



# City Commission Work Session

## Agenda

**April 15, 2021 @ 1:00 pm**

Virtual

### welcome

Agendas and all backup material supporting each agenda item are accessible via the city's website at [cityofwinterpark.org/bpm](http://cityofwinterpark.org/bpm) and include virtual meeting instructions.

### assistance & appeals

Persons with disabilities needing assistance to participate in any of these proceedings should contact the City Clerk's Office ([407-599-3277](tel:407-599-3277)) at least 48 hours in advance of the meeting.

"If a person decides to appeal any decision made by the Board with respect to any matter considered at this hearing, a record of the proceedings is needed to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based." (F.S. 286.0105).

### please note

Times are projected and subject to change.

- 
1. Call to Order
  2. Discussion Item(s)
    - a. Discussion on OAO Zoning Code 2 hours
  3. Adjournment



## City Commission Work Session

# agenda item

item type Discussion Item(s)	meeting date April 15, 2021
prepared by Allison McGillis	approved by Bronce Stephenson, Michelle Neuner, Randy Knight
board approval Completed	
strategic objective	

### subject

Discussion on OAO Zoning Code

### motion / recommendation

### background

### alternatives / other considerations

### fiscal impact

### ATTACHMENTS:

[Zoning Code OAO 2.0 Draft 3.29.2021.docx](#)

### ATTACHMENTS:

[Commission Input Worksheet - 3.18.21 - DeCiccio.xlsx](#)

### ATTACHMENTS:

[Anderson Adds 4-3-21- ComCooper OAO Input Worksheet-Jan21.xlsx](#)

OAO ZONING CODE DRAFT  
3.29.2021 VERSION

---- = text/map that still need edits  
---- = text that still requires Commission discussion  
---- = Cooper comments sent to staff that still requires further discussion  
---- = DeCiccio comments sent to staff that still requires further discussion

ORDINANCE NO. \_\_\_\_\_

AN ORDINANCE OF THE CITY OF WINTER PARK,  
FLORIDA, AMENDING CHAPTER 58 "LAND  
DEVELOPMENT CODE" ARTICLE III, "ZONING" SO AS  
TO ADOPT A NEW ZONING DISTRICT SECTION 58-83  
OVERLAY DISTRICT (OD); PROVIDING FOR  
CONFLICTS; SEVERABILITY AND AN EFFECTIVE  
DATE.

WHEREAS, the Creation of the Orange Avenue Overlay process was an extensive multi-year community planning effort that involved continuous opportunities for public input and unique collaborative opportunities, such as: a community walkshop, surveys, citizen boards and focus groups, educational videos, open houses, charettes, written-only visioning, work sessions and much more; and

WHEREAS, the Winter Park Planning and Zoning Board, acting as the designated Local Planning Agency, has reviewed and recommended adoption of proposed amendments to the Zoning Regulations portion of the Land Development Code having held an advertised public hearing on \_\_\_\_\_, and has recommended approval of this Ordinance to the City Commission; and

WHEREAS, the City Commission of the City of Winter Park held a duly noticed public hearing on this Ordinance set forth hereunder and considered findings and advice of staff, citizens, the Orange Avenue Overlay Steering Committee, the Planning & Zoning Board and all interested parties submitting written and oral comments and supporting data and analysis, and after complete deliberation, hereby finds the requested change consistent with the City of Winter Park Comprehensive Plan and that sufficient, competent, and substantial evidence supports the Land Development Code changes set forth hereunder; and

WHEREAS, Comprehensive Plan Policy 1-2.4.14, Mixed Use Designation directed. Within one year from the adoption of this Comprehensive Plan, the City will create a mixed use overlay or district for commercially designated parcels that would be intended to facilitate design and use flexibility to achieve pedestrian scale, innovative transit connectivity and maximizing open space within a commercially viable and architecturally desirable design. Complementary uses may include, but are not limited to retail, entertainment, office, civic and residential uses. The City shall also prepare companion land development code regulations that implement the proposed mixed use overlay or district

simultaneously with any policy amendments related to this overlay or district. All policies related to this overlay or district will be subject to a Comprehensive Plan amendment.

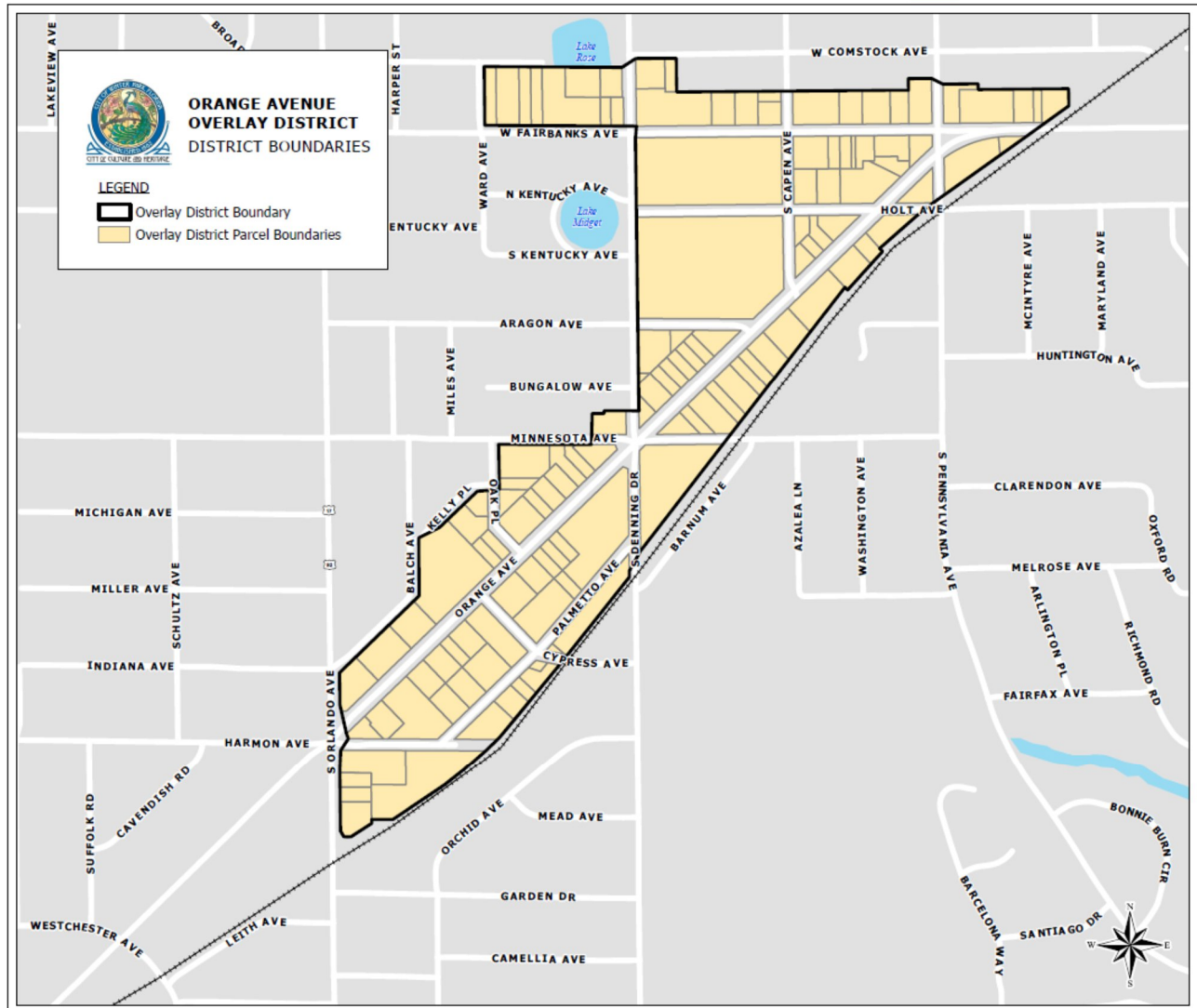
WHEREAS, the City Commission hereby finds that this Ordinance serves a legitimate government purpose, meets the requirements of law and is in the best interests of the public health, safety, and welfare of the citizens of Winter Park, Florida.

NOW, THEREFORE, BE IT ENACTED BY THE CITY OF WINTER PARK, FLORIDA:

SECTION 1. Amendment. That Chapter 58 "Land Development Code", Article III "Zoning" of the Code of Ordinances is hereby amended and modified by adopting Section 58-83 Overlay Districts (OD), as follows:

- (1) Establishment of Overlay District. The Winter Park City Commission adopts this overlay district as needed in order to implement specific purposes, intents, and design standards based upon the adopted Comprehensive Plan amendment for the Orange Avenue Overlay District, which shall be applied as additional standards to other regulations required by the City. Such overlay district shall be made a part of this section of the Land Development Code. Upon adoption, the boundaries of such overlay district shall be shown on the Winter Park Zoning Map.
- (2) Applicability.
  - (a) All projects located within this overlay district shall adhere to the requirements of the Land Development Code, except to the extent provisions of the adopted overlay district replace, supersede, or provide different requirements. (Note: Consider legal review given attorney challenge of Henderson language)
  - (b) Where any Section or provisions of the Orange Avenue Overlay District adopted into the Land Development Code are found to be in conflict with or which overlay district Sections or provisions replace, supersede, or override other Sections or provisions of the Land Development Code, the relevant Sections and provisions in the adopted overlay district shall apply and control. Any overlay district acts as an additional layer of zoning over the base zoning district. (Note: Consider legal review given attorney challenge of Henderson language)
- (3) Overlay District. The provisions and regulations for the Orange Avenue Overlay District within the City of Winter Park are outlined below.
  - (1) Orange Avenue Overlay District. It is the intent of the Orange Avenue Overlay District ("OAO") to provide enhanced standards to protect and promote the unique characteristics of the Orange Avenue area and create a distinct gateway into Winter Park. This OAO is used to create a sense of place established through specific architectural styles, streetscape design, open space areas, setbacks, site design, landscaping and other regulatory controls. The following provisions and regulations apply to all properties located within the OAO unless expressly provided otherwise.

- a. Location and boundaries. The OAO boundaries are identified on the following map. The provisions and regulations found herein shall only apply to the properties located within this defined area. Each parcel of property within the OAO is identified by tax parcel identification number on the attached Exhibit "A", and each Parcel, in addition to retaining its underlying zoning classification (as modified by these OAO regulations and provisions), shall reflect on "OAO" designation on the City's zoning map.



- b. Purpose.

- (1) Encourage sustainable development, redevelopment, and adaptive reuse that will become a long-term asset to Winter Park;
- (2) Create and enhance connectivity to the surrounding neighborhoods and promote connectivity to all of Winter Park;
- (3) Utilize and incentivize private development and/or redevelopment to create solutions for the existing problems that

small properties and business-owners in the Orange Avenue area face;

- (4) Create public improvements that will benefit all residents and visitors of Winter Park;
- (5) Provide the opportunity for existing businesses and properties to improve their structures, their businesses and their building facades;
- (6) Restrict uses and create regulations that promote the development of the Orange Avenue area as a special place within Winter Park that promotes an environment of arts, healthy-living, cuisine, culture, heritage, social interaction, healthcare, local business, education, connectivity and community;
- (7) Create better connectivity to and the promotion of Mead Botanical Garden;
- (8) Meet the goals of the Community Redevelopment Agency (CRA) in the areas of the OAO that fall within the CRA boundary;
- (9) Protect and encourage development of an area that represents a significant opportunity for public and private investment, which is important to the long-term economic health of Winter Park;
- (10) Establish regulations that protect the investment of existing and new businesses from unattractive, unsustainable and non-compatible uses;
- (11) Ensure the area is visually pleasing and creates place that encourages community and is developed in a coordinated fashion;
- (12) Follow the principles of the Vision Winter Park, Comprehensive Plan and Sustainability Plan documents;
- (13) Encourage mixed-use development, shared parking and shared stormwater management.
- (14) Give special attention to landscaping, architectural detail, meaningful open space, buffering, signage, lighting, and building setbacks;
- (15) Encourage architectural creativity, quality and variation to create a unique district with its own identity;
- (16) Promote the history of Winter Park and the Orange Avenue area, including the area known as Designers Row;
- (17) Keep the traditional scale within the OAO;
- (18) Create an Arts & Cultural Corridor;
- (19) Protect and promote Historic architecture, where it exists in the area;
- (20) Attract new businesses, retain small businesses and encourage locally-owned businesses in the Orange Avenue area; and



(21) Enhance pedestrian walkability, connectivity and safety and to create a safe, comfortable and convenient pedestrian experience with shaded sidewalks, interesting business fronts, connected destinations and walkable block sizes.

c. Permitted Uses. Any use not listed specifically as an allowed use herein shall be deemed to be prohibited in the OAO. The following uses up to 10,000 square feet shall be allowed by-right on any property within the OAO, unless otherwise specified within the subarea policies:

- (1) Antique Stores
- (2) Bars, taverns, cocktail lounges (with food sales)
- (3) Blueprinting, photocopying and printing offices
- (4) Boutique Hotel (120 rooms or less). (100 rooms or less)
- (5) Breweries/distilleries (with food sales)
- (6) Market or corner store (up to 5,000 square feet and excluding convenience store)
- (7) Dry Cleaning (Drop-Off Only, with off-site processing & no drive-thru)
- (8) Financial institutions, including banks, savings and loan associations and credit unions (with a maximum of 2 drive-thru lanes, which are screened from view)
- (9) Fine arts museums, fine arts instruction, dance instruction and music instruction
- (10) Fitness facility, exercise or health club (up to 5,000 square feet)
- (11) Food Halls
- (12) Government services
- (13) Grocer (not including convenience store), up to 10,000 square feet.
- (14) Health and wellness studios (up to 5,000 square feet)
- (15) Mixed-Use Development
- (16) Nonprofit organization offices
- (17) Personal services (spa, barber shop, hair salon, nail salon, massage, cosmetic treatment)
- (18) Pet supply shop, pet grooming, pet daycare (provided that there shall be no outside kennels, pens or runs. No overnight or weekend boarding of animals)
- (19) Photography Studio
- (20) Professional offices (including medical and dental offices)
- (21) Recreational facilities up to 5,000 square feet



- (22) Residential uses such as condominiums, townhomes, apartments, lofts, studios. If more than 4 units is proposed as a part of any new development, remodel or addition, each of the units shall be a minimum of 750 square feet of living area. Excluding single-family detached homes;
- (23) Restaurants/Fast-Casual Dining/Fine-Dining
- (24) Retail businesses involving the sale of merchandise on the premises within enclosed buildings and excluding resale establishments or pawn shops (other than clothing resale stores), a maximum size of 10,000 square feet.
- (25) Theater
- (26) Uses customarily incidental and accessory to the permitted uses, including the repair of goods of the types sold in stores are permitted. Such repair must be carried on within a completely enclosed building, may not be carried on as a separate business, and provided further that there shall be no manufacturing, assembling, compounding, processing or treatment of products other than that which is clearly incidental and essential to the permitted uses. No external signage shall be permitted for accessory uses. Needs discussion – example of business signs along Morse that are multi-tenant buildings

d. Conditional Uses. The following uses shall be allowed only with approval of a Conditional Use on any property within the OAO, unless otherwise specified within subarea policies:

- (1) Bars, taverns, cocktail lounges (without food sales)
- (2) Breweries/distilleries (without food sales)
- (3) Fitness facility, exercise or health club over 5,000 square feet, but less than 10,000 square feet
- (4) Recreational facilities over 5,000 square feet, but less than 10,000 square feet
- (5) Buildings or retail uses over 10,000 square feet in size.

e. Prohibited Uses. Without limiting the uses that are prohibited in the OAO, the following uses shall be prohibited in the OAO:

- (1) Adult-oriented businesses
- (2) Automotive-related businesses (i.e. auto sales, auto repair, auto rental, body shops, auto wash, auto audio, auto glass, auto tinting, auto parts sales) what about Holler doing a McLaren type business?
- (3) Standalone massage parlors (not part of a health spa)
- (4) Fast food (with or without drive-thru)
- (5) Gas stations/convenience stores

- (6) Liquor stores
- (7) Pawn shops/check cashing
- (8) Tattoo parlors
- (9) Vape/smoke shops
- (10) Pain Management Clinics
- (11) Medical Marijuana Dispensaries, Processing, Growing, etc.
- (12) Billboards

f. Non-Conforming Uses. Existing but non-conforming properties uses and properties, which existed as of the date the OAO became effective, shall be allowed to continue in the same manner after the adoption of the OAO. However, such nonconforming uses and properties shall be subject to the provisions of this OAO when any of the conditions below occur. The Planning & Transportation Director or City Traffic Engineer or their designee may require the property owner to provide a site-specific traffic study to determine the potential impacts of the proposed changes as part of the site development plan.

