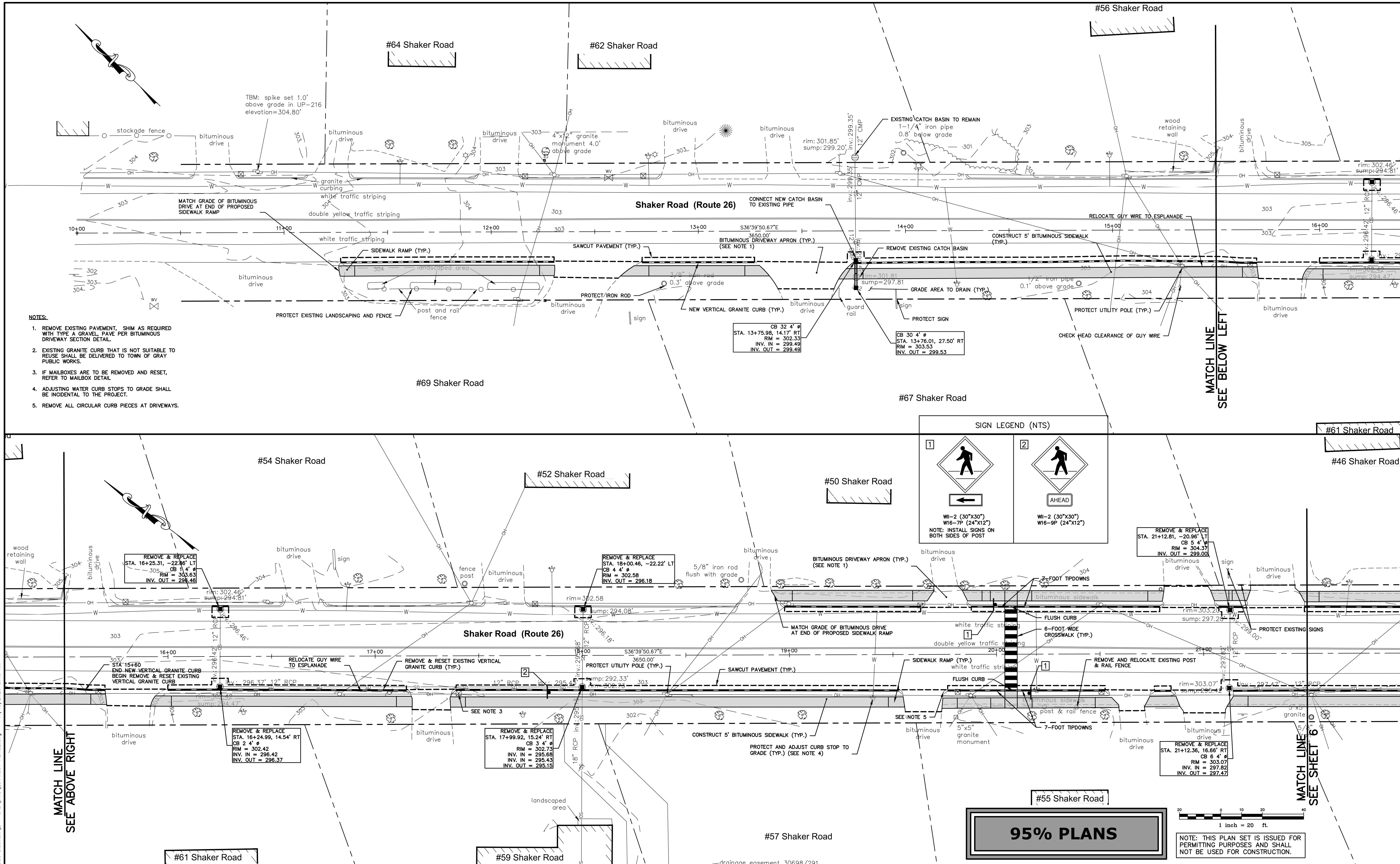
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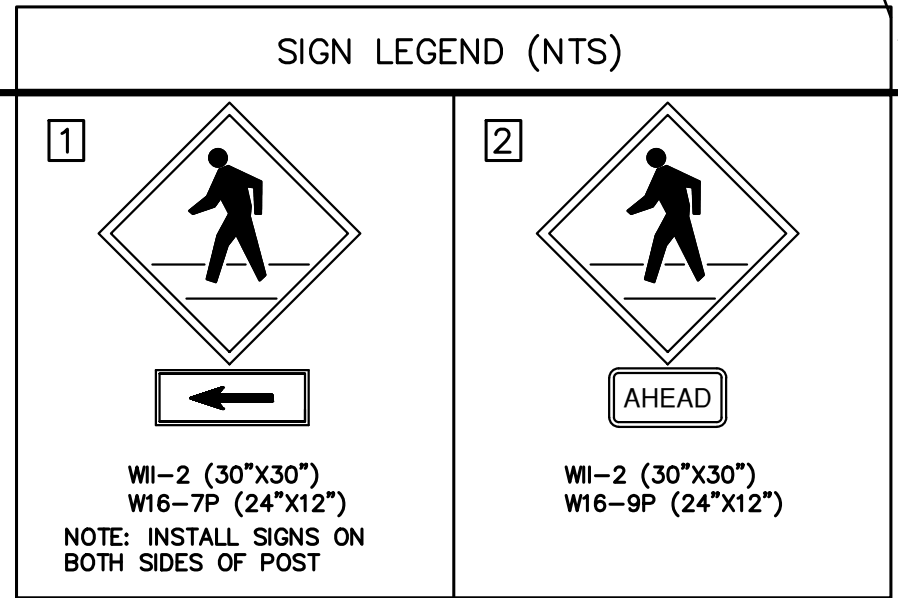
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|---------------|--|
| Drawing Name: | Sidewalk and Roadway Details |
| Project: | Shaker Road Sidewalk and Drainage Improvements Gray, Maine |
| Client: | Town of Gray 24 Main Street, Gray, Maine 04039 |