- (1) The principle use on the property is discontinued for a period of three (3) calendar months;
- (2) The structure is destroyed or significantly altered by sixty (60) percent or greater; or
- (3) Enlargements of any existing non-conforming structures and/or uses are made to the property that increases the gross square footage.

g. Residential Uses within the Overlay District. The OAO shall not apply to existing residential uses or structures developed prior to the effective date of this section. Protections of existing residential uses shall be implemented. Properties currently used as and developed as residential prior to the effective date of this section shall continue to be subject only to current Land Development Code standards and protections governing such properties' underlying zoning designation and not those set forth in the OAO, unless and until such properties redevelop. Any change of use or redevelopment on the property shall require compliance with OAO standards. In order to protect existing structures that are used as a residence, new non-residential or mixed use development within the OAO shall have its structures setback at least 35 feet from the property boundaries of parcels with existing residential structures used as a residence that are not intended to be part of the proposed development, unless a written consent can be obtained from owner(s) of the affected existing residential structure(s) consenting to the waiver of such minimum setback requirement of this subsection. Parking garages shall be setback at least 100 feet from the property boundary of parcels used and developed as single-family or low-density residential.

h. Landscaping Requirements. Development and or redevelopment in the OAO shall comply with all landscaping requirements of other provisions of the Land Development Code and future corridor landscape plans, unless specifically addressed within the OAO. In addition to other requirements of Sec 58-334 of the Land Development Code, all street frontages within the OAO shall contain at least one shade tree for every 50 feet of linear lot line. Side yards and rear yards not facing streets shall contain at least one understory tree for every 30 linear feet of side or rear lot lines.

i. Circulation Requirements.

(1) The OAO is designed to enhance pedestrian walkability, connectivity and safety and to create a safe, comfortable and convenient pedestrian experience. Towards this end, the OAOs transportation system is based on inter-connected streets forming small blocks similar to successful patterns of the more walkable areas of the City such as the Park Avenue Corridor.

(2) Block Configuration/Lot Standards: Any additional development or redevelopment of parcels in excess of 2 acres within the OAO shall be required to create a block structure and pedestrian corridors conducive to pedestrian safety, comfort, and vehicular circulation.

(a) New or redeveloped buildings within the OAO shall have building widths of between a minimum of 50 feet and a maximum of 300 feet wide where they interface with pedestrian oriented street frontages.

(b) Development shall provide a continuous driveway or alley that connects to adjacent parcels and roadways and provides access for emergency vehicles and city services.

(c) To the extent possible, given the size and shape of the parcel, new or redeveloped blocks shall be between a minimum perimeter of 1000 linear feet and a maximum perimeter of 1800 linear feet. The City goal is to encourage walkability based upon successful Park Avenue Corridor block perimeters of 1300 linear feet. Where existing block perimeters are smaller than 1800 linear feet, no further breakdown is required. Where block perimeters are larger than 1800 linear feet, new development shall be organized to break the land mass into walkable blocks.

(d) Driveways or alleys shall be constructed at widths adequate for dumpsters, trash compactors and circulation of solid waste collection vehicles, large truck delivery and allow emergency vehicles to maneuver past parked delivery vehicles and waste receptacles.

(3) Street Realignment and Parallel Orange Avenue Access. Where Palmetto Drive, Vivian Drive and Harmon Drive provide secondary and primary access to properties within the OAO area, portions of these roadways may be considered to be closed, vacated or re-aligned

subject to City Commission approval, so long as a parallel public access road and approved by the City Commission, is maintained to allow vehicular access between Orlando Avenue and Denning Drive. Protection of on-street parking, maximizing ease of traffic flow for Palmetto Avenue and maintaining the existing 50-foot public right of way shall be matters of priority concern should the roadway be realigned. Any road shall be constructed with a minimum street travel lane width of 24 feet unobstructed and shall have unobstructed vertical clearance per National Fire Protection Act (NFPA) codes, shall replace all existing on-street parking and maintain the existing 50-foot public right of way.

(4) Street Sections: Any new, realigned, or redesigned street sections within the OAO shall be designed to create and maintain a quality comfortable walking environment encouraging the use of multimodal transit options including:

(a) On-street parking is to be provided to the maximum extent possible to buffer pedestrians from vehicular traffic. Parking space dimensions shall be not less than 9ft x 18ft.

(b) Vehicular traffic lanes on streets with bus routes shall be designed at not less than 12 feet to support safe circulation of busses.

(c) Bus transfers are to be located inside project boundaries to lesson traffic delays and increase safety on major roads. Bus transfers shall be coordinated and approved by Lynx.

(d) Shaded sidewalks and benches for resting and waiting for public transit shall be provided.

(e) Care shall be taken to minimize curb cuts and maximize visibility surrounding curb cuts.

(f). New development in conjunction with the City shall pursue approval from FDOT to utilize the FDOT rail right of way for pedestrian/bicycle trails where appropriate prior to submission of project plans for City approval.

(5) Sidewalks: You may want to move your sidewalk section to be part of this. Sidewalk Design and Utilization (having trouble with formatting) All buildings, parking areas, public spaces, amenity features, and adjoining developments of similar use shall be linked with sidewalks. Sidewalks shall be provided along public streets that provide access to the development. A minimum 17-foot sidewalk is to be provided along Orange Avenue. Fairbanks Avenue and Orlando Avenue shall provide sidewalks at least ten feet wide with a minimum 5-foot landscape buffer along the back of curb, between the roadway and sidewalk. All other sidewalks in the OAO shall be at (should sidewalk be narrower? Planting

strip important.) 10-feet in width with a minimum 5-foot landscape buffer. If sufficient right-of way is not available, the building may need to be set back to accommodate these wider sidewalks and the additional space required to create the required sidewalks as defined herein shall be dedicated to public access through easement. Sidewalks shall be constructed in accordance with the standards for sidewalks set forth in City of Winter Park Engineering Standards and Landscape standards and constructed at the sole cost of the developer.

(a) Restaurants and cafes with seating within the public right-of-way shall be subject to the regulations contained in Chapter 90, Article VI - Sidewalk Cafes, of the Winter Park code of Ordinances.

(6) New Streetscapes: In order to improve the pedestrian experience, new development or redevelopment shall provide the streetscape area including sidewalks, lighting, landscape and street furniture and will dedicate pedestrian easements over this area prior to certificate of occupancy.

(7) Access Standard:

(a) All city services including utility access, utility equipment, solid waste containers shall be placed at the rear of lots and accessed by driveways or alleys opening to side streets.

(b) Garage and parking shall be accessed through driveways or alleys opening to side streets.

j. Architectural Standards.

(1) Building Height. To allow for flexibility in design, but preserving development standards that will reduce building massing, buildings shall be measured in stories. For parking garages, floors visible on the exterior of the building shall be counted towards building height (ex. A 4-story building wraps around a 5-story interior parking garage, only the 4 stories would count as they are the only part visible).

The first floor of any building shall be allowed to be a maximum of 18 feet in height. When mezzanines, balconies or lofts are provided, first floor heights of 20 feet may be allowed. Mezzanine, balcony or loft levels shall be allowed within the first story, as long as they do not cover more than 30% of the first-floor area and stay within the maximum 20-foot first floor height area. Mezzanines, balconies or lofts shall not be allowed above the first floor. Each floor above the first floor shall have a maximum height of 13 or 12 feet. Is 20 feet tall enough for a mezzanine?

For multi-story buildings over two stories in height, there shall be terracing and/or additional setbacks to accomplish vertical articulation is mandated to create relief to the overall massing of the building facades, as discussed later in this chapter.

- (2) Setbacks/Stepbacks. For any building over two stories in height that is built up to the allowable building line (established by the greater of setback or sidewalk requirement), each additional floor shall be setback a minimum of ten (10) feet, or shall be within the allowable envelope as depicted in the figures below. All setbacks are measured from the property line:

Figure 1 – Allowable Building Envelope Cross-Section:

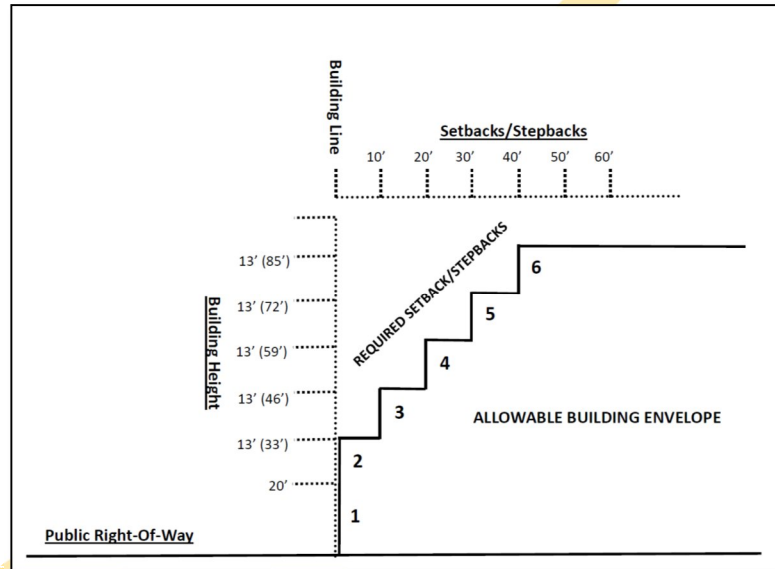


Figure 2 – FDOT Roadway Setback/Stepbacks (17-92 & Fairbanks):

Change floor to ceiling height to match code. Change from state roads to all roads for 10 ft setback.

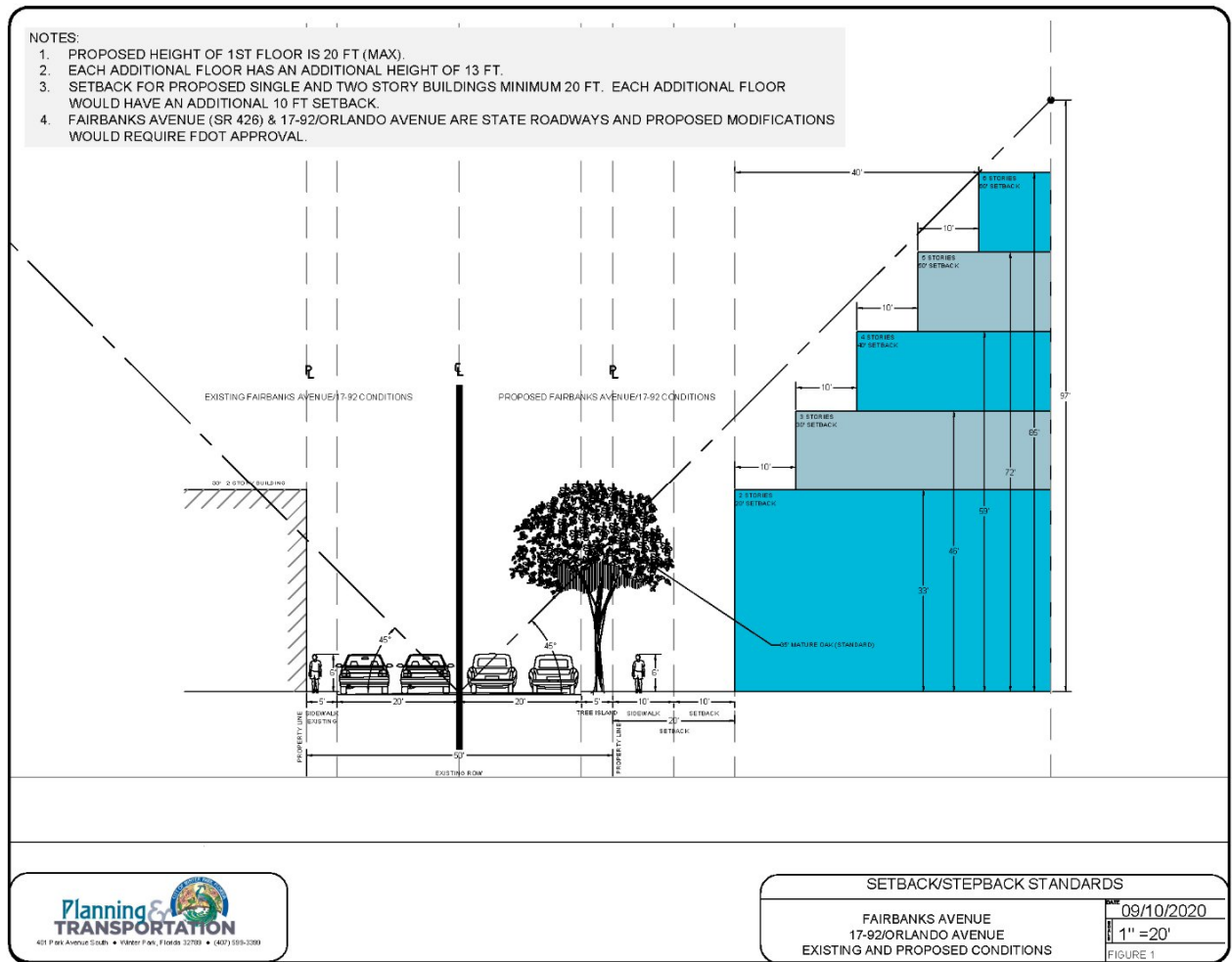
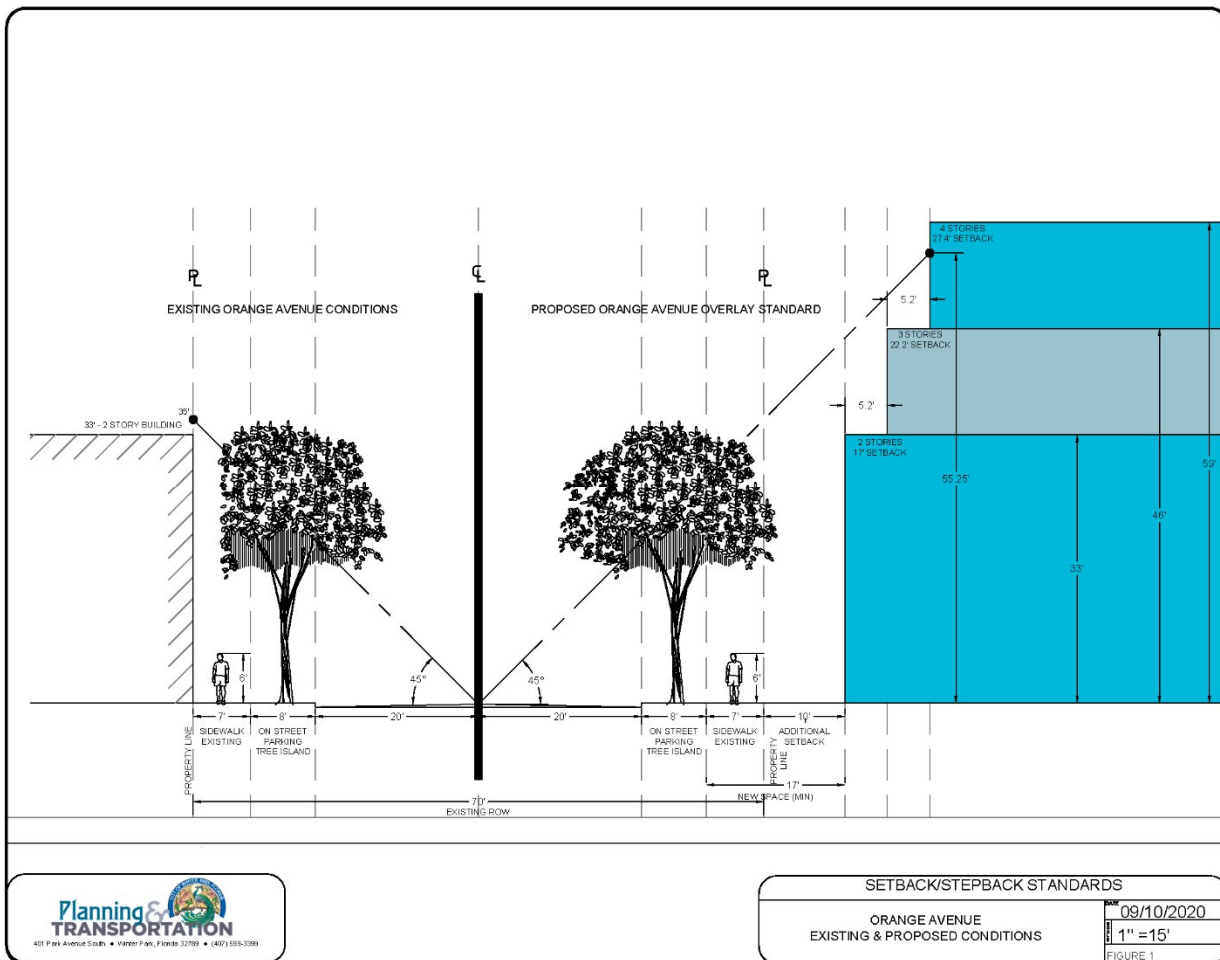


Figure 3 – Typical Cross-Section for Orange Avenue Setback/Stepbacks





Update Figure 3 (says 1 on drawing) to show 10ft not 5ft. Delete the third story on Orange Avenue.

If a building is constructed within the allowable building envelope as depicted above, the first floor shall always be defined and articulated as addressed later in the OAO. Additionally, the building shall not exceed more than two stories of vertical wall without a setback/stepback, cornice, balcony or other major façade breakup, which shall create visual and massing relief. All walls shall provide two or three of the articulation options listed below, at a minimum of every 4 feet.

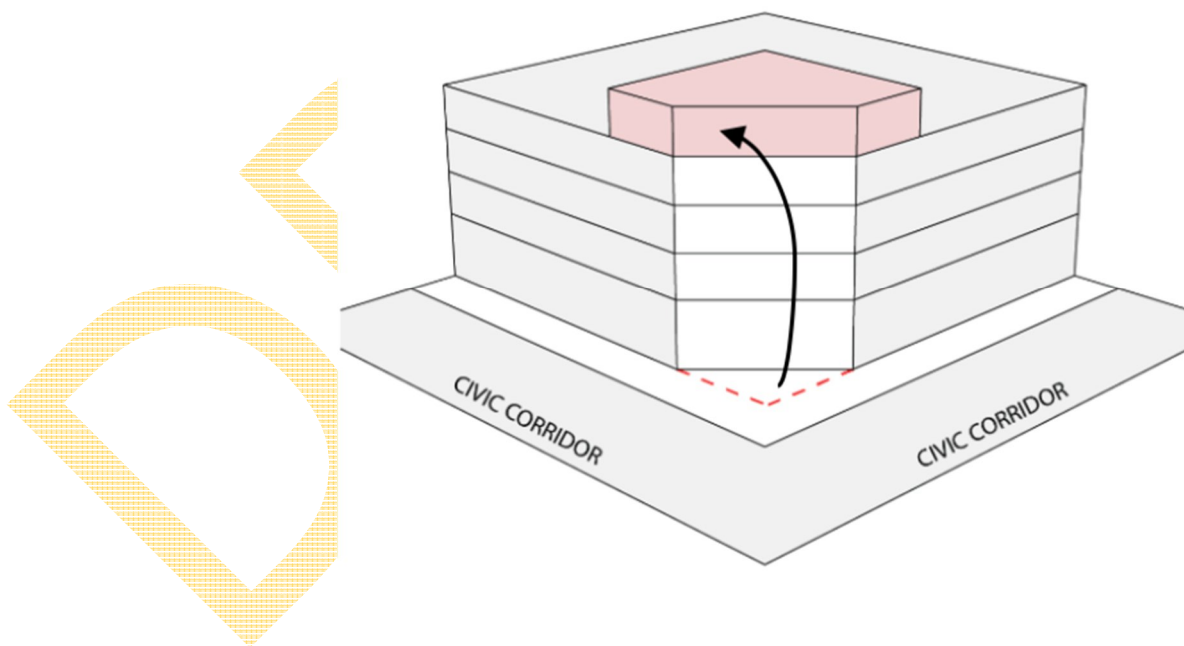
Each subarea of the OAO may define additional required setbacks for that area that may differ from what is defined in this section. The defined building envelope depicted above shall be maintained, regardless of any setbacks.

All setbacks and/or stepbacks shall provide space for the healthy development of shade tree crowns. The City Arborist shall review and recommend species selection and positioning to ensure compliance.

Building setbacks/stepbacks as defined herein shall not be required for properties abutting the railroad right-of-way. However, these rear facades shall provide articulation offset 4 feet, material change, window, entryway or other breakup of the building façade at least every 30-50 feet, both vertically and horizontally.

(2) Corner Treatment.

- (a) Properties fronting on a street corner have two public facades, and require special architectural treatment and detailing that will have the highest impact and visibility, at the corner location. To promote pedestrian safety, buildings placed on corner lots shall be setback to allow for line of sight for all modes of transportation, based on speeds and site conditions.
- (b) On corner parcels, allow up to 10 ft more corner building height and square footage equivalent to the additional setback at the intersections of OAO road, in exchange for setting building back from these intersections. This shall be in lieu of corner architectural height increase allowance not in addition to.



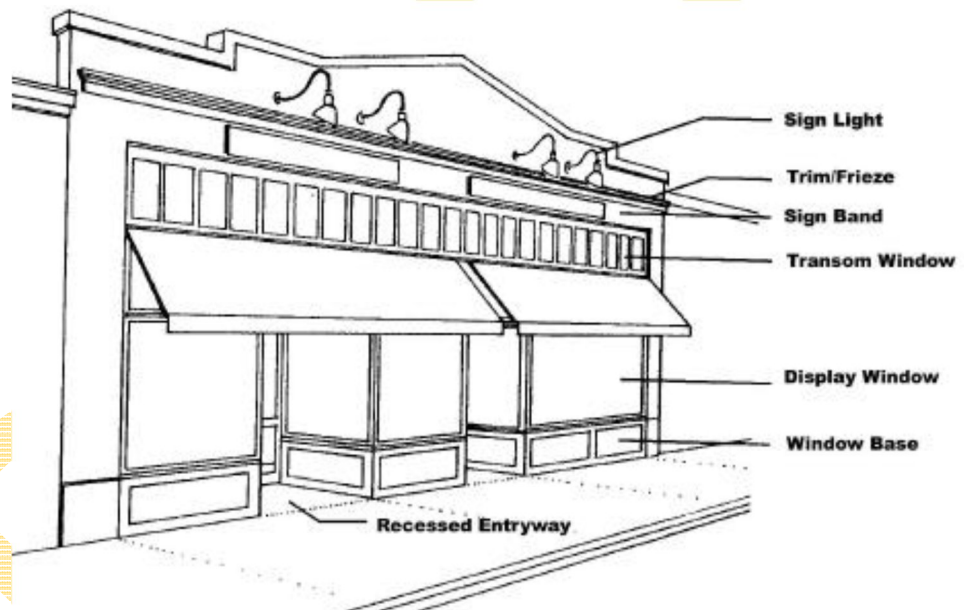
- (3) Facades. The intent of this section is to provide visual interest to all facades by requiring a minimum level of detail features on facades. These detail features shall not consist solely of applied graphics or paint. There shall be some sort of articulation, material or color change, window, entryway or other breakup of the building façade at least every 50 feet. Murals shall be allowed to contribute towards façade breakup.

- (a) All facades of buildings with a gross floor area of ten thousand square feet or more shall be required to incorporate at least three (3) of the following facade treatments. At least one of these treatments shall repeat horizontally. All such design elements shall repeat at intervals of no more than thirty (30) feet, either horizontally or vertically.
- (b) All facades of buildings with a gross floor area of less than thousand (10,000) square feet shall be required to incorporate at least two (2) of the following facade treatments. At least one (1) of these treatments shall repeat horizontally. All such design elements shall repeat at intervals of no more than twenty-five (25) feet, either horizontally or vertically.
1. Expression of a vertical architectural treatment with a minimum width of twelve (12) inches.
  2. Building stepbacks, offsets or projections, a minimum of four (4) feet in depth.
  3. Texture and /or material change.
  4. Architectural banding.
  5. Pattern change.
  6. Other treatment that, in the opinion of the Planning Director after review by the Appearance Review Board, meets the intent of this section.
- (c) No building shall exceed more than 300 feet of horizontal length on any street facing façade. When multiple buildings are included on the same parcel, those buildings shall be separated by 20 feet to support growth of healthy canopy crowns or provide courtyard, parking or other gathering spaces on the site. ~~without separation. If a building is set back more than 50 feet from the roadway, this requirement shall not be required. For buildings setback less than 50 feet, no more than 100 horizontal feet of façade of the same architectural style shall be allowed, to achieve variation of building design.~~

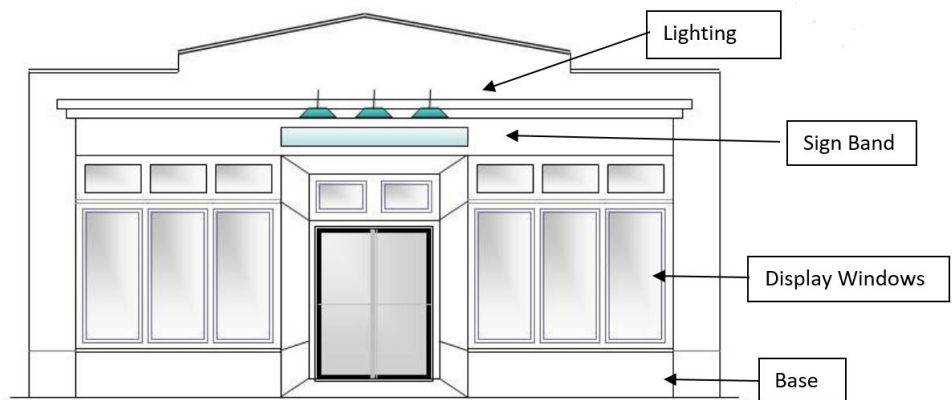
Commercial Façade Treatment Example 1:



Commercial Façade Treatment Example 2:



Commercial Façade Treatment Example 3:



- (4) Additional Façade Treatment Requirements. Roof lines, parapets and building heights shall be architecturally articulated and diverse in design form with adequately scaled and proportioned architectural treatments which complement the building's design.

The window, wall and roofing treatment should be of high-quality materials and consistent on both the front and the back of the building and any street facing side wall of the building. Allowable building materials shall include brick, natural stone, glass, architectural metal, concrete, wood, or similar material with a longer life expectancy. Exterior Insulation and Finish System (EIFS) shall not be allowed. Stucco, hardiboard siding (or similar materials) or concrete block shall not exceed more than 50% of any façade. Detailing is encouraged to enhance the façade.

Structures shall be sited so as to create visual relationships with sidewalks, street alignments, trees, green space and neighboring businesses; create visual anchors at entries, provide interesting architectural perspectives featuring appropriate facade treatments and maximize the pedestrian relationship to the sidewalk. They shall also take into consideration the existing structures and be in unison with their scale and style.

- (5) Glazing Requirements. The lower story of the building has the most immediate visual impact on the passerby. Traditionally, buildings along urban streets have a high proportion of glazing to solid wall surface, with higher floor to floor heights, on the first story. To maintain continuity with this treatment, non-residential buildings on Orange Avenue shall have 60% minimum glass on the first story and shall be allowed height up to 20 feet. Along other roads within the OAO, non-residential buildings shall have 40% minimum glass on the first story. The first floor shall be clearly defined and articulated from upper stories.

- (6) Building Entrance. Primary building entrances in the OAO shall be clearly defined, and shall be recessed or framed by a sheltering element such as an awning, arcade, overhang, or portico (in order to provide weather protection for pedestrians). Awnings are encouraged. Awning standards include:

- (a) Awnings for a building façade shall be of compatible color, look, shape, and height;
- (b) Awnings shall provide vertical clearance of no less than eight (8) feet above sidewalks; and
- (c) Awnings are not allowed to hang over vehicular traffic ways.

- (7) Architectural Towers, Spires, Green Roof Features, Solar Panels, Chimneys, Or Other Architectural Appendages. Any architectural tower, spire, chimney, flag pole or other architectural appendage

to a building shall conform to the underlying subarea height limit. However, when necessary to meet the building code requirements, chimneys may exceed the height by the minimum required.

Architectural appendages, roof decks, solar panels, roof garden or green roof features, embellishments and other architectural features may be permitted to exceed the roof heights specified in that subarea, on a limited basis encompassing no more than 30 percent of the building roof area or 30% of building street facing façade width not to exceed 30 linear feet of a given facade, or up to ten feet of additional height upon approval of the City Commission, based on a finding that said features are compatible with adjacent projects.