Drawing No.
3



- NOTES:**
1. REMOVE EXISTING PAVEMENT, SHIM AS REQUIRED WITH TYPE A GRAVEL, PAVE PER BITUMINOUS DRIVEWAY SECTION DETAIL.
 2. EXISTING GRANITE CURB THAT IS NOT SUITABLE TO REUSE SHALL BE DELIVERED TO TOWN OF GRAY PUBLIC WORKS.
 3. IF MAILBOXES ARE TO BE REMOVED AND RESET, REFER TO MAILBOX DETAIL.
 4. ADJUSTING WATER CURB STOPS TO GRADE SHALL BE INCIDENTAL TO THE PROJECT.
 5. REMOVE ALL CIRCULAR CURB PIECES AT DRIVEWAYS.



95% PLANS

NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

U:\2344.861_Gray_Shaker Road Sidewalk\2 - CAD\DWG\2344.861-PBASE.dwg 11/19/2020 8:45 AM

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Design: CJD Draft: LAN Date: NOV 2020
 Checked: WCH Scale: 1"=20' Job No.: 2344.861
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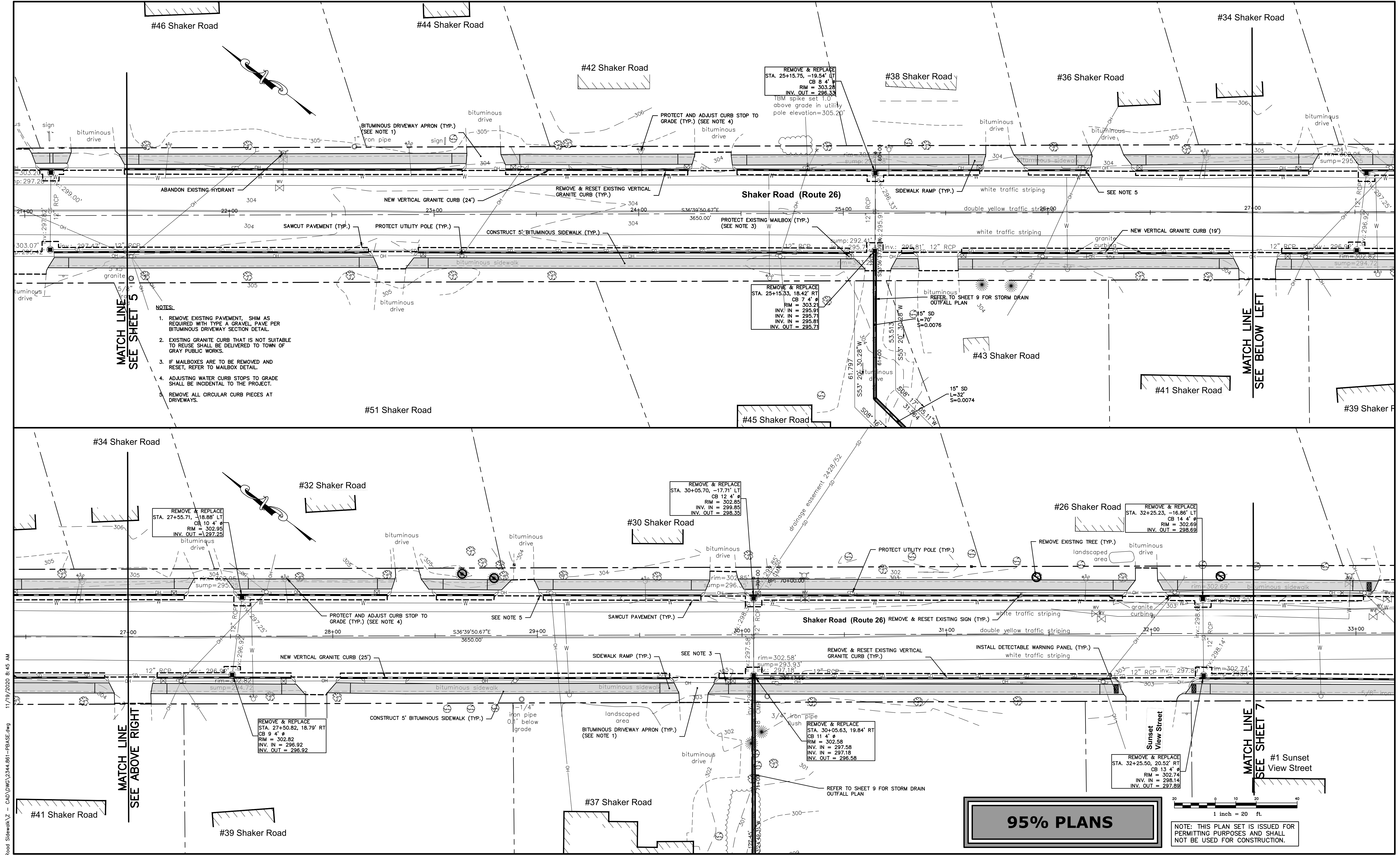
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Drawing Name: **Sidewalk Improvement Plan**
 Project: **Shaker Road Sidewalk and Drainage Improvements**
 Client: **Town of Gray**
 24 Main Street, Gray, Maine 04039