- (9) Mechanical Penthouses, Rooftop Mechanical and Air-Conditioning Equipment, Stair Tower Enclosures, Elevators and Parapets. Mechanical penthouses, mechanical and air-conditioning equipment, stair tower enclosures, or elevators on rooftops of buildings shall not exceed a total height of ten feet (building code official shall have the ability to give administrative variances to this requirement based on life-safety or equipment needs for elevation) above the allowable building height. Any penthouses shall only be used for mechanical equipment to serve the building. Parapets, or mansard roofs serving as parapets, may extend a maximum of five feet above the height limit in the underlying zoning district unless other parapet heights are more restrictive for the respective underlying zoning district. In addition, mechanical equipment and air-conditioning equipment on rooftops shall be screened from view from ground level on all buildings in all zoning districts and shall be located to the maximum extent possible so that they are not visible from any street.

- (10) Appearance Review. In addition to meeting the architectural standards as set forth in this section. All external renovation or development projects within the OAO shall undergo Appearance Review prior to permitting. For developments requiring a conditional use approval having a land area of more than 80,000 square feet, having more than 25 residential units, or having structures exceeding 35,000 gross square feet above grade, professionally prepared fully rendered 3-D digital architectural perspective images and elevations that show all sides of the proposed building(s), parking areas, parking structures and any other site improvement shall be submitted to and reviewed by and commented upon by a City established review committee, consisting of three (3) Licensed Architects, and 2 from the following list of professions: Professional Land Planner, Licensed Landscape Architect and Licensed Professional Engineer, to ensure high quality architecture consistent with the goals and



objectives of the OAO. At the request of the Director of Planning an Acoustical Engineer may be invited to comment on projects having rooftop or open-air elements. The comments and recommendations of the Appearance Review Committee shall be transmitted to the Building Official, Director of Planning, Planning and Zoning Board and City Commission for consideration in rendering a decision on the proposed conditional use or building permit sought for the proposed development.

k. Parking Requirements & Access Management.

- (1) To the extent net street parking, over and above that currently existing within the OAO, is provided by the developer prior to certificate of occupancy, such net new parking directly adjacent to the development lot may count towards satisfying code parking requirements.
- (2) Parallel Orange Avenue Access. Where Palmetto Drive, Vivian Drive and Harmon Drive provide secondary and primary access to properties within the OAO area, portions of these roadways may be considered to be closed, vacated or re-aligned, so long as a parallel access road, as approved by the City Commission, is maintained to allow vehicular access between Orlando Avenue and Denning Drive. Protection of on-street parking, maximizing ease of traffic flow for Palmetto Avenue and maintaining the existing 50-foot public right of way shall be matters of priority concern should the roadway be re-aligned. Any road shall be constructed with a minimum street travel lane width of 24 feet unobstructed and shall have unobstructed vertical clearance per National Fire Protection Act (NFPA) codes, shall replace all existing on-street parking and maintain the existing 50-foot public right of way.
- (3) Off-Street Parking Requirements. Unless specifically provided within the OAO, parking spaces, parking management plans and parking leases shall be provided in accordance with Land Development Code requirements.
  - (a) General Business and Retail Commercial: One parking space for each 333 square feet of gross floor space.
  - (b) Office, Professional or Public Buildings: One parking space for each 333 square feet of gross floor space.
  - (c) Medical Office: One parking space per 200 square feet of gross floor space.
  - (d) Hotel: One parking space for each guest room shall be provided. Other ancillary uses in the hotel (restaurant, spa, retail, meeting space, etc.) shall only be required to provide parking at 50% of in accordance with the off-street parking requirements defined in Section 58-86.



(e) Restaurants, Food Service Establishments, Nightclubs, Taverns or Lounges: One space for every four seats.

(f) Multi-family residential: 2.5 spaces per dwelling unit in accordance with Sec 58-86

~~(1) Each one-bedroom or studio unit shall be required to provide 1.25 dedicated parking spaces per unit.~~

~~(2) Each two-bedroom unit shall be required to provide 1.5 dedicated spaces per unit.~~

~~(3) Each three-bedroom or above unit shall be required to provide 2 dedicated parking spaces per unit.~~

Do not need 2.5 spaces per unit – needs to be based on the size of the units (example – studio/1 bedroom does not need 2.5 spaces)

(4) Off-street Parking Access Design. Parking access to properties along Orange Avenue, Orlando Avenue and Fairbanks Avenue shall be through an alley originating from side streets or from a side street if frontage is available on a side street. In mid-block locations without existing alleys, new alleys shall be provided and accessed from the frontage street. Garages shall not be located in front of the primary building. If side street frontage is not available, access from cross-access easements preferably in the form of shared rear alleys shall be utilized. If cross-access is not available, a driveway will be allowed from Orange Avenue, Orlando Avenue, or Fairbanks Avenue, provided the driveway and building gap surrounding it is not more than 20 feet. Any new driveways or curb cuts along Fairbanks Avenue or Orlando Ave shall require FDOT coordination prior to submittal. Throughout the OAO, cross-access agreements, preferably in the form of shared rear alleys are required, to reduce the number of curb cuts and driveways. The intent is to maintain the building street wall without large voids for access driveways and improve traffic circulation by providing rear access for services and deliveries. The goal in this parking arrangement is to decrease the visibility of parking from the street as much as possible, by having parking behind the building and to reduce the turning movements with limited visibility across multiple lanes of traffic.

(5) Off-site Parking. Required parking may be located within 750 feet of the building, or within the closest parking structure where excess parking is available for lease. Off -site leased parking must comply with additional requirements of Sec 58-86: (j) Remote Parking Lots Encumbered. In the event of new construction, addition, or change in intensity of use of the principal building or property being serviced by the remote parking lot, all existing parking spaces located on such remote lot shall be allocated to the existing building or principal use to meet the minimum requirements of this article, and any additional spaces may then be allocated to that portion of the building or

property which is the subject of the new construction, addition, or change in intensity of use.

- (6) **Bicycle Parking Requirements.** Unless specifically provided within the OAO, bicycle Parking shall be required in accordance with other Land Development Code Standards. Where large projects provide parking garages, 20% of the required bike parking shall be provided onsite. The other 80% of required bike parking may be located within City right-of-way, along bike paths, or within greenspace areas throughout the OAO. The City shall determine where the off-site bicycle parking shall be located.
- (7) **Parking Exclusion.** A parking exclusion shall apply only to existing square footage or floor space. Parking shall be provided as required by the OAO parking standards or other provisions of the Land Development Code, where not specifically addressed herein. The OAO shall apply for any net new building or net new floor space created by redevelopment, new construction, additions, alterations, or remodeling or for any change in use requiring additional parking such as an office or retail space conversion to restaurant. Existing parking spaces may be counted to satisfy this requirement only where such existing spaces are in excess of the parking space requirements of this section for any existing floor space.
  - (a) **Floor Area Ratio for Parking Structures.** In subareas where the city encourages the use of structured parking, an opportunity to achieve increased floor area ratio (FAR) is included in the Subarea development standards. Parking structures shall count towards the floor area ratio (FAR) for any property within the OAO (except for underground level and open top level), developments that provide parking structures meeting of the following conditions are eligible to take advantage of the Bonuses offered in the OAO Development Enhancement Menu.
  - (b) **Provide for multi-property parking collectives.** For smaller-scale garages, multiple property owners may create a collective where parking can be built on a property that can serve multiple properties that do not have adequate available parking.
  - (c) **Provide level-two electric vehicle charging stations** for a minimum of 2% of all spaces within the structure.
  - (d) **Provide and construct parking spaces** at least 10% above what is required to meet code minimum requirements. This excess parking must be available for lease at or below fair market value (based on area parking lease comparables) to small businesses in the OAO. When parking spaces to satisfy the 10% requirement are located on the primary parcel as part of structured parking, the square footage associated

with those spaces shall not be included in FAR calculations. At the discretion of the City Commission, a portion of this additional parking may be provided in another location of verified parking deficiency within the OAO. Any parking spaces provided under this provision shall be constructed in conformance with the standards of the Subarea in which they are constructed, subject to an approved Parking Management Plan and supported by a recorded instrument acceptable to the City Attorney. Provision of parking spaces shall run concurrent with the primary development use. The City's preference is structured parking. Should the City Commission agree to accept surface parking spaces as part of the required 10% additional parking, the number of spaces will be adjusted to reflect the comparable value of surface versus structured parking based on current market values as determined by the Office of Management and Budget.

- (e) Stairwells required for parking garages shall be designed to have open walls, visible to the outside of the garage.
- (f) Parking structures shall be designed to allow sufficient airflow to ensure that all structured parking remain "open-air." Mechanical ventilation shall not be allowed for structured parking within the OAO, unless the parking structure is located below ground.
- (g) Parking structures shall be screened at least 50% on all visible sides with green walls, living walls, liner buildings (with adequate spacing to allow air and light to enter garage), murals (that do not include advertising of any type), mature shade trees or vegetative screening, or designed with architectural details to match the primary structure. that soften the appearance from looking like a stark parking garage wall.
- (h) Subarea A properties shall be offered priority for parking garage spaces that are required to be available for lease. Parking spaces within parking garages shall also be eligible for purchase by other properties, first opportunity to purchase shall be given to Subareas A.

Parking structures shall meet height, building setback, allowable building envelope area and screening requirements as outlined in the OAO.

Parking garages shall be set back from the Orange Avenue, Fairbanks Avenue or Orlando Avenue frontages in accordance with requirements of Section 58-83(3).i. (2) Setbacks Allowable Building Envelope Cross-Section based on height. Parking structures shall be screened as required in the OAO. Parking shall be accessed from private drives or public/private alleys

originating and terminating on side streets to the maximum extent feasible. They shall be designed in an architectural style that is compatible with its building counterpart and shall also conform to the City's parking garage design guidelines, be subject to Parking Management Plans and standards in Section 58-84 and Section 58-86.

Parking structures shall not be permitted on the north side of Fairbanks Avenue.

I. Public Notice and Hearing Requirements.

- (1) In addition to notice required by state law, City-wide notice, as defined in Section 58-89, shall be required for OAO code text amendments and any proposed development within the OAO that requires Conditional Use approval and has a land area of more than 80,000 square feet, or a building of more than 35,000 square feet or having more than 25 residential units. Said notice of the hearing shall be published in a newspaper of general circulation within the city at least 30 days in advance of the hearing; written notice of the time and place of such meeting and the proposed action to be taken shall be posted upon the property and mailed to all owners of record of property within 1,500 feet of the property, and mailed to all households as determined from the listing of utility billing addresses within the entire city limits at least 30 days prior to the public hearing. The public notice posted on the property shall be erected to be in full view of the general public on each parcel, street side, and shall be erected by the applicant.
- (2) In addition to notice required by state law and Section 58-89, for proposed development within the OAO requiring Conditional Use Approval, but not qualifying for city-wide notice, public notice shall be required as follows: Said notice of the hearing shall be published in a newspaper of general circulation within the city at least 15 days in advance of the hearing; written notice of the time and place of such meeting and the proposed action to be taken shall be posted upon the property and mailed to all owners of property of record within 1,500 feet of the property requested for Conditional Use, at least 15 days prior to the public hearing. The public notice posted on the property shall be erected to be in full view of the general public on each parcel, street side, and shall be erected by the applicant.
- (3) All changes to OAO zoning code text and subarea maps and approval of Conditional Uses for all projects that meet the size, density or intensity requirements for city-wide notice, shall be deemed approved when the change has received the affirmative vote of a majority of the city commission on at least two (2) separate days at either regular or special meetings of the commission. If the city commission approves the required conditional use at the first public hearing, such approval shall not

be considered final until the second approval at the second public hearing.

m. Meaningful Open Space Requirements. It is the intent of the OAO to ensure that the development and enhancement of properties includes the creation of meaningful, useable, accessible, green and beautiful open space that invites the public to relax, interact, recreate, unwind and stimulate social connection. Where properties are planned for redevelopment, meaningful open space and the design of structures around these open spaces is the most important consideration. At a minimum, each property 2 acres in size and above, or any project covering 2 acres, that is redeveloped shall provide at least 25% meaningful open space, which is predominately visible from public right of ways, open to and available to the public. At least 65% of open-space areas provided shall be greenspace and 80% of open space areas shall be pervious or semi-pervious. Pervious areas such as retention ponds, parking lot islands or landscape planting areas around building bases shall not be counted as open space. Existing park space shall not count towards open space requirements. Open space shall be areas that are open and inviting to the public. Open space can include green areas, hardscape areas, semi-pervious areas, balcony or roof areas that are open to the public and other similar-type spaces. At least 90% of the open space shall be provided at ground level. The intent is that each of these areas create the opportunity for social interaction, relaxation, recreation and reflection. Given Winter Park's location on a recharge area of the Floridan Aquifer, nothing set forth in the OAO standards supersedes the requirement of Comprehensive Plan Policy 4-6.1.3 requiring not less than 25% of the total building lot land square footage be pervious ground. Is this excessive?

n. Signage Requirements. Signage within the OAO with frontage along Orange Avenue shall comply with the following requirements:

- (1) Each occupant shall be permitted a maximum of two signs indicating the business, commodities, service or other activity sold, offered or conducted on the premises. Where one occupancy has two signs, only the following combinations of sign types shall be permitted: One wall or canopy sign; one projecting sign and one wall or canopy sign; one canopy sign and one under-canopy sign. These signs shall also comply with the applicable provisions of Sections 58-125 through 58-128.
- (2) Projecting signs on properties or buildings within the OAO shall be limited to an area of each face of 20 square feet and shall have a minimum clearance of seven feet.
- (3) The maximum copy area of canopy signs shall be two square feet per linear foot of canopy front and sides. These signs should also comply with applicable provisions of Section 58-128.



- (4) Signs attached to the underside of a canopy (under-canopy signs) shall have a copy area no greater than six square feet, with a maximum letter height of nine inches, subject to a minimum clearance of seven feet from the sidewalk.
  - (5) OAO properties may not have digital, electronic, and/or internally-illuminated signs, such as backlit plastic, acrylic or glass. Front lighting of signs is encouraged. External illumination must be provided by a light source that is installed to prevent direct light from shining onto the street or adjacent properties. Flashing or moving lights are not permitted. Backlit halo-type opaque sign lettering is permitted; however, the light color must be white or subdued and muted such as a pastel shade. Sign faces and sides may not be translucent and must be an opaque material such as metal or wood.
  - (6) Ground signs or monument signs (excluding pole and pylon signs) are only permitted within the Subareas C, D, E, I, J and K. Ground signs within these subareas are limited to 30 square feet in size, and shall be located as to prevent interference with pedestrian and vehicular traffic. The design and location of such signs shall be subject to the approval of the Planning and Community Development director and/or Building Department director via a sign location and design plan to ensure that the sign does not interfere with pedestrian traffic, parking or does not create excessive signage in one area.
- o. Sidewalk Design and Utilization. All buildings, parking areas, public spaces, amenity features, and adjoining developments of similar use shall be linked with sidewalks. Sidewalks shall be provided along public streets that provide access to the development. A minimum 17-foot sidewalk is to be provided along Orange Avenue. Fairbanks Avenue and Orlando Avenue shall provide sidewalks at least ten feet wide with a minimum 5-foot landscape buffer along the back of curb, between the roadway and sidewalk. All other sidewalks in the OAO shall be at least 10-feet in width with a minimum 5-foot landscape buffer. If sufficient right-of way is not available, the building may need to be set back to accommodate these wider sidewalks and the additional space required to create the required sidewalks as defined herein shall be dedicated to public access through easement. Sidewalks shall be constructed in accordance with the standards for sidewalks set forth in City of Winter Park Engineering Standards and Landscape standards and constructed at the sole cost of the developer. Restaurants and cafes with seating within the public right-of-way shall be subject to the regulations contained in Chapter 90, Article VI - Sidewalk Cafes, of the Winter Park code of Ordinances.
- p. Display of Merchandise Outside of Commercial Buildings. Only properties within the OAO with frontages along Orange Avenue are allowed one display of merchandise to be located outside of a commercial business exclusive of beautification elements such as

plants (that are not for sale). This display must be placed within two feet of the front wall or window of the building. This display must not block or impede pedestrian traffic or be placed on the public sidewalk and at least six feet of clear sidewalk width must remain for pedestrian traffic. This display must be no more than six feet in height and no more than two feet in width. The display must be safely secured and removed under windy conditions. The display must be removed when the business is not open. An outside display is not permitted if the business chooses to place an outdoor portable sign.

q.

Localized Mobility Fee for Orange Avenue Area recommended by GAI.  
r. Contribution to Transportation Infrastructure: Development and redevelopment within the OAO requiring a conditional use approval shall contribute to the cost of transportation improvements prior to permitting based upon the estimated cost to fund the design, engineering, permitting and construction of those transportation projects within or proximate to the OAO that are impacted by such development or redevelopment. The owner and developer of a project shall enter into an agreement with the City, as part of conditional use approval. The City has initiated action to identify needed transportation improvements and establish a formula for calculating such fee. Credits against such future transportation impact fee will be negotiated depending upon timing of request for project permitting.

This is necessary, but is the OAO the place to do it? Should it apply to the entire city?

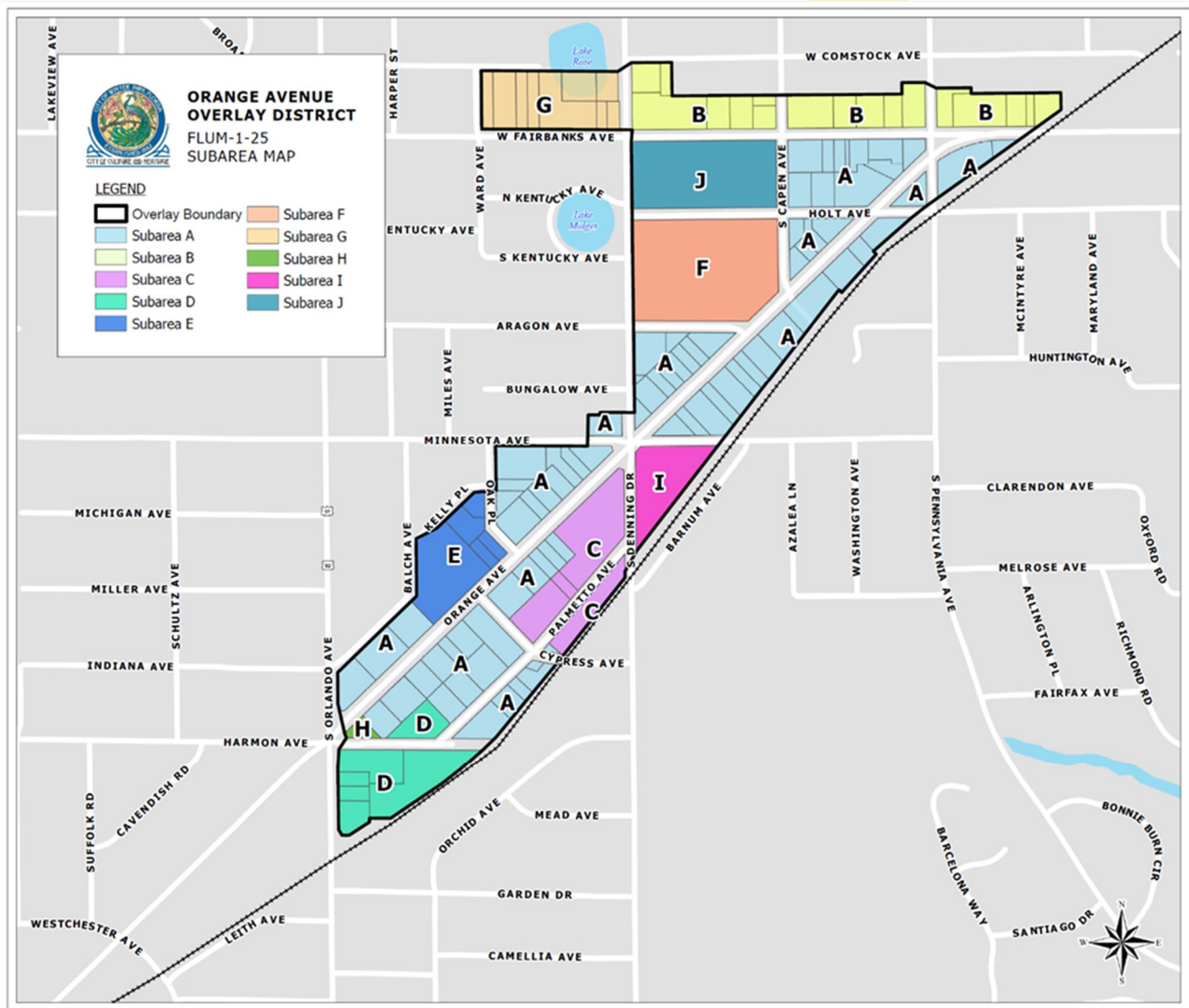
s. Contribution to Transportation **and Mobility** Infrastructure. Development and redevelopment within the OAO requiring a conditional use approval shall contribute to the cost of transportation **and mobility** improvements prior to permitting based upon the estimated cost to fund the design, engineering, permitting, and construction of those transportation **and mobility** projects within or proximate to the OAO that are impacted by such development or redevelopment. The owner and developer of a project shall enter into an agreement with the City, as part of conditional use approval. **addressing the project's contribution to transportation infrastructure, as part of conditional use approval. In the event the City establishes a mobility fee, road impact fee, special assessment or other funding mechanism a project's payment of such required fee may satisfy requirements of this policy. \*\*\* Incorporates City Attorney Comments**

t. Stormwater Management: To increase efficiency of land use, at the discretion of the Director of Planning and Transportation, stormwater management systems serving multiple building developments may be considered provided such systems are made



available before certificate of occupancy and recorded with a legal instrument acceptable to the City Attorney.

- u. Orange Avenue Overlay District Subareas and Standards for Development. The subarea map depicted below delineates the different subareas of the OAO and the specific standards, requirements and opportunities for each subarea. Each subarea has unique characteristics, issues and opportunities.



After the adoption of the OAO, no changes to the subarea map shall be allowed. Variances to height, number of stories, FAR, permitted/prohibited uses, required open space or maximum residential density shall not be granted for any property or development within the Orange Avenue Overlay District. Variances to other development standards shall be considered, with sufficient

showing of reasoning and hardship, as outlined in Section 58-90 and 58-92.

The standards detailed in the OAO are the maximum density and intensity parameters potentially permitted in each respective subarea. These maximum standards are not an entitlement and are not achievable in all situations. Many factors may limit the achievable FAR including limitations imposed by the maximum height, physical limitations imposed by property dimensions and natural features as well as compliance with applicable code requirements such as, but not limited to parking, setbacks, lot coverage and design standards.

Parcels to be developed or redeveloped within Subareas D, I & J shall contain at least 25% of the Cumulative Gross Floor Area as Mixed-Use, ensuring that no single-use developments occur that may not create the vibrant mixed-use district that is desired.

- (1) Subarea A. In order to allow for the remodeling and renovation of the buildings within this subarea and to enhance the OAO, these properties shall not be required to comply with those Land Development Code standards listed below, as long as additional square footage is not added to the buildings. If additional square footage is added, as to parking requirements, the properties shall be required only to provide the required parking for the new net square footage of the property. Any additional impervious surface created shall meet all City of Winter Park stormwater requirements for the new impervious area. All remodels, renovations or reconstructions that are allowed to remain as legally non-conforming under other Land Development Code requirements, shall still be required to meet applicable building and life safety codes as determined by the City Building Official and Fire Official. If the use of the building is changed (i.e. office conversion to café), parking shall be provided as required by Code.

- (a) Exemptions. The following Land Development Code Requirements shall not be required to be met for renovation or remodel of existing structures that will maintain the existing use, gross square footage, number of stories and conditions of the property:
  1. Floor Area Ratio (to the extent existing structure already exceeds FAR)
  2. Minimum Parking Requirements
  3. Stormwater Retention (but a minimum of 10 cubic feet of stormwater treatment and storage shall be created)
  4. Impervious Surface Percentage
  5. Setbacks
  6. Landscaping
  7. Height (no increase in stories)

(b) Reconstruction of Buildings. Given the age of buildings within Subarea A, the properties within this subarea shall be allowed to reconstruct the same building footprint (and square footage) when the building on the site is completely demolished, regardless of non-conforming status, on the site without being required to meet all development standards. Reconstruction of buildings shall be required to provide stormwater retention and the sidewalk widths as described herein. All building and life safety codes shall be met with all reconstruction.

(c) Subarea A Development Standards:

1. Base Floor Area Ratio (FAR): 45%
2. Maximum Achievable FAR with Residential: 60%
3. Maximum Height: 2 Stories for any properties abutting Orange Avenue and Minnesota Avenue, and 3 stories for all other properties within the subarea.
4. Maximum Impervious Coverage: 85%
5. Setbacks: None, except front setbacks must allow for at least a 17-foot wide sidewalk along Orange Avenue. Fairbanks Avenue and Orlando Avenue and other streets in the OAO shall each be designed to provide for a 10-foot wide sidewalk, with a minimum 5-foot landscape buffer on the back of curb. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2
6. Maximum Residential Density: 17 units per acre
7. Historic Preservation: it is a purpose of the OAO to protect and promote historic resources within the OAO. Towards that end the CRA in conjunction with the City, shall provide financial and administrative assistance to historic assets to submit applications for designation on the local and National Register of Historic Places. Additionally, discounts on license and permit fees shall be offered to incentivize registration on the local register of Historic Places. Applications for façade grants from historic assets shall be given prioritization.

(2) Subarea B.

(a) Subarea B Development Standards:

1. Base Floor Area Ratio: 45%
2. Maximum Achievable FAR with Residential: 60%
3. Maximum Height: 2 stories when property line is shared with residential use or zoning. Structures on parcels not having a common boundary with residential may increase to 3 stories if the 3<sup>rd</sup> floor is set back an

additional 10 feet from front and rear setback lines. 3<sup>rd</sup> floors shall only allow residential uses.

4. Maximum Impervious Coverage: 85%
5. Setbacks:
  - a. Front/Street: 20 feet. Front setbacks must allow for at least a 10-footwide sidewalk and 5-foot landscape buffer area on the back of curb along Fairbanks Avenue. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement.
  - b. Side: 0
  - c. Rear: 35 ft
  - d. Third stories shall require an additional 10-foot setback from the front and rear setbacks.
6. Maximum Residential Density: 17 units per acre

- (b) Additional Development Regulations. For properties within this Subarea B with an underlying zoning of single-family residential, these properties shall only be used for single-family residential use, open space, or transportation improvements.

(3) Subarea C.

(a) Subarea C Development Standards:

1. Base Floor Area Ratio: 25%  
Bonus FAR exclusively for Structured Parking: 65%
2. Maximum Achievable FAR with Parking Structure: 90%
3. Maximum Height: 2 Stories for Commercial Structures and Parking garage shall be allowed up to 4-levels. 1 story with a mezzanine level and a covered rooftop area for a total of 30 to 35 feet (can be an awning – needed to protect from elements) Total FAR must be divided between 2 buildings – do not want one building
4. Maximum Impervious Coverage: 85%
5. Setbacks: 0 front setback along Orange Avenue, Denning Drive, or Palmetto Avenue, except front setbacks on Orange must allow for at least a 17-foot wide sidewalk and setbacks on Denning and Palmetto must allow for a 10-foot wide sidewalk and 5-foot landscape buffer on back of curb. Where the building requires an additional setback to achieve a 17-foot sidewalk, the area shall be dedicated as a public access easement. The bike trail may substitute for applicable sidewalk requirements at the discretion of the Director of Planning. Where the building requires an additional

setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2

6. Maximum Residential Density: 0 units per acre

- (b) Intersection and Open Space Viewshed. Due to the unique shape of Subarea C and proximity to a unique intersection, this additional requirement creating a viewshed shall apply. The viewshed area is banded by the lines described as follows: Start where the property lines of Subarea C meet at the intersection of Denning Drive and Orange Avenue; then travel 150 feet southwest along Orange Avenue's southeast right of way line; thence easterly to the point on the west boundary of South Denning Drive that is 150 feet south of the starting point; then north along the west boundary of South Denning Drive to the starting point ("viewshed"). This viewshed shall be an open space area not available for the construction of structures or storage or placement of equipment, material or items otherwise allowed in the OAO.
- (c) Palmetto Re-Alignment. Palmetto Avenue may be relocated to allow for different development scenarios on the site. Protection of on-street parking, maximizing ease of traffic flow for Palmetto Avenue and maintaining the existing 50-foot public right of way shall be matters of priority concern should the roadway be realigned. Reference 3(j)1, Parallel Orange Avenue Access.
- (d) Additional Development Requirements.
- (1) A monument sign at least 3 feet in height and 5 feet in width, set in a landscaped bed, shall be required to be provided at the intersection of Denning Drive, Minnesota Drive and Orange Avenue, which directs the public to Mead Botanical Garden. The City shall approve the design and location of the sign.
  - (2) Include 1.5 acres of contiguous park space plus bicycle/pedestrian trail.
  - (3) No residential use.
  - (4) City to retain ownership of this parcel in perpetuity.
  - (5) Stormwater requirements to exceed code.
  - (6) Contribute to parking needs of small businesses in the area.
  - (7) Walkways must exist between buildings onsite and extend from the park area to Cypress Ave.

(4) Subarea D.



- (a) Subarea D Development Standards:
1. Base Floor Area Ratio (FAR): 60%
  2. Maximum Achievable FAR with Enhancements: 100%
  3. Maximum Height: 5 Stories
  4. Maximum Impervious Coverage: 75%
  5. Setbacks:
    - a. Street: Minimum 50 feet along Orlando Avenue. Setbacks must allow for at least a 10-foot wide sidewalk and 5-foot landscape buffer area on the back of curb along Orlando Avenue and Palmetto Avenue. 0 feet along Orange Avenue—except front setbacks must allow for at least a 17-foot wide sidewalk. The bike trail may substitute for applicable sidewalk requirements on one side of realigned Palmetto, at the discretion of the Director of Planning. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2.
  6. Rear: 35 feet
  7. Maximum Residential Density: 17 units per acre
    - (a) Residential Uses shall only be allowed above the ground floor fronting Orange Avenue. On a case by case basis the City Commission may permit the maximum density in this subarea to be exceeded by up to 10% per acre when such allowances are used exclusively for the construction of inclusionary affordable/workforce housing maintained for a period of not less than 20 years. An agreement with terms acceptable to the City and with City Attorney review shall be executed as part of the Conditional Use approval.
- (b) Required Development Enhancements. In order to be eligible for any Development Enhancement Bonuses, any future development of the property within Subarea D shall include structured parking and the following item(s) from the Development Enhancement Menu: CT.1.
- (c) Road Realignment. Realignment of the Harmon Avenue or Vivian Avenue right-of-way may be considered, subject to City Commission approval and shall require dedication of equivalent Right-Of-Way for the re-alignment of Palmetto Ave. Harmon Avenue currently serves as access to businesses on Palmetto Avenue. Replacement of on-street parking and maximizing ease of traffic flow for Palmetto Avenue shall be matters of priority concern should the City

Commission approve closing or vacation of Harmon Ave or Vivian Avenue. Reference 3(j)1, Parallel Orange Avenue Access.