Drawing No. **5**



- NOTES:**
1. REMOVE EXISTING PAVEMENT, SHIM AS REQUIRED WITH TYPE A GRAVEL, PAVE PER BITUMINOUS DRIVEWAY SECTION DETAIL.
 2. EXISTING GRANITE CURB THAT IS NOT SUITABLE TO REUSE SHALL BE DELIVERED TO TOWN OF GRAY PUBLIC WORKS.
 3. IF MAILBOXES ARE TO BE REMOVED AND RESET, REFER TO MAILBOX DETAIL.
 4. ADJUSTING WATER CURB STOPS TO GRADE SHALL BE INCIDENTAL TO THE PROJECT.
 5. REMOVE ALL CIRCULAR CURB PIECES AT DRIVEWAYS.

95% PLANS

NOTE: THIS PLAN SET IS ISSUED FOR PERMITTING PURPOSES AND SHALL NOT BE USED FOR CONSTRUCTION.

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Design: CJD Draft: LAN Date: NOV 2020
 Checked: WCH Scale: 1"=20' Job No.: 2344.861
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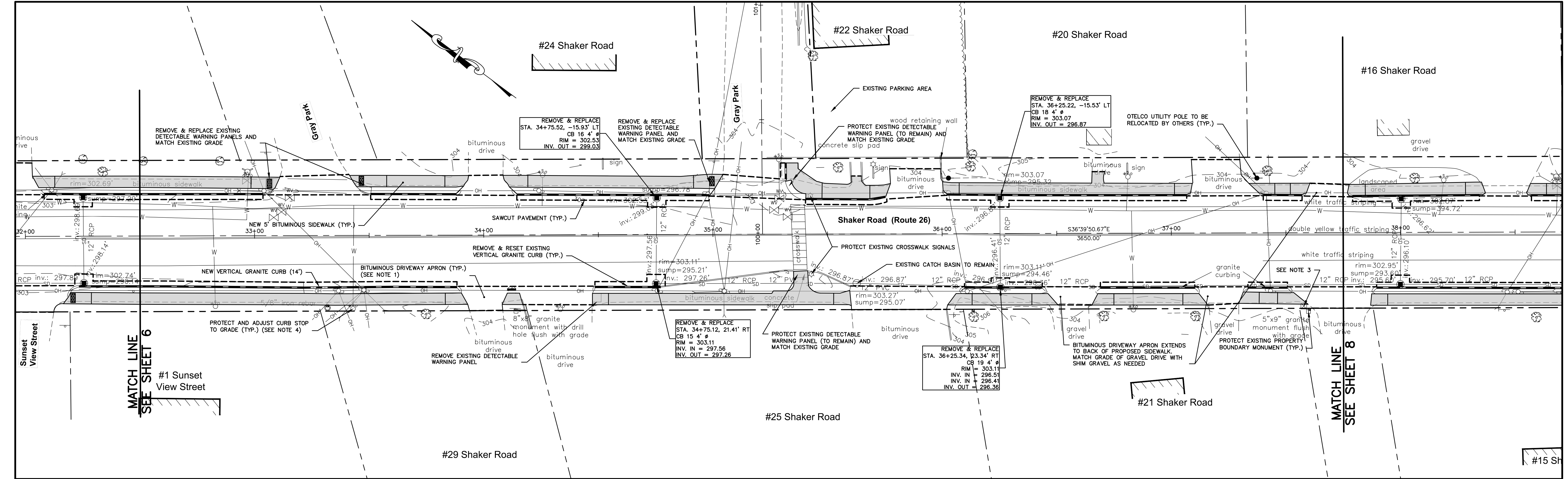
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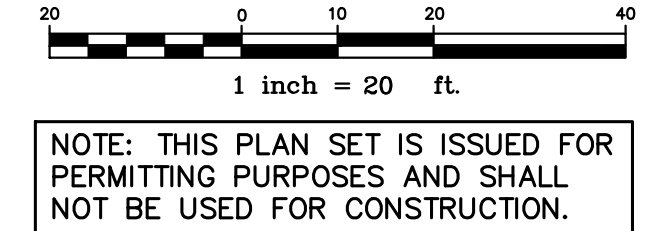
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| Drawing Name: | Sidewalk Improvement Plan |
| Project: | Shaker Road Sidewalk and Drainage Improvements Gray, Maine |
| Client: | Town of Gray 24 Main Street, Gray, Maine 04039 |