(5) Subarea E. (NOTE: this was the subarea west of 17-92, but is now the Jewett property – formerly K)

(a) Subarea E Development Standards:

1. Base Floor Area Ratio: 45%
2. Maximum Achievable Floor Area Ratio with structured parking: 60%.
3. Maximum Height: Maximum 2 Stories
4. Maximum Impervious Coverage: 85%
5. Setbacks:
  - a. 0 front setback along Orange Avenue, except front setbacks must allow for at least a 17-foot wide sidewalk and 5-foot landscape buffer at back of curb. All other street frontages must allow for a 10-foot sidewalk and 5-foot landscape buffer at back of curb. Where the building requires an additional setback to achieve a 17-foot sidewalk, the area shall be dedicated as a public access easement. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2
  - b. Side: 5 feet
  - c. Rear: 20 feet. If abutting residential, shall be a minimum of 35 feet or equal to building height, whichever is greater.
6. Maximum Residential Density: 17 units per acre

(6) Subarea F.

(a) Subarea F Development Standards:

1. Base Floor Area Ratio: 20%
2. Maximum Achievable Floor Area Ratio: 20%
3. Maximum Height: 2 Stories
4. Maximum Impervious Coverage: 50%
5. Setbacks: 20 feet. Orange Avenue setbacks must allow for at least a 17-foot wide sidewalk and 5-foot landscape buffer area on the back of curb. Setbacks on Capen Avenue, Aragon Avenue and Denning Drive must allow for at least a 10-foot wide sidewalk and 5-foot landscape buffer area on the back of curb. Where the building requires an additional setback to achieve a 17-foot sidewalk, the area shall be dedicated as a public access easement. Where the building requires an additional



setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2

6. Maximum Residential Density: No Residential Uses Allowed

- (b) Additional Subarea Requirements: This area shall not be used for any other future purpose than public space, recreation, open sports/recreation facilities, including ancillary structures, or the parking needed to support these uses. No offices, classrooms, residences or other college uses shall be allowed on the property.

#### (7) Subarea G. - remove Subarea G

##### Subarea G Development Standards:

1. Base Floor Area Ratio: 45%
2. Maximum Achievable Floor Area Ratio: 45%
3. Maximum Height: 3 Stories at max of 35 ft
4. Maximum Impervious Coverage: 85%
5. Setbacks: 20 feet. Setbacks must allow for at least a 10-foot wide sidewalk and 5-foot landscape buffer area along Orlando Avenue. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2
6. Maximum Residential Density: 17 Units/Acre

Additional Development Requirements. This Subarea G represents an opportunity to expand Martin Luther King Jr., Park to create an increased greenway connection to Mead Botanical Garden, for cleanup of environmental hazards threatening water quality, educational opportunities regarding karst formation (sinkholes) and to improve traffic flow at a failing intersection. It is the intent of the City of Winter Park to acquire these properties for the extension of Martin Luther King, Jr. Park and to provide for transportation improvements. Any properties acquired by the City of Winter Park shall be dedicated as parkland or towards transportation improvements.

#### (8) Subarea H.

##### (a) Subarea H Development Standards:

1. Base Floor Area Ratio: 0%
2. Maximum Achievable Floor Area Ratio: 0%

3. Maximum Height: N/A
4. Maximum Impervious Coverage: N/A
5. Setbacks: N/A
6. Maximum Residential Density: N/A

(9) Subarea I.

(a) Subarea I Development Standards:

1. Base Floor Area Ratio (FAR): 45%
2. Maximum FAR with Residential: 60%
3. Maximum Achievable FAR with Enhancements: 100%
4. Maximum Height: 3 Stories
5. Maximum Impervious Coverage: 75%
6. Setbacks: Orange Avenue 0 front setback, except front setbacks must allow for at least a 17-foot wide sidewalk. On Denning Drive and Minnesota Avenue setbacks must allow for at least a 10-ft wide setback and a 5-ft planting strip back of curb. Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2
2. Maximum Residential Density: 17 units per acre. Residential Uses shall only be allowed above the ground floor. On a case-by-case basis the City Commission may permit the maximum density in this subarea to be exceeded by up to 10% per acre when such allowances are used exclusively for the construction of inclusionary affordable/workforce housing maintained for a period of not less than 20 years. An agreement with terms acceptable to the City and with City Attorney review shall be executed as part of the Conditional Use approval.

(b) Required Development Enhancements. In order to be eligible for any Development Enhancement Bonuses, any future development of the property within Subarea I shall include structured parking and the following item(s) from the Development Enhancement Menu: CT.7. The City of Winter Park shall determine the area required to be dedicated for intersection improvements.

(c) Intersection and Open Space Viewshed. Due to the unique shape of the property and proximity to a unique intersection, this additional requirement creating a viewshed shall apply. The viewshed area is bounded by the lines described as follows: start where the property lines of Subarea I meet at the intersection of Denning Drive and Minnesota Avenue; then travel 50 feet east along Minnesota Avenue's southern right of way line; thence south westerly

to the point on the east boundary of S. Denning Drive that is 30 feet south of the starting point; then north to the starting point ("viewshed"). This viewshed shall be an open space area not available for the construction of structures or storage or placement of equipment, material or items otherwise allowed in the OAO. This viewshed is in addition to other setback requirements for Subarea I.

(10) Subarea J.

(a) Subarea J Development Standards:

1. Base Floor Area Ratio: 60%
2. Maximum Achievable Floor Area Ratio with Enhancements: 100%
3. Maximum Height: 3 stories fronting on Fairbanks Avenue and Denning Drive. 4 Stories when setback 80 feet from Fairbanks Avenue and Denning Drive. To the extent buildings on the southern half of this parcel are oriented towards Holt Avenue and back to a continuous service/emergency ally, not less than 30 feet in width running from Capen Avenue to Denning Drive, the maximum height on the southern half of this parcel is 4 stories. (Note: Get input from Public Works Intent to accommodate dumpster and drive aisle and drop off like Center Street).
4. Maximum Impervious Coverage: 75%
5. Setbacks: 20 feet Fairbanks Street Setback. Setbacks must allow for at least a 10-foot wide sidewalk and 5-foot landscape buffer area at the back of curb along Fairbanks Avenue. On Holt Avenue, Denning Drive and Capen Avenue setbacks must allow for at least a 10-ft wide sidewalk and a 5-ft planting strip back of curb. ~~at adjacent streets.~~ Where the building requires an additional setback to achieve the required sidewalk, landscape buffer, or street tree canopy clearance, the area shall be dedicated as a public access easement. Additionally, all building setbacks shall be measured from the property line and shall meet the requirements as depicted of 3.1.i.2
6. Rear Setbacks (setbacks between rear of buildings): 30 feet. A service alley of 30 feet running from Denning Drive to Capen Avenue may serve as the required rear setback for buildings fronting Fairbanks and Holt.
7. Maximum Residential Density: 17 units per acre. Residential Uses shall only be allowed above the ground floor. On a case-by-case basis the City Commission may permit the maximum density in this subarea to be exceeded by up to 10% per acre when such allowances are used exclusively for the construction of inclusionary affordable/workforce housing maintained for a period of not less than 20 years. An agreement with terms

acceptable to the City and with City Attorney review shall be executed as part of the Conditional Use approval.

8. Access to the property shall only be allowed from Capen Avenue or Holt Avenue.

(b) Required Development Enhancements: In order to be eligible for any Development Enhancement Bonuses, any future development of the property within Subarea J shall include structured parking and the following item from the Development Enhancement Menu: CT.7. The City of Winter Park shall determine the area required to be dedicated for intersection improvements and the required land may be property under common ownership located offsite from Subarea J.

v. Orange Avenue Overlay District Development Enhancements/ Bonuses. Enhanced development entitlements shall be considered for the OAO developments including structured parking, but shall be earned, rather than simply granted.

(1) Percentage-Based Development Enhancement Menu. A percentage-based upgrade system for properties with new developments or redevelopments that include structured parking within the OAO is hereby established. For properties to obtain additional development entitlements, Floor Area Ratio (FAR) beyond what is provided in the relevant subarea or underlying zoning, the following provisions and Tables are established.

(2) Property owners or developers providing structured parking may use any combination of the Development Enhancement Menu to obtain up to the Maximum Achievable Floor Area Ratio. Certain subareas shall require certain Enhancements to be met. Each development enhancement utilized shall be required to be designed and shown on any development plans submitted for development of a property. Off-site improvements shall require plan submittal for the area where the improvements are proposed. Any enhancement or improvement shall be designed by a licensed professional (Architect, Civil Engineer, Landscape Architect, Structural Engineer, etc.) as determined by the Planning Director.

(3) Unless otherwise stated in the Development Enhancement Menu, the developer is obligated to maintain, operate and replace with comparable product at the end of the enhancements useful life for a period of 20 years based on industry standards and best practices.

(4) At the end of the 20-year period, or should the development be torn down, or transfer ownership; the City at its sole discretion may take ownership or take over the maintenance of the asset. The City will be credited for remaining value of the obligation. (This language needs legal rewording.)

- (5) The following OAO Development Enhancement Menu was created to address the wide-ranging issues affecting the Orange Avenue area, while meeting the goals of the Comprehensive Plan, the Vision Winter Park plan and the Winter Park Sustainability Plan. To ensure that the intent of the Development Enhancement Menu is met, any project that utilizes this menu shall be reviewed by the Planning Director in order to make a recommendation to the City Commission as to whether and to what extent an increase in FAR is allowed.
- (6) Where the City requires Transportation, Affordable Housing or Parks Usage Fees, those fees are separate from enhancements proposed in the following Enhancement/Incentives Menu. Unless otherwise stated within this section, enhancements are not eligible to serve as credits against otherwise required fees.

**Table 1: Orange Avenue Overlay District Development Enhancement Menu – Sustainability Category**

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
S-2	Green Roof	At least 50% of the total surface area of the principal building's roof is a green roof constructed in accordance with ASTM green building standards.	10%	10%
S-4	Recycling	Recycling Receptacles provided for each use on the site building and large collection receptacle placed in the dumpster area of the site.	2%	2%
S-5	Rainwater Reuse	At least 75% of rain water from the roofs of structures is captured and recycled for landscape irrigation.	5%	5%

**Table 2: Orange Avenue Overlay District Development Enhancement Menu – Infrastructure & Stormwater Category**

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
IS-1	Stormwater Retention Beyond Code Minimum Requirements	Each 1% of additional water quality treatment and stormwater retention capacity, beyond what is required to accommodate development of a site provided (that can feasibly receive off-site stormwater) shall earn a 0.5% entitlement bonus. The calculation shall be based on the retention required on the site to meet City and St. Johns requirements. Vaulting, Underground Storage or Raingarden Areas Shall Be Allowed.	Max 15% Entitlements Bonus	15%

**Table 3: Orange Avenue Overlay District Development Enhancement Menu – Arts & Culture Category**

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase

AC.3	Space For Non-Profit Arts & Cultural Organizations	For each 1,000 square feet of space that is built specifically and solely for non-profit arts and cultural facilities, a 1% entitlement bonus shall be granted. The space provided for these non-profit users shall not count towards the FAR of the site. The space shall only be rented to Arts & Cultural organizations with non-profit 501.C.3 status, in perpetuity. Parking shall also be provided and shared parking is encouraged. The rents charged shall not exceed 80% of the median rents charged for similar properties in the area. The rents shall not increase more than 3% per year.	Max 15% Entitlement Bonus	15%
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Table 4: Orange Avenue Overlay District Development Enhancement Menu – Parking Category

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
P.1	Public Parking	Each 3 Dedicated Public Parking Spaces within a Parking Structure, Which Remain Free for Public Parking at All Times, Shall Earn a 1% Entitlement Bonus	Max 10% Entitlement Bonus	10%

Table 5: Orange Avenue Overlay District Development Enhancement Menu – Miscellaneous Category

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
	Workforce Housing Provided	Each Unit Provided at Orange County Workforce Housing Standards Shall Earn a 0.5% Entitlement Increase	Max 20 Units	10%

Table 6: Orange Avenue Overlay District Development Enhancement Menu – Meaningful Open Space Category

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
OS.1	Open Space Beyond Minimums	Each 1/4 acre (10,890 square feet) of additional dedicated open space available to the public (When?) beyond the required minimum shall earn a 5% Entitlement Bonus, or 20% per acre.	20%	20%
OS.2	Shade Tree Planting	Each 25-inches of Shade Tree Caliper Planted Onsite Beyond Minimum Requirements Shall Earn 1% Entitlement Bonus. Species Shall Be City Arborist Approved and Planted with Irrigation. A minimum 5" caliper tree shall be required.	Max 250-Inch Tree Caliper Bonus	10%
OS.3	Tree Fund Donation	Payment may be made into the City of Winter Park Tree Replacement Trust Fund, so that meaningful trees can be planted throughout the City to maintain and grow our tree canopy. For each donation of \$10,000 to the Tree Replacement Trust Fund, a 1% Entitlement Increase Shall Be Granted.	Maximum \$50,000 Donation	5%



Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
OS.4	Mead Garden Improvements	Donation to City of Winter Park designated for Mead Botanical Garden Improvements/Restoration/Enhancements Shall Earn a 1% Entitlement Increase for Each \$10,000 Donation. The Funds Shall Only Be Used for Capital Improvements or Enhancements in Mead Botanical Garden.	Max \$100,000.00 Donation	10%
OS.5	Donation of Land for Parks	For each 5,000 square feet of land donated to the City of Winter Park for park space located adjacent to existing public parkland (which is accepted by the City Commission as meaningful and useful park land), shall earn an additional 1% Entitlement Increase.	Maximum 20%	20%
OS.6	Martin Luther King, Jr. Park Expansion	Donation to the City of Winter Park, Park Acquisition Fund Shall Earn a 1% Entitlement Increase for Each \$10,000 Donation. Funds Shall Only Be Used for the Acquisition of Additional Park Land. The Funds Shall Only Be Used For the Acquisition of the Area Identified as Subarea "G" herein, to expand Martin Luther King, Jr. Park.	Max \$100,000.00 Donation	10%
OS.7	Social Connection Amenities	Provide amenities, that support community interaction and are open to the general public, creating third places:	Max Based on Type	1%
		Yard games (life-size chess/checkers, bocce ball, bean-bag toss, walking labyrinth)		3 earns 1%
		Multi-Generational Play Areas		3%
		Fountain/splash pad/water feature		3%
		Stage areas for music/art performance		2%
		Dedicated Standalone Public Restrooms (not a part of a business onsite)		2%
		Public seating/gathering spaces of significant size (street furniture, seating walls, outdoor furniture, fire pits)		2%

Table 7: Orange Avenue Overlay District Development Enhancement Menu – Connectivity & Transportation Category