Drawing No.
6



- NOTES:**
1. REMOVE EXISTING PAVEMENT, SHIM AS REQUIRED WITH TYPE A GRAVEL, PAVE PER BITUMINOUS DRIVEWAY SECTION DETAIL.
 2. EXISTING GRANITE CURB THAT IS NOT SUITABLE TO REUSE SHALL BE DELIVERED TO TOWN OF GRAY PUBLIC WORKS.
 3. IF MAILBOXES ARE TO BE REMOVED AND RESET, REFER TO MAILBOX DETAIL.
 4. ADJUSTING WATER CURB STOPS TO GRADE SHALL BE INCIDENTAL TO THE PROJECT.
 5. REMOVE ALL CIRCULAR CURB PIECES AT DRIVEWAYS.
 6. ACCORDING TO SURVEY, THERE IS A 1" IRON ROD AT THIS APPROXIMATE LOCATION. COORDINATE WITH A P.L.S. TO MOVE ROD BEFORE PAVING.

95% PLANS



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| Checked: WCH | Scale: 1"=20' | Job No.: 2344.861 |
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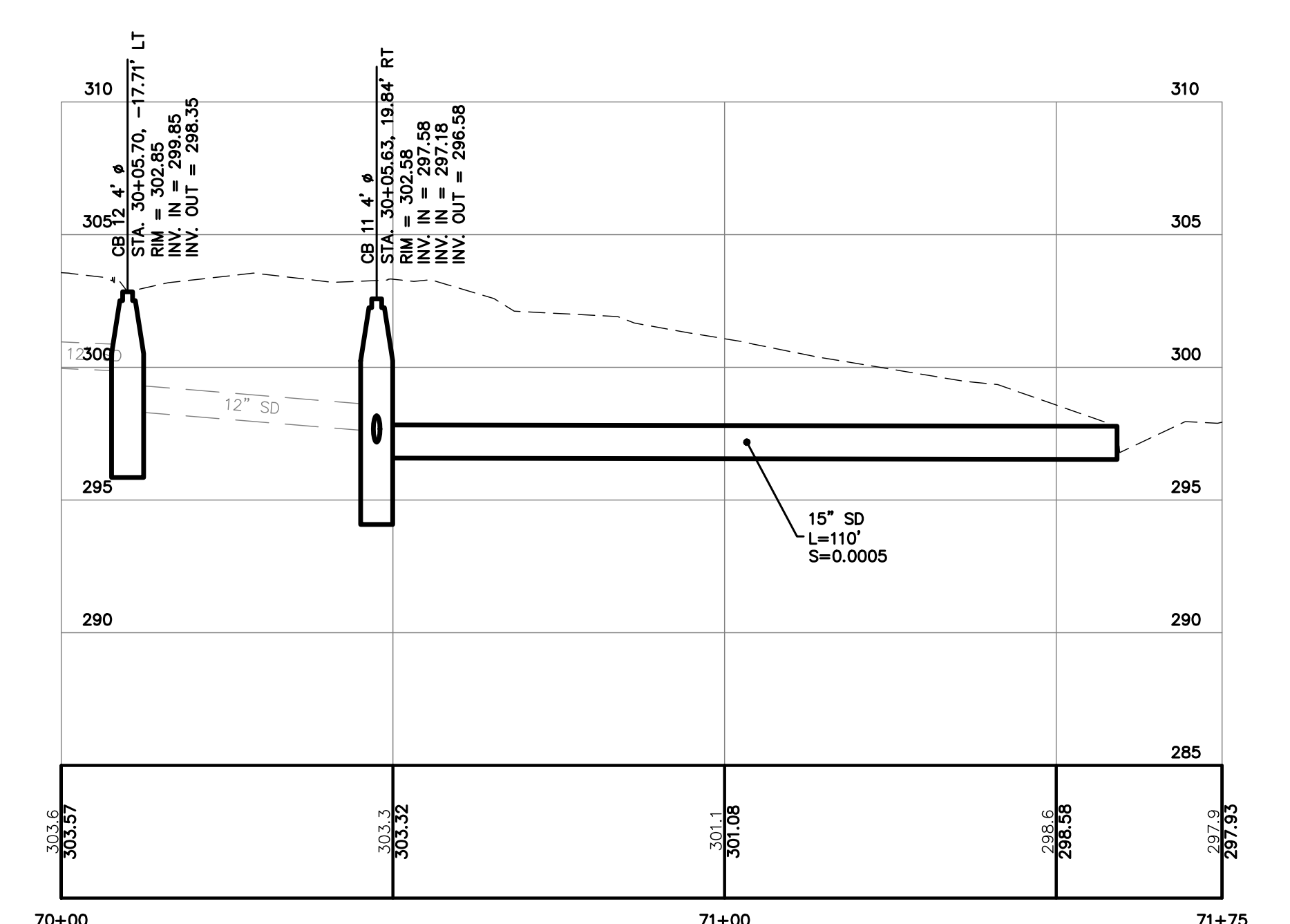
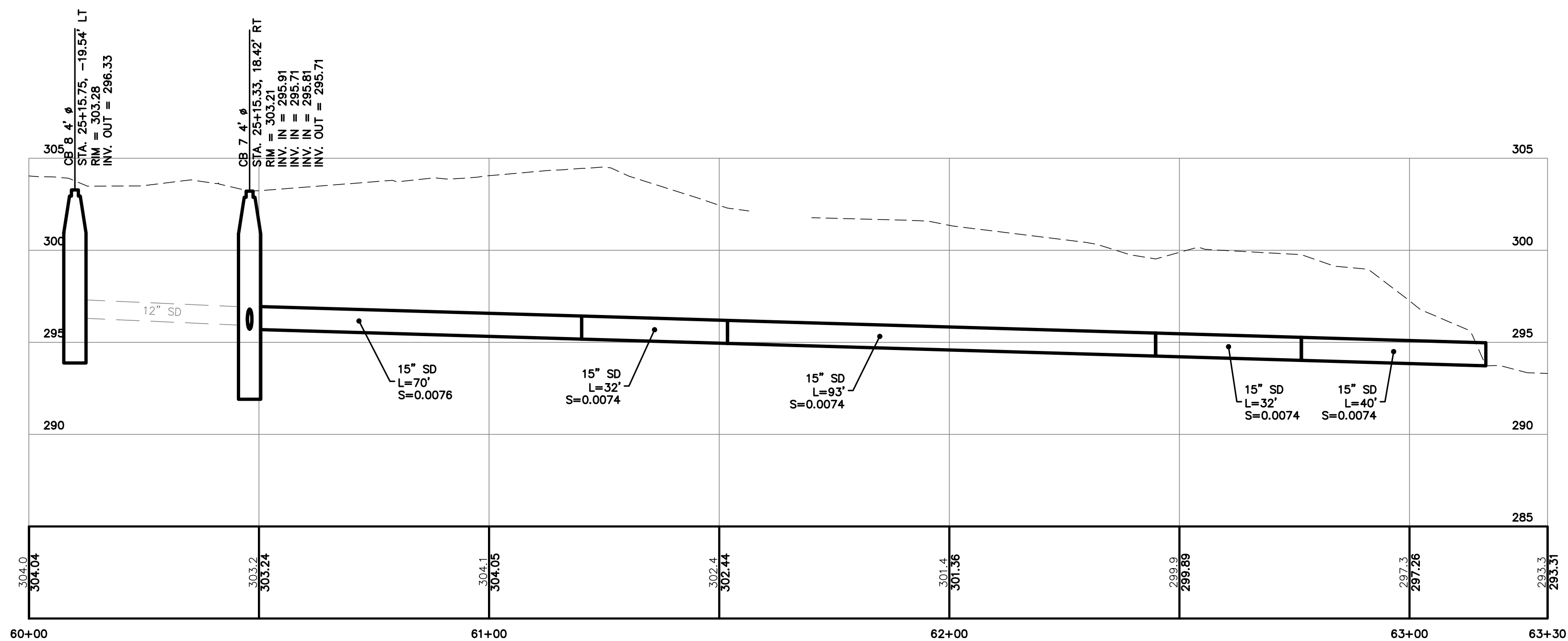
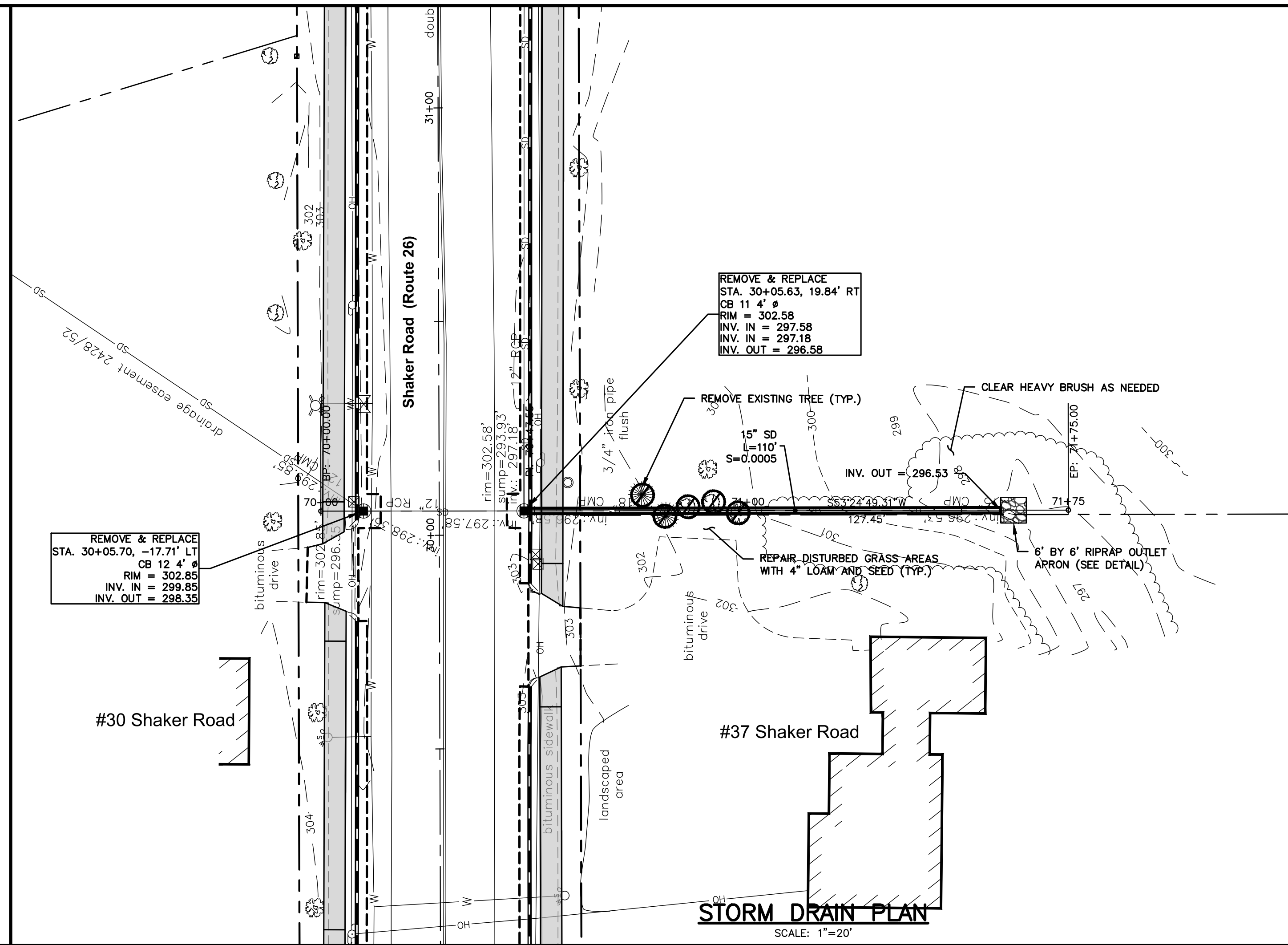
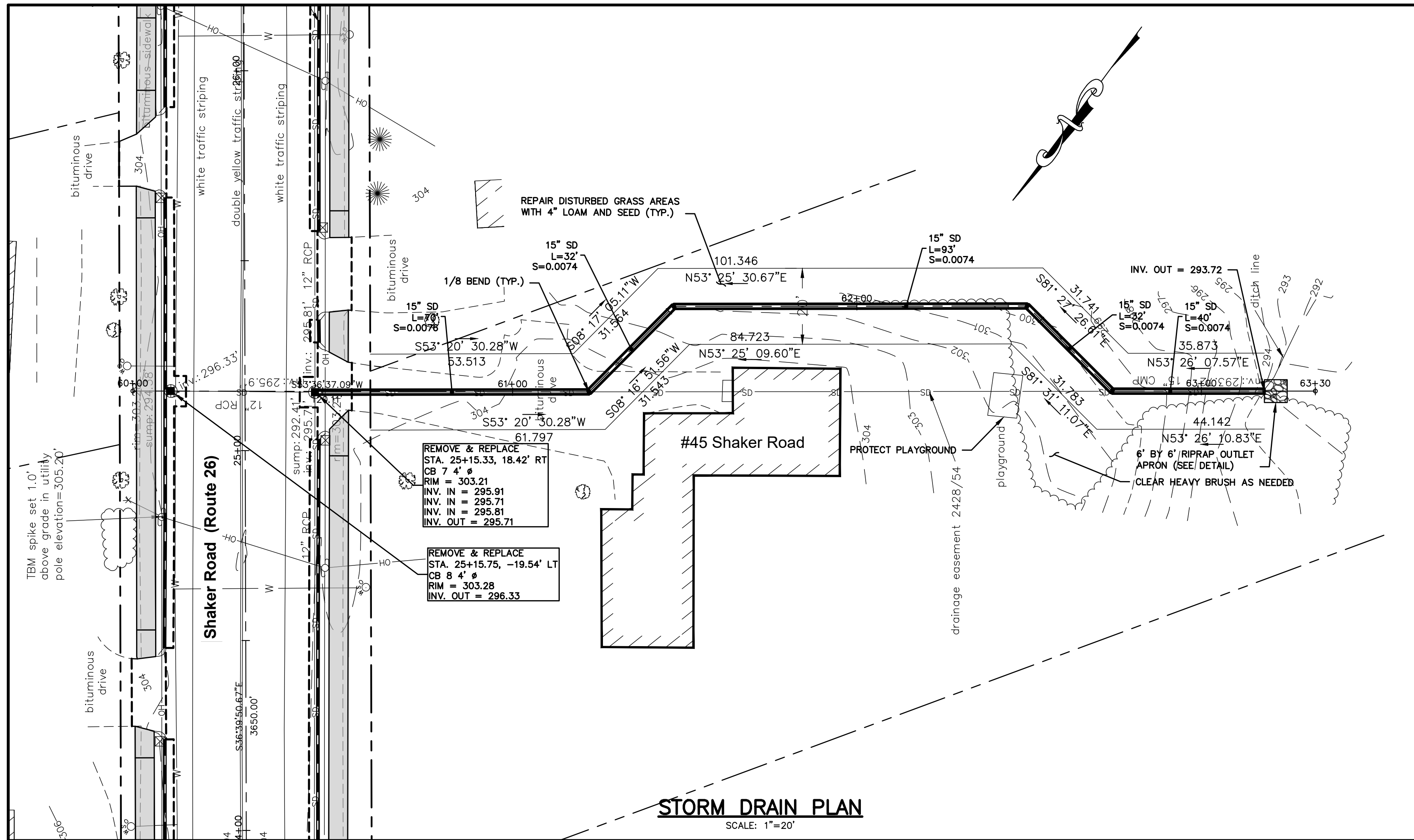
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| Drawing Name: | Sidewalk Improvement Plan |
| Project: | Shaker Road Sidewalk and Drainage Improvements Gray, Maine |
| Client: | Town of Gray 24 Main Street, Gray, Maine 04039 |