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
CT.1	Rail-Trail Construction & Easement	Option A - Provide Dedicated Utility & Public Access Easement of a Minimum of 20 feet and Construction of Rail-Trail with a 12-foot Trail Width, to match regional trail widths and 4-foot planting strip along each side within said 20 foot easement, earns 1% Entitlement Bonus for each 50 linear feet of trail, with decorative light pole (as selected by City of Winter Park to match other areas of town) & shade or understory tree of minimum 5" caliper (as selected by Urban Forestry) with irrigation for every 50 feet of railroad frontage. The trail and easement shall connect from the property line where the rail enters, to the property line where the rail exits. The trail shall be designed to align with existing or future trail locations and the design of the trail shall be determined on the site plan when a project is submitted for consideration	Max 20%	20%
CT.2	Off-Site Trails	Donation to the Construction of Bike/Pedestrian Trails. Due to the unique circumstances and properties in each area, every section of future trail will have challenges and opportunities. Because no two areas are the same, it is preferable to have developers pay into a trails fund, with design and installation provided by the City. Each \$10,000 donation shall earn a 1% entitlement bonus.	Max \$100,000 donation	10%
CT.3	Denning Drive Mobility Extension	Complete Extension of Denning Drive from Orange Ave to Mead Botanical Garden, Minimum 12' Wide Multi-Use Paved Path With Decorative Lighting, and Shade Tree in Grate with Irrigation Every 50 feet, with Required ADA Crossings & Signage on E side of Denning Drive	25%	25%
CT.5	Bicycle/Pedestrian Repair Facilities & Rest Areas	Provide bicycle/pedestrian amenities that are available to the community near any Bike Trail facility. One of each of these facilities shall be allowed to locate in one or more of the following locations: Rail Trail area as defined in this chapter, In Martin Luther King, Jr. Park along a bike trail, Along the new Bike Path connecting to Mead Botanical Garden, or along the Denning Drive bicycle facilities. Each location shall require the following elements under a covered roof or shade area: Bicycle Fix-It Stations with bike lift, air pump and tools; water fountain and water bottle filling; bike rack; trash and recycling receptacles; and a bench.	2% per location, Max 3 locations per development. Can be located off-site	6%
CT.6	Covered Transit Stops	Bench, Trash Receptacle, Recycling Receptacle and Covered Area Provided for Transit Users At a Stop on a Bus Route	1%	1%

Category Number	Enhancement	Description & Potential Bonus	Enhancement Achievement Maximums	Maximum FAR Increase
CT.7	Land Donation for Transportation Improvements	Each 100 square feet of land dedicated to the City of Winter Park or FDOT as right-of-way for needed transportation improvements, shall earn a 1% Entitlement Bonus	Max 25%	25%

w. Definitions.

- (a) **Affordable Housing:** Affordable housing means a dwelling unit, with regard to a unit for sale, which costs less than 80 percent of the median price of the single-family homes sold the previous year in the Orlando metropolitan area; and with regard to a unit for rent, one which rents monthly for less than 80 percent of the median monthly cost of similar sized units for the previous year in the Orlando metropolitan area and for which the purchaser's or renter's income or combined family income does not exceed 80 percent of the median family income for the Orlando metropolitan area.
- (b) **Boutique Hotel:** A boutique hotel having typically less than 120 rooms and is largely characterized by its smaller size, personalized service and local personality, which can vary dramatically depending on where the property is located. They cater to the individual, providing very personalized, intimate service. These properties are designed to blend into the community and reflect the neighborhoods and cultures around them.
- (c) **Building Story:** Building story means a section of a building between the surface of a floor and the floorplate of the floor above it.
- (d) **Common Ownership or Commonly Owned:** Properties shall be deemed to be under "common ownership" or "commonly owned" if the properties are owned by the same entity or affiliated entities with substantially similar control and management.
- (e) **EIFS:** Exterior Insulation and Finish System. A non-load bearing exterior wall cladding system consisting of a thermal insulation board, adhesively and/ or mechanically attached to the substrate, base coat with reinforced fiberglass mesh and a textured finish coat.
- (f) **Fast Casual Restaurant:** Fast casual restaurants offer consumers freshly-prepared, higher-quality food in an informal setting, with counter service to keep things speedy.
- (g) **Floor Area Ratio (FAR):** The gross floor area divided by the land area of the building site excluding land areas across a public street under the same ownership. The gross floor area ratio is the square footage of the building or buildings on the property (and contiguous properties being used in connection with such

building(s)) divided by the area of such property in square feet. This mathematical expression (gross floor area ÷ land area = floor area ratio) shall determine the maximum building size permitted. The floor area of parking structures is included in the calculation of the Floor Area Ratio with the exception of underground parking, open-air top-level parking and the 10% parking in excess of code required in accordance with Section 58:83 j(7)(c).

- (h) Food Hall: Unlike food courts made up of fast food chains, food halls typically mix local artisan restaurants, butcher shops and other food-oriented boutiques under one roof.
- (i) Green Roof: A green roof or living roof is a roof of a building that is partially or completely covered with vegetation and a growing medium, planted over a waterproofing membrane. It may also include additional layers such as a root barrier and drainage and irrigation systems. Green roofs serve several purposes for a building, such as absorbing rainwater, providing insulation, creating a habitat for wildlife, increasing benevolence and decreasing stress of the people around the roof by providing a more aesthetically pleasing landscape, and helping to lower urban air temperatures and mitigate the heat island effect.
- (j) Green Wall/Living Wall: Living walls or green walls are self-sufficient vertical gardens that are attached to the exterior or interior of a building. They differ from green façades (e.g. ivy walls) in that the plants root in a structural support which is fastened to the wall itself. The plants receive water and nutrients from within the vertical support instead of from the ground.
- (k) Impervious Area: Impervious Areas are man-made areas that cannot absorb water from rain or snow. Impervious Area Examples: Roofs; Roads; Sidewalks; Driveways; Parking Lots.
- (l) Meaningful Open Space: Privately -owned property that is not a part of the inside of a building. These areas are intended to provide for the use and benefit of the general public, and are legally accessible by the general public. These areas are accessible and designed for outdoor living, gathering, landscaping, recreation, pedestrian activity, meaningful, useable, accessible, green and beautiful open space that invites the public to relax, interact, recreate, unwind and stimulate social connection. Open space shall not be retention ponds, parking lot islands or landscape planting areas around building bases. Meaningful opens spaces are areas that are open and inviting to the public. Open space can include green areas, hardscape areas, semi-pervious areas, balcony or roof areas that are open to the public and other similar-type spaces. The intent is that each of

these areas create the opportunity for social interaction, relaxation, recreation and reflection.

- (m) **Mixed-Use:** Mixed-use development combines two or more types of land use into a building or set of buildings that are physically and functionally integrated and mutually supporting. This can be some combination of residential, commercial, industrial, office, institutional, or other land uses. The form of mixed-use development can be vertical or horizontal. Vertical mixed-use occurs when different uses inhabit the same building and sit atop one another, such as residential or office uses over ground floor retail. Horizontal mixed-use occurs when uses are placed next to each other, such as an apartment building that is adjacent to offices, restaurants, or retail shops. Mixed-use areas often create the main street/downtown, activity center, or commercial corridor of a local community, district, or neighborhood. They frequently involve stacking uses – residential or office above retail, for example, in low or midrise buildings, but are predominately made up of a variety of individual buildings arranged around streets and around public squares or other open spaces.
- (n) **Multi-Generational Play Area:** Instead of focusing exclusively on children, these playgrounds broaden their scope to include equipment, activities and amenities for those older than age 12—and perhaps significantly older—so that anyone who visits the playground, regardless of age or ability, can find something there they enjoy.
- (o) **Multi-Modal Transportation:** This concept is that all modes of transportation should have equality and there shouldn't be the typical hierarchy where private automobiles have more opportunities at the cost of pedestrians, cyclists, public transportation users and handicapped persons.
- (p) **Overlay District:** An area where certain additional requirements are mapped upon an underlying zoning district(s). The district modifies or supplements the underlying zoning regulations and allows for flexibility in design and the ability to apply more area specific requirements including, but not limited to, architecture, height, setbacks, use, open space, landscaping, historic preservation, floor area ratio, parking, public improvements, access, stormwater, etc. In the instance of conflicting requirements, the stricter shall apply.
- (q) **Percentage-Based Development Enhancement:** In exchange for the ability to obtain additional development entitlements above those currently allowed by the underlying zoning. Subject to approval by the City Commission, certain public improvements and area-wide solutions will be required by those who develop or redevelop properties and the requirements for Development

Enhancement Bonuses must be met. Subject to approval by the City Commission, property owners or developers may propose use of any combination of the Development Enhancement Menu to earn their way up to the Maximum Achievable Floor Area Ratio.

- (r) Pervious Area: A pervious surface is a surface that allows the percolation of water into the underlying soil. Pervious surfaces include grass, mulched groundcover, planted areas, vegetated roofs, permeable paving as well as porches and decks erected on pier foundations that maintain the covered lot surface's water permeability.
- (s) Placemaking: As both an overarching idea and a hands-on approach for improving a neighborhood, city, or region, placemaking inspires people to collectively reimagine and reinvent public spaces as the heart of every community. Strengthening the connection between people and the places they share, placemaking refers to a collaborative process by which we can shape our public and private realm in order to maximize shared value. More than just promoting better urban design, placemaking facilitates creative patterns of use, paying particular attention to the physical, cultural, and social identities that define a place and support its ongoing evolution. With community-based participation at its center, an effective placemaking process capitalizes on a local community's assets, inspiration, and potential, and it results in the creation of quality public spaces that contribute to people's health, happiness, and well-being.
- (t) Public Improvements: Any drainage facility, roadway, parkway, pedestrian way, off-street parking area, lot improvements, sidewalk, bike lane, park, public facility, pedestrian crossing, boulevard or other facility which benefits the public.
- (u) Residential Density: Measured in dwelling units per gross acre. Maximum densities determine the number of apartment, townhome, condominium or other multifamily units allowed.
- (v) Walkability: A measure of how well streets are designed to incorporate pedestrian scale elements and to create equal access for pedestrians. A walkable area has health, environmental, and economic benefits. It keeps pedestrians interested, safe and engaged with the built environment around them. With community-based participation at its center, an effective placemaking process capitalizes on a local community's assets, inspiration, and potential, and it results in the creation of quality public spaces that contribute to people's health, happiness, and well-being.
- (w) Workforce Housing: A dwelling unit, with regard to a unit for sale, which costs less than 120 percent of the median price of the



single-family homes sold the previous year in the Orange County metropolitan area; and with regard to a unit for rent, one which rents monthly for less than 120 percent of the median monthly cost of similar sized units for the previous year in the Orange County metropolitan area, and for which the purchaser's or renter's income or combined family income does not exceed 120 percent of the median family income for the Orange County metropolitan area.

SECTION 2. Vesting. In order to not adversely affect development projects that may be in process and for which expenditures have been made in reliance upon the existing code provisions, the City will allow certain developments to be subject to the underlying zoning of the property prior to the adoption of this Ordinance provided such development's site and building floor plans have been received and approved by the City prior to the effective date of this Ordinance. However, for any development project that received a conditional use approval from the City Commission prior to the effective date of this Ordinance, the expiration of that conditional use approval per Section 58-90 shall apply.

SECTION 3. Severability. If any Section or portion of a Section of this Ordinance proves to be invalid, unlawful, or unconstitutional, it shall not be held to invalidate or impair the validity, force, or effect of any other Section or part of this Ordinance.

SECTION 4. Conflicts. To the extent any provision or provisions of this Ordinance conflict with the provision or provisions of other Ordinances, the provisions of this Ordinance control.

SECTION 5. Codification. Section 1 of this Ordinance shall be codified and made a part of the City of Winter Park Land Development Code, and the sections of this Ordinance may be renumbered or relettered to accomplish this intention. The word "Ordinance" may be changed to "Section," "Article," or other appropriate word. The City Clerk is given liberal authority to ensure proper codification of this Ordinance, including the right to correct scrivener's errors.

SECTION 6. Effective Date. This Ordinance shall become effective upon the comprehensive plan amendments provided for under Ordinance \_\_\_\_\_ become effective. If Ordinance \_\_\_\_\_ does not become effective, then this Ordinance shall be null and void.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, held in City Hall, Winter Park, on this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

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Mayor **Steve Leary**

Attest:

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City Clerk

DRAFT

	A	B	C	D	E
1				Commission Comments on OAO Zoning Code Draft - Dated 3.18.21	
2	Commissioner	Zoning Code Page/Section		Topic Referenced	DeCiccio Comments - 4-7-21
3	Cooper	6	c. Permitted Uses	No external signage shall be permitted for accessory uses.	Want to make sure that businesses operating out of a larger building or under a main tenant (example - businesses on Morse off Park Avenue) are able to have signage of their own.
4	DeCiccio	6	e. Prohibited Uses	(2) Automotive-related businesses (i.e. auto sales, auto repair, auto rental, body shops, auto wash, auto audio, auto glass, auto tinting, auto parts sales)	what about Holler doing a McLaren type business? This needs further discussion.
5	DeCiccio	10	j. Architectural Standards	When mezzanines, balconies or lofts are provided, first floor heights of 20 feet may be allowed.	Is 20 feet tall enough for a mezzanine?
6	DeCiccio	20	f. Multi-Family parking	(f) Multi-family residential: 2.5 spaces per dwelling unit in accordance with Sec 58-86	Do not need 2.5 spaces per unit needs to be based on the size of the units (example studio/1bedroom does not need 2.5 spaces)
7	Cooper	24	m. Open Space Requirements	"Given Winter Park's location on a recharge area of the Floridan Aquifer, nothing set forth in the OAO standards supersedes the requirement of Comprehensive Plan Policy 4-6.1.3 requiring not less than 25% of the total building lot land square footage be pervious ground."	Is this excessive? Need to discuss
8	Cooper	26	r. Contribution to Transportation Infrastructure	Development and redevelopment within the OAO requiring a conditional use approval shall contribute to the cost of transportation improvements prior to permitting based upon the estimated cost to fund the design, engineering, permitting and construction of those transportation projects within or proximate to the OAO that are impacted by such development or redevelopment. The owner and developer of a project shall enter into an agreement with the City, as part of conditional use approval. The City has initiated action to identify needed transportation improvements and establish a formula for calculating such fee. Credits against such future transportation impact fee will be negotiated depending upon timing of request for project permitting.	This is necessary, but is the OAO the place to do it? Should it apply to the entire city?
9	DeCiccio	31	Subarea C	Building heights and other Requirements	1 story with a mezzanine level and a covered rooftop area for a total of 30 to 35 feet (can be an awning needed to protect from elements) . Total FAR must be divided between 2 buildings do not want one building.
10	DeCiccio	32	Subarea C	Additional Development Requirements	Walkways must exist between buildings onsite and extend from the park area to Cypress Ave.