Drawing No.
7



95% PLANS

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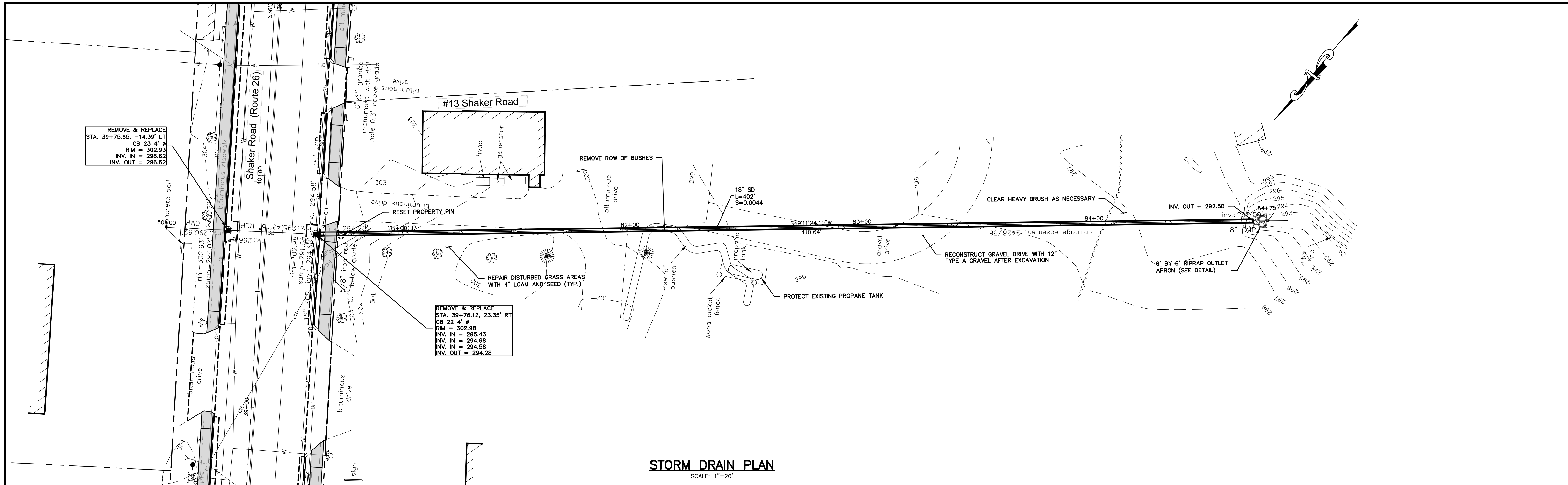
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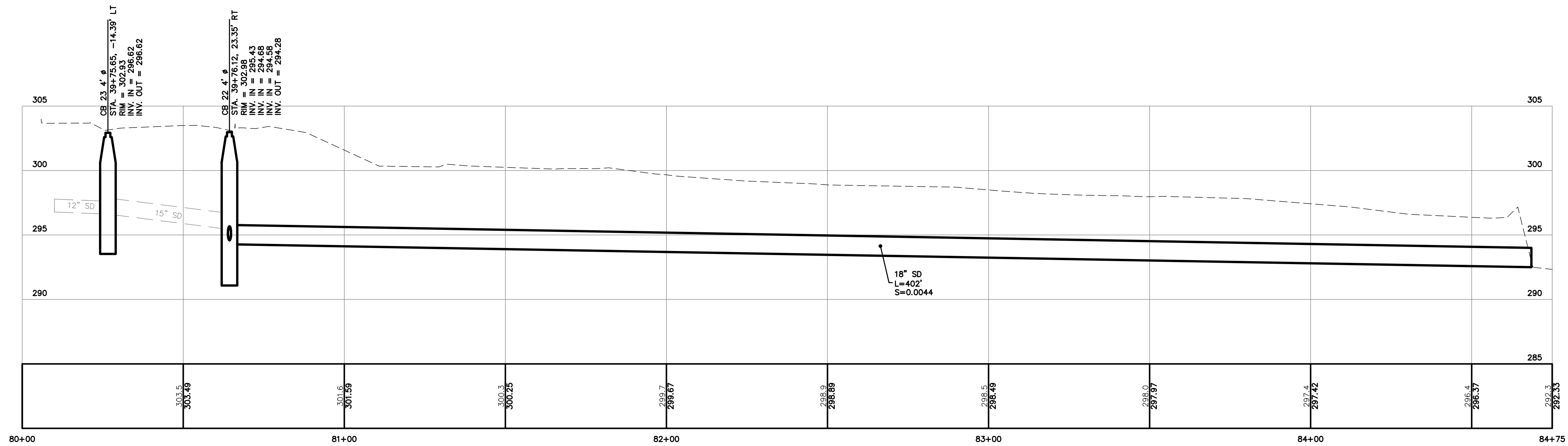
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Drawing Name: Storm Drain Plan and Profile
 Project: Shaker Road Sidewalk and Drainage Improvements
 Gray, Maine
 Client: Town of Gray
 24 Main Street, Gray, Maine 04039

Drawing No. 9

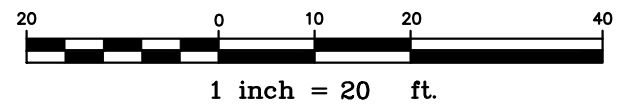


STORM DRAIN PLAN
SCALE: 1"=20'



STORM DRAIN PROFILE
1"=5' VERT.

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| Project: | Shaker Road Sidewalk and Drainage Improvements Gray, Maine |
| Client: | Town of Gray 24 Main Street, Gray, Maine 04039 |

Drawing No.
10