	A	B	C	D	E	F	G	H	I	J
1				COMMENTS TO 3-18-21 DRAFT OF OAO 2.0 ZONING CODE DRAFT						
2	Commissioner	CP/LUDC Page	CP/LUDC Reference	Recommendation	Commission Consensus	Phil 4-3-21				
3	Cooper	3 thru 4	Purpose	We have not addressed many of the purposes listed. Let's incorporate provisions to address them. Example: (16) promoting history including Designers Row, (18) create Arts and Culture Corridor, (19) protect and promote Historic architecture (both the Playhouse at 711 N Orange Ave and the Dentist office at 1355 Orange Avenue are recognized historic resources ) and (20) lastly encouraging locally-owned businesses.		ok				
4	Anderson	4	Purpose	(12) Discuss and ensure application of the Code/Comp Plan DO IN FACT follow the principles of the Vision and Sustainability Plan, or adapt the language		ok				
5	Anderson	4	Purpose	(13) note that we are encouraging mixed use, even if the mixed use is within the overlay, versus in a single parcel.		ok				
6	Cooper	7	f. Non conforming Uses	Ensure businesses that are non-conforming uses are advised of these provisions in time to participate in public hearings.		ok				
7	Anderson	7	g. Residential Uses	DISCUSS: how to avoid patchworks of vacant lots that diminish the "viability" of a neighborhood?		ok				
8	Cooper	8	Traditional Neighborhood Design (TND)	<p>Insert new section here: Standards for Circulation. Traditional Neighborhood Design (TND)</p> <p>(1) The OAO is designed to enhance pedestrian walkability, connectivity and safety and to create a safe, comfortable and convenient pedestrian experience. Towards this end, the OAOs transportation system is based on inter-connected streets forming small blocks similar to successful patterns of the more walkable areas of the City such as the Park Avenue Corridor.</p> <p>(2) Block Configuration/Lot Standards: Any additional development or redevelopment of parcels in excess of 2 acres within the OAO shall be required to create a block structure and pedestrian corridors conducive to pedestrian safety, comfort, and vehicular circulation.</p> <p>a) New or redeveloped buildings within the OAO shall have building widths of between a minimum of 50 feet and a maximum of 300 feet wide where they interface with pedestrian oriented street frontages.</p> <p>b) Development shall provide a continuous driveway or alley that connects to adjacent parcels and roadways and provides access for emergency vehicles and city services.</p> <p>c) To the extent possible, given the size and shape of the parcel, new or redeveloped blocks shall be between a minimum perimeter of 800 linear feet and a maximum perimeter of 1300 linear feet. The City goal is to encourage walkability based upon successful Park Avenue Corridor block perimeters of 1100 linear feet. Where existing block perimeters are smaller than 1300 linear feet, no further breakdown is required. Where block perimeters are larger than 1300 linear feet, new development shall be organized to break the land mass into walkable blocks.</p> <p>d) Driveways or alleys shall be constructed at widths adequate for dumpsters, trash compactors and circulation of solid waste collection vehicles, large truck delivery and allow emergency vehicles to maneuver past parked delivery vehicles and waste receptacles .</p>		For further discussion. Question the alleyway				
9		8	I Circulation (3) Street Realignment	Recommend J1 Parallel Orange Avenue Access be included here. Use language that ensures the City owns and controls any dedicated ROW that replaces abandoned ROW.						
10		9	(4) Street Section (f)	What is a realistic process and timing to pursue SunRail ROW use?						
11	Cooper	44418	I Circulation Street Sections	<p>Add new: <del>(4)</del> Street Sections: Any new, realigned, or redesigned street sections within the OAO shall be designed to create and maintain a quality comfortable walking environment encouraging the use of multimodal transit options including:</p> <p><del>(a)</del> On-street parking is to be provided to the maximum extent possible to buffer pedestrians from vehicular traffic. Parking space dimensions shall be not less than 9ft x 18ft.</p> <p><del>(b)</del> Vehicular traffic lanes on streets with bus routes shall be designed at not less than 12 feet to support safe circulation of busses.</p> <p><del>(c)</del> Bus transfers are to be located inside project boundaries to lesson traffic delays and increase safety on major roads. Bus transfers shall be coordinated and approved by Lynx.</p> <p><del>(d)</del> Shaded sidewalks and benches for resting and waiting for public transit shall be provided.</p> <p><del>(e)</del> Care shall be taken to minimize curb cuts and maximize visibility surrounding curb cuts.</p> <p>(f). New development in conjunction with the City shall pursue approval from FDOT to utilize the FDOT rail right of way for pedestrian/bicycle trails where appropriate prior to submission of project plans for City approval.</p>		OK				

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12	Cooper		I Circulation (5) Sidewalks	Recommend Sidewalk Design and Utilization be included here.		Discuss 17 ft Orange Ave sidewalk				
13	Cooper		I Circulation (6) New Streetscapes. (7) Access Standard.	(6). New Streetscapes: In order to improve the pedestrian experience, new development or redevelopment shall provide the streetscape area including sidewalks, lighting, landscape and street furniture and will dedicate pedestrian easements over this area prior to certificate of occupancy. (7) Access Standard: (a) All city services including utility access, utility equipment, solid waste containers shall be placed at the rear of lots and accessed by driveways or alleys opening to side streets. (b) Garage and parking shall be accessed through driveways or alleys opening to side streets.		Discuss Alleys				
14										
15	Cooper	10	J.(2) Setbacks/Stepbacks	Update Figure 2/3 to increase setback to 10ft and delete any floors not permitted by code.		OK.				
16	Anderson	10	J.(2) Setbacks/Stepbacks	Clarify "All setbacks of additional floors shall be measured from the allowable building line"		Discuss				
17	Anderson	11	Setbacks/Stepbacks, Also Subarea J, B, A	Figure 2- Discuss setback from Fairbanks and goals of the sidewalk. Need to consider Transit preferred lane width, safety and intention of the sidewalk, also consider the Maitland and Paseo actual setbacks.		Discuss				
18	Cooper	12	Setbacks/Setbacks	Clarify "space for healthy development of shade tree crowns" by adding: "Setbacks on street frontages shall provide a minimum 25 ft from the center of the planting strip to the front of the building." (D Lamn Jan21).		Discuss				
19	Cooper	13	Setbacks/Setbacks	While setbacks are not required from Railroad ROW, add "Setbacks are required from bike/pedestrian trail to allow space for healthy development of shade tree crowns. Shade trees shall be placed not more than 50 feet apart along the trail."		Discuss				
20	Cooper	14	Facades	Delete "some sort of". Delete "color" as articulation. Change "50" feet to "30" feet. Change "shall" to "may" regarding murals.		OK				
21	Cooper	14	Separation	This should standalone and not be part of Facades. Put a period after "200ft of horizontal length and delete "without separation". 200ft is the maximum building lot width in Longwood code and 150 ft is maximum width in Portland code. I recommend WP use 200ft.		300 LF, pending TND discussion and WP experience				
22	Cooper	16	(4) Additional Façade Treatment	Apply requirement to all sides visible from a public ROW and not just front and back. "Require" detailing to enhance façade versus encouraging it. You may want to specify all street facing facades are to be treated as "building fronts".		OK				
23	Anderson	17	(7) Architectural Appendages	Allowing appendages to be visible by 30% of façade seems too much. Would 10-15% be more appropriate? Also, clarify that 10 feet of additional height is from the roof, not the parapet. i.e. with a parapet, you would only see 5 feet.		Discuss				
24	Cooper	17	(10) Architectural Review	Clarify when 3-D digital images and elevations are required. Add "or" between the 3 criteria. Should you also define what drawings are required for appearance review of projects not needing citywide notice?		OK				
25	Cooper	18	Architectural Review	Does the City want drawings stamped by a Licensed Architect? (see attached Skofield request)		Discuss				
26		18	Parking Rqmt & Access Management	Based on public forum comments, residents were told this on street parking would be available to the public free. Delete option to count as code compliant parking.		OK				
27	Anderson	17	k. Parking	Generally: reconfirm that LandDesign images are using the OAO code ratios. The LandDesign tables in the 3-14-21 presentation do not match the draft code.		OK				
28	Cooper	18	j.(1). Parallel Orange Ave Access	Ensure any new or realigned road ROW is dedicated to the City. Ownership.		OK				
29	Cooper	18	j(2) (d) Hotel	Delete "at 50% of" and add "in accordance with Section 58:86(b)(16) Off Street Parking and Loading Requirements." Require off street parking in compliance with our existing Land Use Codes for Hotels and Multifamily.		Discuss				
30	Anderson	17	k. Parking- Hotels	Hotels- Ancillary Uses (restaurants, etc) might need to be re-examined. Full off street parking may be excessive, but depends on whether the restaurant is sized for JUST the hotel population or for the community-at-large		Discuss				
31	Cooper	19	j(f) Multi-family residential	Delete (1) - (3) and require parking "in accordance with Section 58:86(b)(20) Off Street Parking and Loading Requirements.".		Discuss changing to "1 space per BR"				
32	Anderson	20	(7) a Parking- FAR Incentives	Should a Parking Structure be "Mandated" as the ONLY means to use incentives? What about smaller businesses?		Discuss				
33	Anderson	20	(7) a Parking- FAR Incentives	Create a 100% FAR illustration		ok				
34	Cooper	20	(7) d FAR Parking	Clarify "small businesses". " In Subarea A"		ok				
35	Anderson	20	(7) d FAR Parking	Unclear on last sentence regarding surface parking when used to meet the 10% additional parking.		discuss				
36	Anderson	22	i. (1) Public Notices	Do we need to reference that mailed notices will meet the standard of Conditional Uses, e.g. pictures, tables, etc?		discuss				
37	Cooper	23	i. Meaningful Open Space	WP is on a high recharge area of the Floridan Aquifer. Require "NLT 25% of total lot square footage be pervious ground," consistent with CP Policy # 4-6.1.3 for Planned Developments. (attached) This new intensity is comparable to Planned Developments. As it is written now 80% of only the open space areas are required to be pervious or semi-pervious.		ok, but put into a unique section. Seems confusing mixing up the 2 goals.				

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38	Cooper	23	I. Meaningful Open Space	Define what "open and available to the public" means.		ok				
39	Cooper	25	p. Mobility Fee	Have legal draft language making it clear that the projects will be required to contribute a mobility fee prior to permitting and that the City plans to have the regulations and fee prescribed prior to approval. Impact vrs Mobility. Consider addition of Fairbanks "overpass" or "pedestrian tunnel". Legislative caps.		Discussion forthcoming.				
40	Cooper	25	Stormwater Mgmt	Add section clarifying our intent that within the OAO we encourage shared stormwater management.		ok				
41	Anderson	27	OAO Standards	D, I and J. Note that Mixed use can be vertical or horizontal.		ok				
42	Anderson	27	Subarea A (a) Exemptions	Do we need to add 8. an exemption for parking?		ok				
43	Cooper	29	Subarea A 6	6. Add: It is the City's intention to protect and promote historic resources within the OAO. Towards that end the CRA in conjunction with the City, shall provide financial and administrative assistance to the Winter Park Playhouse and 1355 Orange Avenue to submit application for designation on the local and National Register of Historic Places. Incentives in the way of relaxation of license and permit fees shall be offered to incentivize registration on the local register of Historic Places. The City shall also highlight Designers Row, Winter Park Playhouse and 1355 Orange Avenue on the city website and periodically in city news releases.		ok				
44	Cooper	29	Subarea C Height	Heights limited to 2 stories with the exception of parking structure which is limited to 3 stories/4 parking levels. Delete "with roof deck allowed above 2nd level."		ok				
45		29	Max Impervious Coverage	Change from 85% to 75% consistent with other parcels of Planned Development level FARs.		ok				
46	Cooper	30	Subarea D	Remove parcel northeast of Harmon facing Orange Avenue from Subarea D and include in Subarea A as it abuts A parcels to the northeast, the east (rear) and is directly across Orange Ave from Subarea A parcels and should have consistent entitlements. Parcels within the OAO should be grouped by common elements not by ownership. On the south end of the OAO they are not grouped by ownership.		disagree/ discuss				
47	Anderson	32	Subarea G	Remove from OAO.		ok				
48	Cooper	34	Subarea J(a) 3	With the provision of an easement for a service/emergency ally, maximum height on the southern half of this parcel oriented towards the Rollins ballfield could allow 4 stories.		discuss alley				
49	Cooper	35	Subarea J c	This parcel exceeds reasonable block sizes and the City should require an ally to service the buildings and provide access for emergency vehicles from the rear with buildings fronting W Fairbanks and Holt. Allys are a requirement in the Smartcode and would model the success achieved as a service ally by Central Avenue parallel to Park Avenue. Our current codes require 10ft loading and service areas behind buildings. This requirement does not appear to have been moved forward into the OAO.		discuss				
50	Cooper	35	Subarea J, setbacks	Setbacks change from 20 ft to 50ft. Reference Area D Orlando Setback and Paseo Canton and Denning Setback.		discuss				
51	Cooper	36-40	Idea on Enhancement/Incentive	Recommend commission identify 3 categories of Enhancements based on importance to the success of the OAO. Categorize the Enhancements into one of the 3 groups. Then decide whether those categories are worth 1.5x their value, 2.0x their value or 20x their value (example...pick any factor based on what that enhancement is worth to the City. On the Incentive Side base all value on the per square foot land value (define source) of the property in question. Calculate the per square foot yield at the allowable FAR. At the time of application. The office of Budget Management would (1) Estimate the value of the Enhancement being offered (including O&M for 20 years). (2). Apply the value factor (1.5x or 20x) initially agreed to by the Commission. (3). Calculate Incentive as the FAR increase to most closely align with the calculated Enhancement adjusted value.		Agreed on multiplier concept, discuss				
52	Anderson	35, 43	Enhancement/Incentive	Change to Incentives based on additional FAR SF from Percentage based incentive.		ok				
53	Cooper	36-40	q. Development Enhancements/Bonuses	Until staff can provide values for incentives and enhancements we will not be able to select appropriate FAR increases for each enhancement. I will revisit this section when that information is made available.		later				
54	Cooper	36	(u)1(4) Development Enhancement	Change "credited" to "compensated".		ok				
55	Cooper	37	q. Development Enhancements/Bonuses Table 4 p.1	Add option of providing surface parking where verifiable need exists. Distinguish between value of surface vrs structured parking		discuss				
56	Anderson	38	q. Workforce Housing Incentive	Add back FAR increase for Workforce Housing. In other words, take the "Land and Sitework" costs out of the Workforce unit by granting the FAR. May need to be 1.5 to 2x to really incentivize.		ok				
57	Cooper	38	q. Development Enhancements/Bonuses, Table 6, OS.5	Expand to specifically include "expansion of Central Park and creation of the Park at Progress Point".		ok				
58	Cooper	38	q. Development Enhancements/Bonuses, Table 6, OS.7	The monetary value of this enhancements is too broad and is minuscule at the lower end. I am willing to define a value up to 2 times the verifiable investment. Otherwise delete yard games, Play Area and water features.		ok				
59	Cooper	40	Definition	Boutique Hotel (b). Define as 10 -100 rooms often in a converted building.		discuss 100 vs 120				
60	Anderson	42	Multi-Modal Definition	Seems overly broad and a major overstatement of what we are trying to do. Suggest trying to accommodate many types of travel, particularly bicycle/scooter/pedestrian from [Orlando Urban Trail/Mead Garden Trail to MLK Park/Library to Park Avenue/Rollins]		Discuss				



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61	Anderson	44	Workforce housing Definition	change "value" definition so that SF Dwelling Units equate to SF Dwelling Units and Condos equate to Condos. Is 120% mandated? Shouldn't the Workforce Value be less than the Median Selling Price?		Discuss				